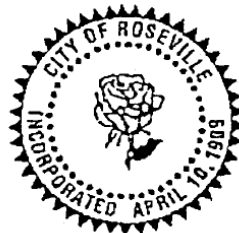


# **NORTH CENTRAL ROSEVILLE SPECIFIC PLAN**

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**City of Roseville**



ADOPTED JULY 5, 1990  
RESOLUTION #90-170



# ***NORTH CENTRAL ROSEVILLE SPECIFIC PLAN***

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*July 5, 1990*

Resolution# 90-170

Final

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# North Central Roseville Specific Plan

**Adopted July 5, 1990  
Resolution #90-170**

## Incorporated Amendments:

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November 6, 1991; Resolution #91-280  
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October 7, 1998; Resolution # 98-363 (Parcels 21B, 42 & 42B)  
January 6, 1999; Resolution # 99-20 (Parcels 27, 28 & 93)  
January 19, 2000; Resolution #00-35 (Parcels 43, 48A&B, 49A&B, 55A, 61 & 65)  
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July 5, 2000; Resolution #00-304 (Parcels 20C, 54A&B, 56)  
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June 5, 2013; Resolution #13-213 (Parcel 46)  
April 3, 2024; Resolution #24-088 (Parcels 35, 36, 95)



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# 1. Introduction and Project Setting

## 1.1 The Specific Plan Area

The City of Roseville is located in the Central Valley of Northern California within comfortable driving distance of both the Sierra Nevada Mountains and the Pacific Coast. As shown in Figure 1-1, the North Central Roseville Specific Plan Area encompasses approximately **1,823** acres and is located within the City of Roseville, generally between Washington Boulevard and Interstate 80. Interstate 80 provides a regional link between Roseville and the San Francisco and/ or Lake Tahoe regions.

## 1.2 Purpose and Content

The City of Roseville amended the Land Use Element of the City General Plan on November 6, 1985 by allocating preliminary land use within each of four Specific Plan Areas in the City. The North Central Roseville Specific Plan refines the 1985 land use allocation and serves as the ultimate guide to development in the area. The Specific Plan implements the intent of the City by setting forth regulations, conditions and programs which will further carry out the objectives and policies of the General Plan. The Specific Plan process provides the opportunity to plan, coordinate, implement and monitor development over a large area in a comprehensive manner.

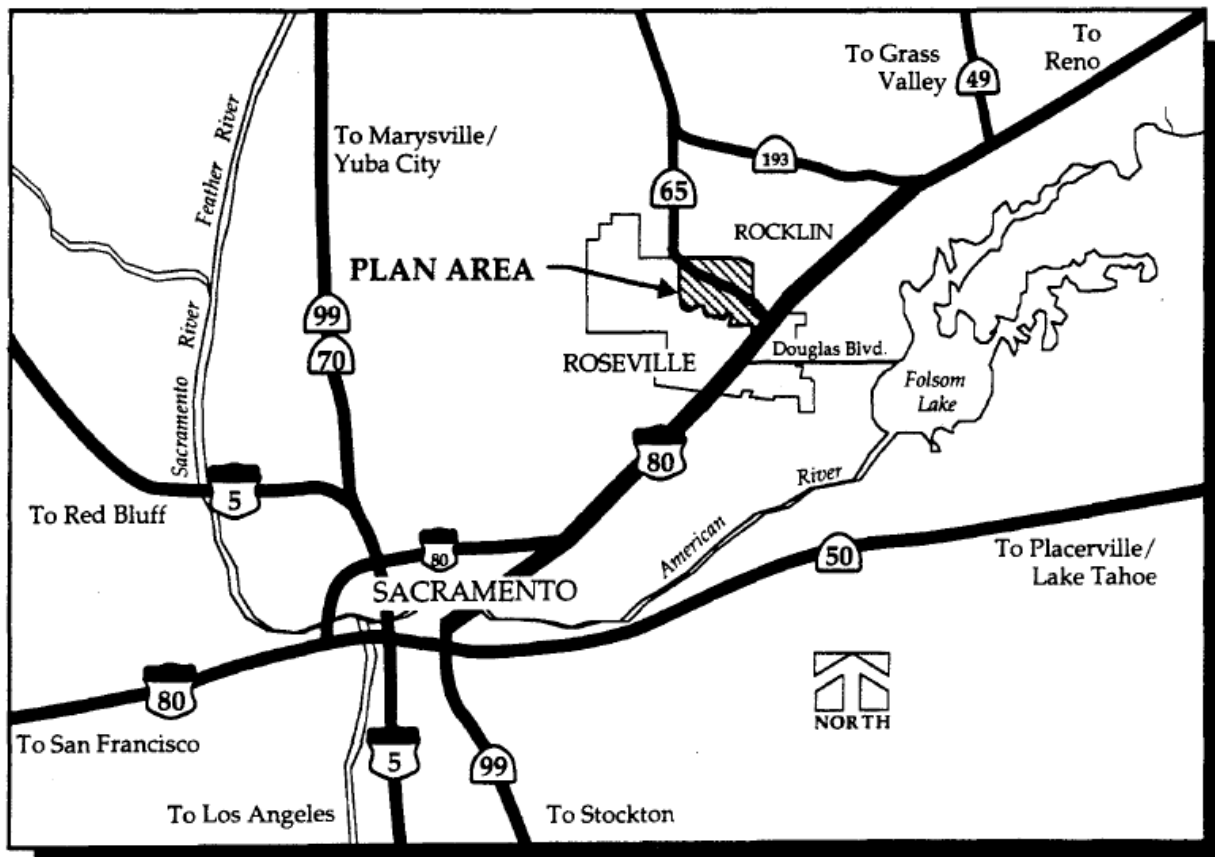


Figure 1-1: Regional Location Map

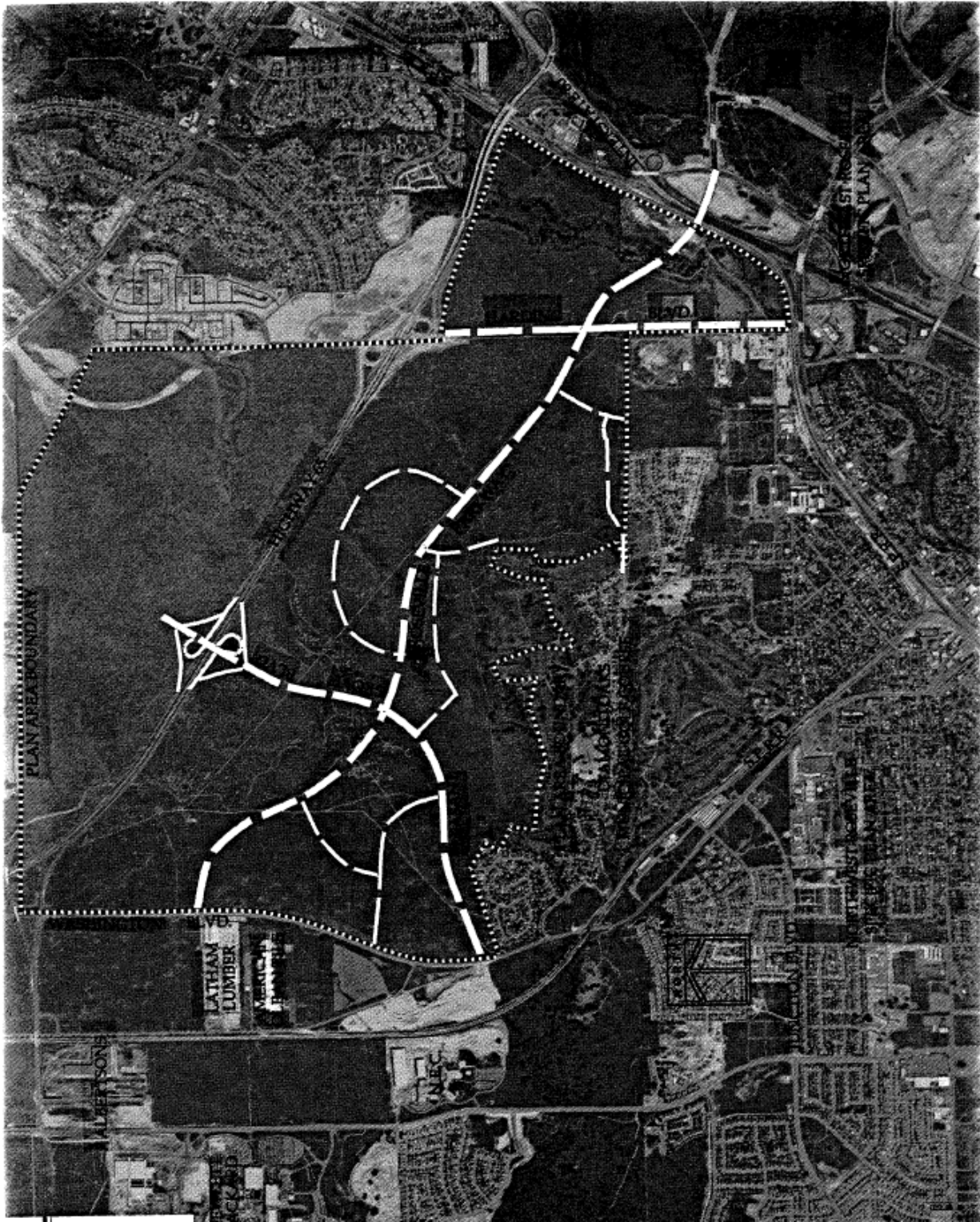


Figure 1-2: Aerial Photograph

The Plan combines the elements of a General Plan, zoning ordinance and capital improvements program into a single document tailored to the North Central Plan Area. The Specific Plan is intended to provide for the orderly and systematic development of the Plan Area in a manner consistent with the policies of the City of Roseville, and the characteristics and limitations of the land. All individual development projects within the Plan Area are subject to the goals, policies and guidelines set forth in this Specific Plan.

The North Central Specific Plan document is composed of several elements, and sections, with related policies, graphics and exhibits, which address the following specific subject areas:

***Section 1. Introduction and Project Setting***

This section provides an overview of the structure and concept of the Specific Plan, and a summary of the factors that influence the character of the Plan Area.

***Section 2. Land Use Element***

The Land Use Element describes the basic concept and organization of the land uses proposed in the Plan Area.

***Section 3. Community Form and Design Element***

The Community Form and Design Element contains the policies which will guide specific development within the Plan Area and, thus, will establish the distinctive features that will come to be identified with the community.

***Section 4. Housing Element***

The Housing Element addresses the concern of providing a housing mix that will be affordable and meet the needs of a portion of the projected future work force and residents of the Specific Plan Area.

***Section 5. Circulation Element***

The Circulation Element summarizes the circulation facilities, including roadways, pedestrian and bicycle systems, public transit and future transportation system management measures.

***Section 6. Resources Management Element***

The Resources Management Element describes the natural features in the Plan Area, and the manner in which they are protected and incorporated into the proposed urban pattern.

***Section 7. Public Facilities and Services Element***

The Public Facilities and Services Element summarizes the basic infrastructure, utilities, schools, parks, fire stations, libraries, and other facilities and services, required for the projected Plan Area population.

***Section 8. Implementation***

The Implementation section briefly outlines public and private land use regulations, proposed methods of maintaining certain public and private areas, infrastructure sequencing, and a description of proposed public facility and infrastructure financing mechanisms.

### 1.3 Legal Authority

The City of Roseville, as a charter city, is adopting this Specific Plan in accordance with a process consistent with the provisions of Article 8, Sections 65450 through 65457 of Title 7 Planning and Land Use Law, California Government Code. These provisions require that a specific plan must be consistent with the adopted general plan of the jurisdiction in which the plan is located. It is required that all subsequent subdivision and development, all public works projects, and zoning regulations must be consistent with the Specific Plan.

The North Central Roseville Specific Plan is consistent with the goals, policies and objectives of the Roseville General Plan.

### 1.4 Plan Area Setting

The City of Roseville is located along Interstate 80 approximately midway between the cities of Sacramento and Auburn. The North Central Roseville Specific Plan Area is generally situated between Interstate 80 and Washington Boulevard, north of and contiguous with the developed portion of Roseville. (Figure 1-3, Local Area Map and Figure 1-4, Plan Area Map). As of May 1990 the Plan Area was primarily undeveloped.

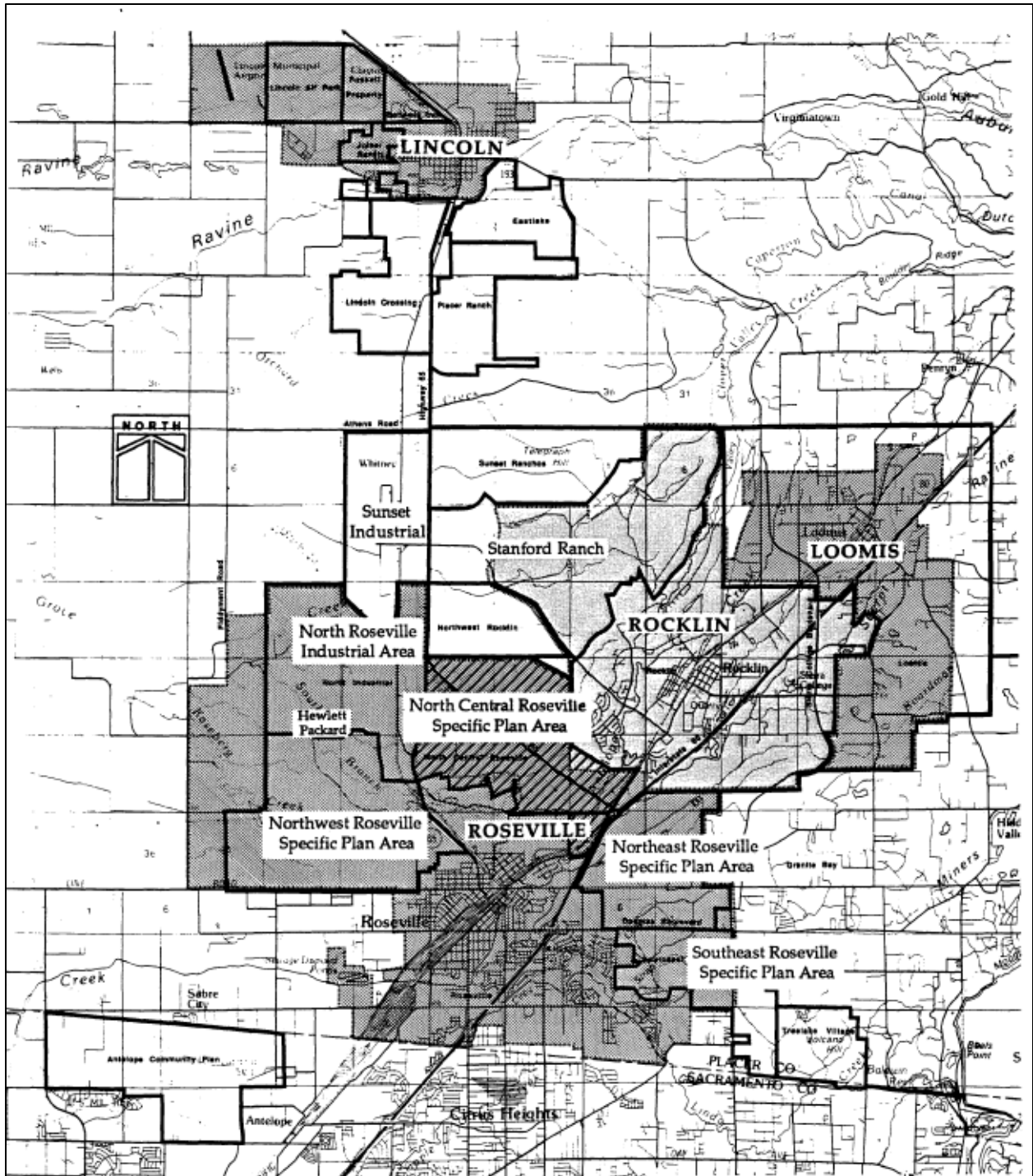


Figure 1-3: Local Area Map

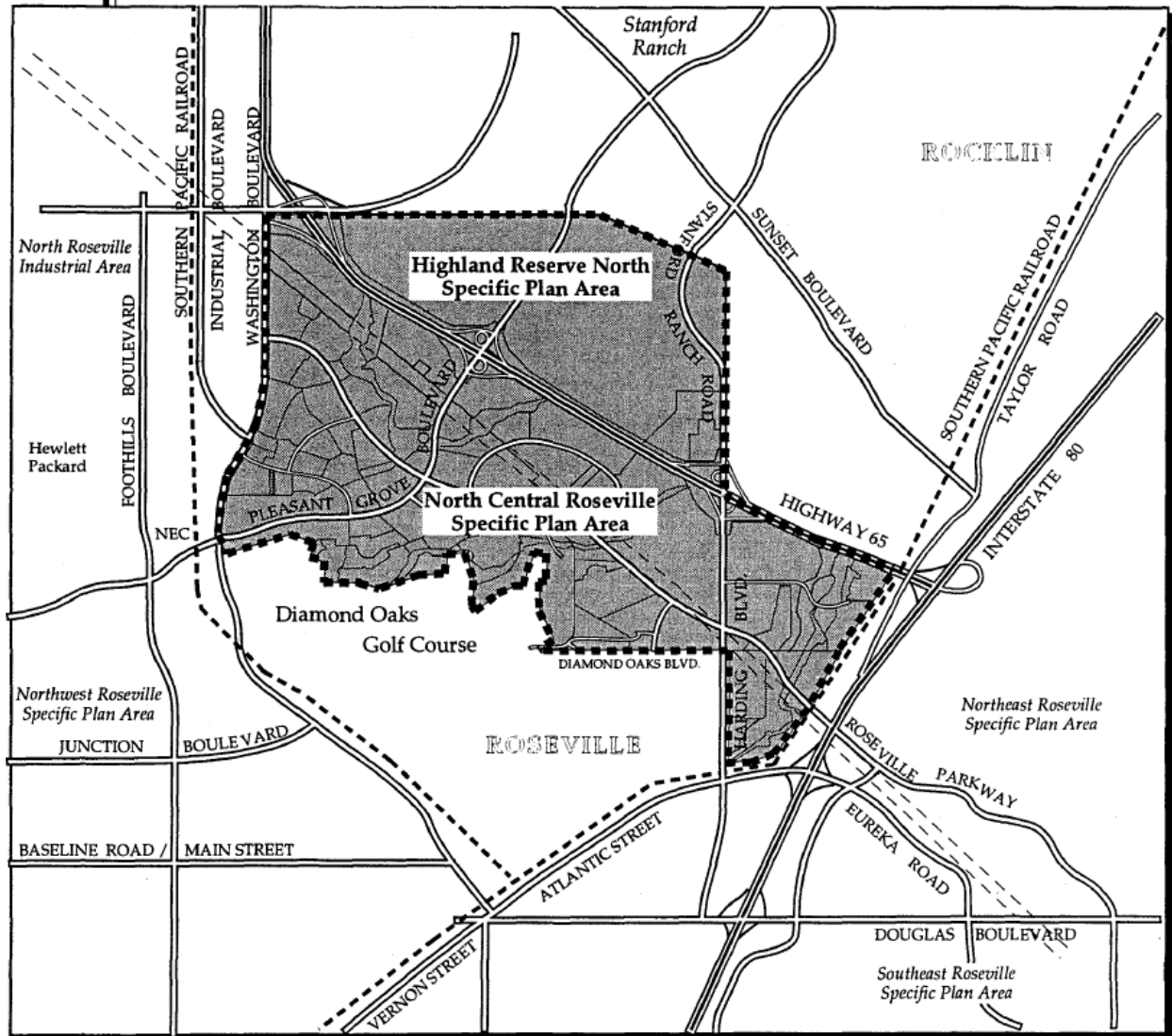


Figure 1-4: Plan Area Map

#### 1.4.1 Surrounding Land Uses

The North Central Specific Plan Area lies along the northern edge of the existing urban area of Roseville. The Plan Area is adjacent to existing and planned urban land uses on three sides, and is transected by Highway 65. The Plan Area is a logical location for the extension of urban uses in the City.

Immediately south of the Plan Area lies the Diamond Oaks Municipal Golf Course and a single family neighborhood. On the southeast edge, near Harding Boulevard, is an industrial area that includes the former City landfill site, a commercial landfill, salvage and transfer operation, and electric substations owned by the City and by PG&E. To the east lies Interstate 80 and the planned commercial, business-professional, and research and development uses in the Northeast Roseville Specific Plan Area. Also to the east, north of Highway 65, lie the existing and planned residential neighborhoods in the City of Rocklin, including single family dwellings and apartments.

An unincorporated area of Placer County immediately north of the Plan Area is currently used, in part, for mining operations, but is otherwise undeveloped and presently unplanned. Industrial land uses, including warehouse and manufacturing activities are located west of the Plan Area, beyond Washington Boulevard. Such uses include: Hewlett Packard, NEC, Albertsons Distribution Center, Latham Lumber and American Olean Tile.

#### 1.4.2 Existing Conditions

Historically, the North Central Plan Area has been utilized for grazing, with limited dry land grain farming to the south and west. Existing man-made features in the Plan Area as of May, 1990 include high-voltage powerlines in parallel easements transecting the site from southeast to northwest, two electrical substations located along the future alignment of Harding Boulevard, the Highway 65 Bypass, and Stanford Ranch Road connecting the bypass to Sunset Boulevard in Rocklin. Remains of 19th Century rock walls occur in the central portion of the Plan. These walls, however, have not been identified as historically significant.

The North Central Specific Plan Area is situated within two watersheds. Over ninety percent (90%) of the Plan Area is in a watershed which drains to Pleasant Grove Creek. The remaining area drains into Antelope Creek, which crosses through the southeastern corner of the Plan Area. Antelope Creek is recognized as the only perennial, anadromous stream in the North Central Specific Plan Area. Figure 1-5, Natural Drainage and Watersheds, illustrates the general bounds of the unnamed sub-area drainages within the Plan Area. Within these generally westerly-oriented intermittent watercourses are numerous smaller intermittent stream courses which carry run-off during winter storm events, but are typically dry for the remainder of the year. These intermittent watercourses have been designated as "lower watershed preserve areas" within the Plan, and wetland conditions have been identified within them.

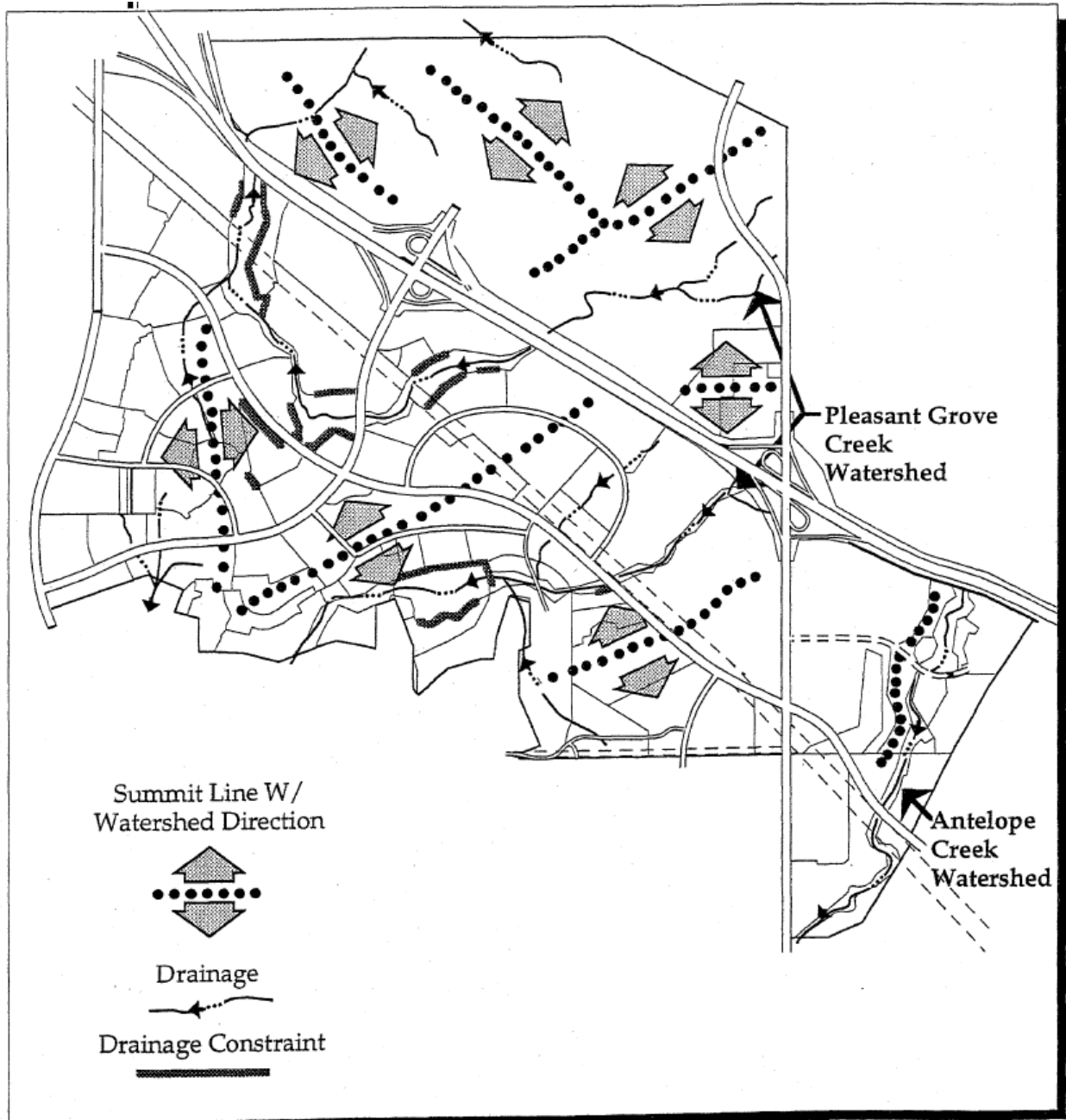


Figure 1-5: Natural Drainage and Watersheds

The Plan Area is characterized by gently rolling terrain sloping generally to the west. Slopes in excess of twenty percent (20%) occur only in small portions of the Plan Area near the lower watershed corridors, most notably on the eastern portion of the Plan along Antelope Creek. (Figure 1-6, Slope Analysis Schematic) The highest elevation in the North Central Specific Plan Area is approximately 280 feet above mean sea level (msl) and is located near the Roseville/Rocklin City limits on the ridge overlooking Antelope Creek.

The Roseville vicinity is generally regarded as a transitional zone between the flat open terrain of the Central Valley and the foothills of the Sierra Nevada Mountains. As a consequence of the underlying

Mehrten geologic formations, the North Central Specific Plan Area is not typical of the terrain and vegetation common to either of these habitats. These geologic formations are the single most significant environmental factor responsible for shaping the character of the Plan Area. The location of the Mehrten Formations within the North Central Area are illustrated in Figure 1-7, Mehrten Formations.

The volcanically-originated Mehrten formations are largely impervious, and overlaying soils are typically shallow and dry, providing extremely limited foundation for vegetation growth. Mehrten deposits occur in two principal forms. The first form, mudflow or breccia, consists of tightly cemented ash and andesitic materials of volcanic origin. Where these flows solidified they formed impervious "caps" around which erosion occurred, and consequently, they are generally found in upland areas between drainages. The second form, Mehrten conglomerate, is classified as a sedimentary formation consisting of cobbles and aggregates cemented in an impervious medium of andesitic sands and silt. The conglomerate is generally more pliable than the mudflow formations. Mehrten material does not extend to the west side of the Plan Area, and soils originating from alluvial deposition are predominant in this portion of the site. The surface horizon in the western area of the plan is typically well drained, varying in depth to an impervious clay hardpan.

Because of the impervious nature of the Mehrten formations and of the clay horizons in adjoining soils, oak woodlands in the North Central Plan are limited to the Antelope Creek corridor and adjacent slope, along with a few trees clustered at the southern perimeter of the Plan Area. The woodland environment has been subjected to extensive grazing over the years and, consequently, is relatively open with minimal understory and middlestory growth, including limited evidence of oak regeneration. Brush species consisting of buckbrush, coffeeberry, poison oak and yerba santa occur around the native oaks where conditions have permitted establishment. Annual grassland covers more than ninety-five percent (95%) of the Plan Area, as illustrated in Figure 1-8, Vegetation Map. The grassland community has been perpetuated throughout the region by grazing, burning, and related agricultural activities.

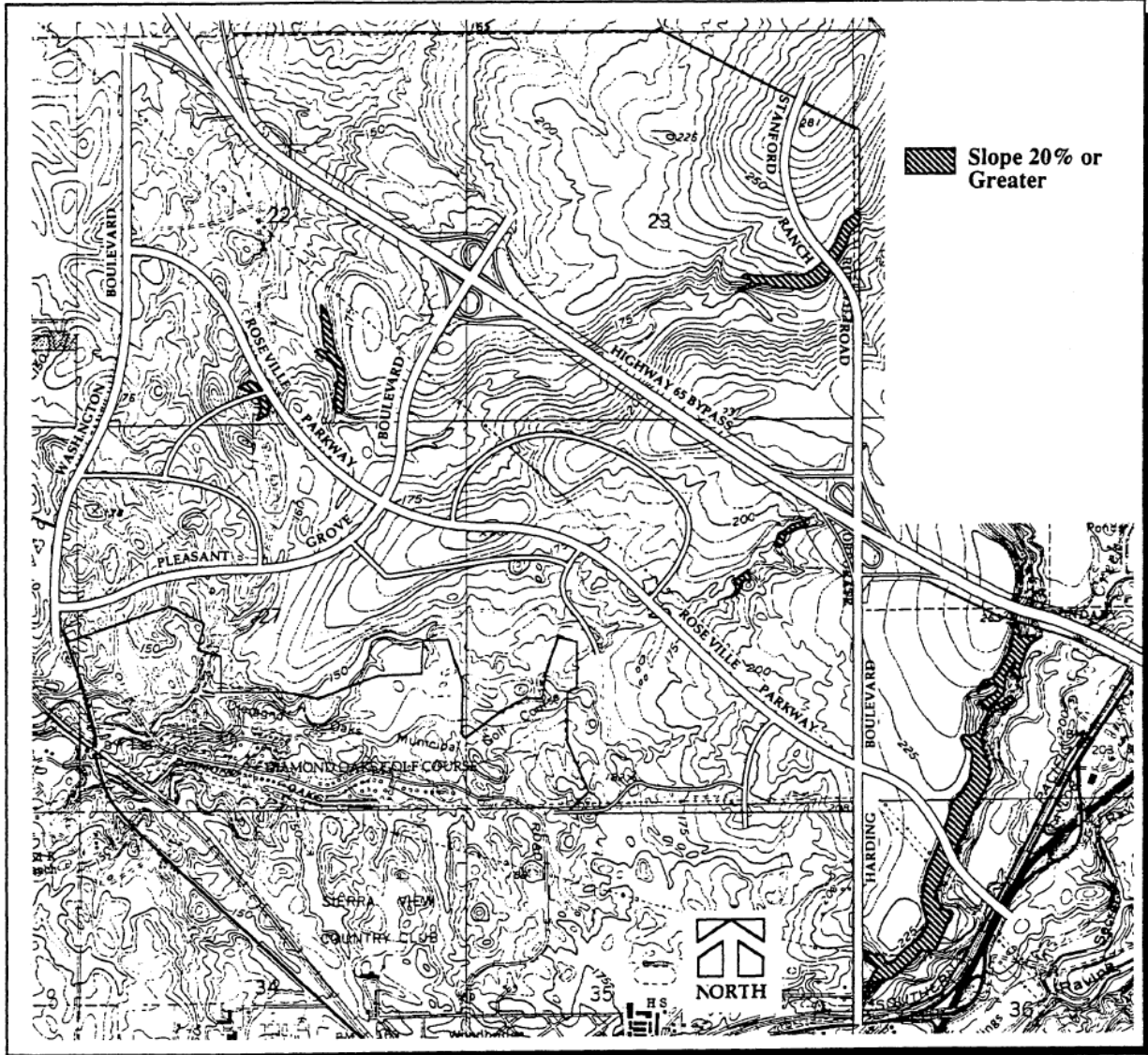


Figure 1-7: Slope Analysis Schematic

Although the Mehrten formations are not conducive to the growth of woody vegetation, the "pocketed" terrain has provided an ideal setting for the evolution of vernal pools. Vernal pools are small, hardpan-floored depressions in valley grassland mosaics that fill with water during winter months. The water regime of a vernal pool is seasonal, generally involving recharge during the winter and spring seasons, eventually drying up during the latter part of the spring. Individual pools vary significantly both in the length of time they remain wet and in plant species diversity. Native plant species have adapted to the unique pool habitat, and as such are limited in their distribution to pool areas. Several plant species of interest have been identified in the vernal pools in the Plan Area.

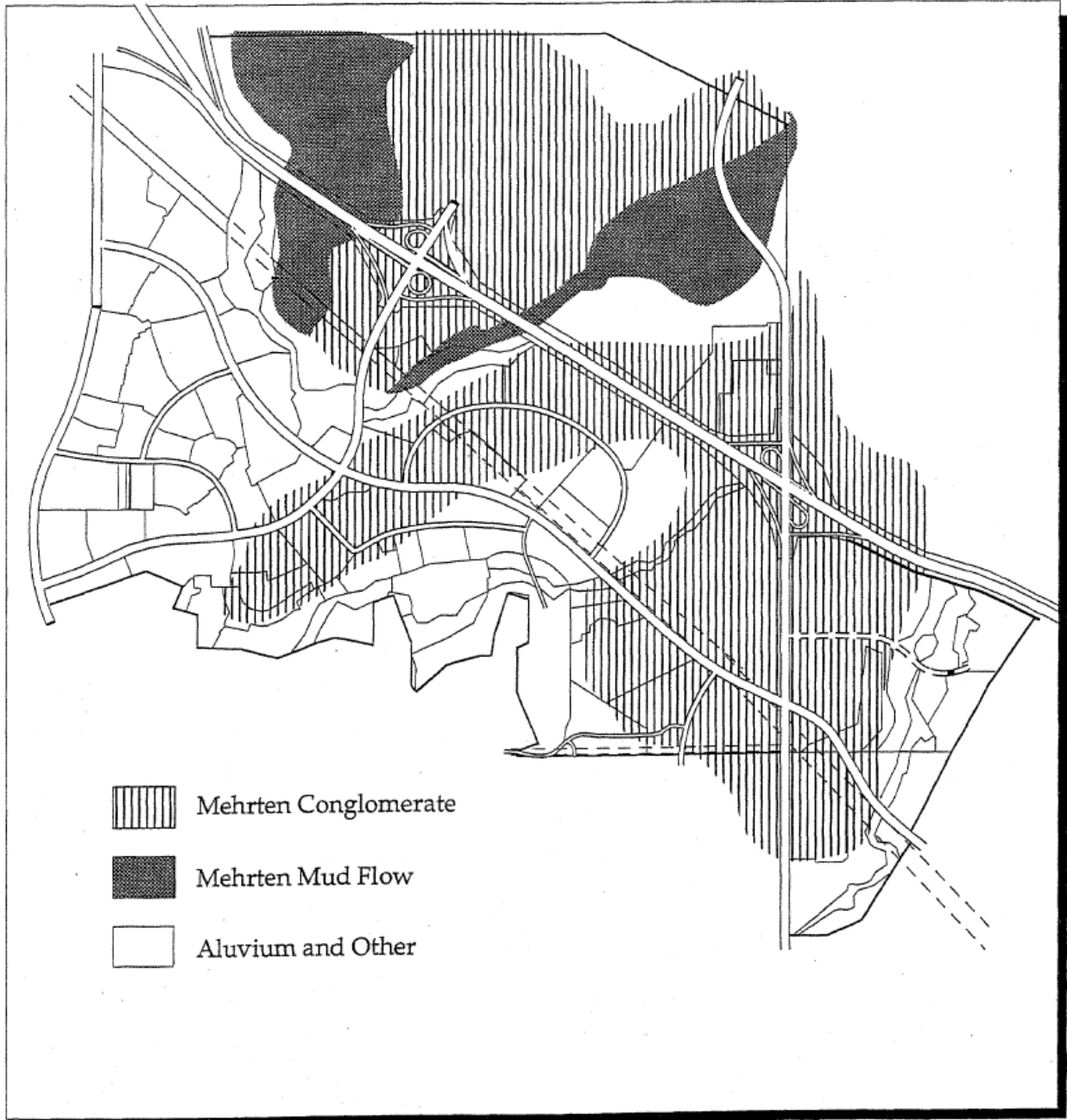


Figure 1-8: Mehrten Formations

The natural features within the North Central Plan Area have been considered as a primary determinant in the land use pattern, and are specifically addressed through policies in the Community Form and Design Element and Resources Management Element.

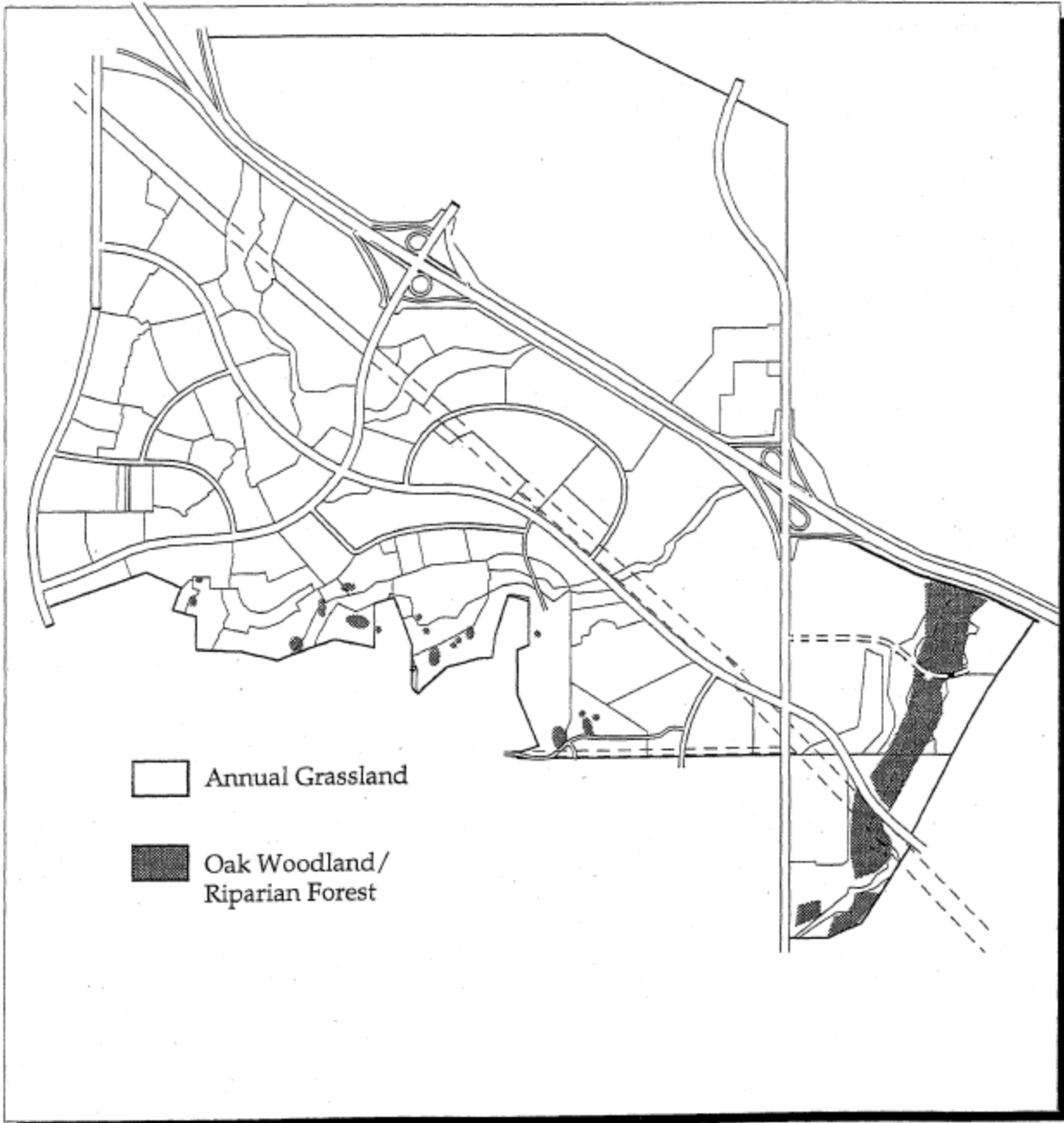


Figure 1-9: Vegetation Map

## 2. Land Use Element

The Land Use Element describes the basic concept and organization of the land uses proposed in the Plan Area. These concepts are further implemented through the policies and guidelines contained in Section 3, the Community Form and Design Element.

Excellent access to the regional transportation network, future light rail, and high visibility along Highway 65 and Interstate 80, provide the opportunity for the three key elements of the North Central Roseville Specific Plan:

1. a regional commercial core
2. high intensity commercial, business-professional, and light industrial employment centers
3. associated residential land use

Each of these three key elements requires a certain degree of individual identity and exclusion of other uses. Contemporary technology, land economics, and patterns of business aggregation dictate a particular scale and pattern of land use.

The complex mix of individual preferences and contemporary economic reality, combined with the ready accessibility of automobiles, often results in a land use pattern where uses are isolated from one another, and automobiles provide the only safe access. The consequence of this pattern is increased reliance on the automobile and increased segregation of the many facets of everyday life. The effect of such segregation on the community social fabric includes: the costs of increasing dependence on automobiles, increased household auto-related expenditures, time lost in commuting, public expenditures for traffic systems, and, of course, the continued deterioration of overall air quality.

The segregation of land uses, and the fragmentation of community life that it engenders, can be partially countered by integrating the appropriate scale and mix of land uses, and by providing people with alternatives to the use of their automobiles. The North Central Specific Plan is one component of an on-going effort by the City of Roseville to provide a balanced land use pattern and a high quality of life for residents and workers. This Specific Plan is based on two key policy documents. The first, the City General Plan, adopted in 1985, established the mix of land use, as well as many principles applied in this Specific Plan. A key emphasis of the General Plan, and specifically the Housing Element, is to provide a mix of housing and employment opportunities that will ensure that residents need not commute far to work. The Specific Plan addresses this need through the location and amount of land allocated to each use.

The second major document is the South Placer Policy Plan adopted by the City in 1982. The Policy Plan sets forth guidelines for land use, spatial relationships and design guidelines intended to facilitate both public transportation and pedestrian access on the community level. The North Central Specific Plan represents the most specific and refined application of the principles established in the Policy Plan; however, the basic concepts of providing pedestrian access and the spatial proximity of work, housing, shopping, and leisure uses are evident in all of the Specific Plans adopted by the City.

### 2.1 Land Use Goals

The North Central Specific Plan is intended to create a unique community within the City of Roseville. The Plan is designed to achieve the following goals:

1. The Plan Area will provide a high intensity regional commercial and economic center for the South Placer region.
2. The Plan Area will provide a comprehensive community form and design which integrates residential and non-residential land uses, such that residents are encouraged to walk to shopping, services, employment and leisure activities.
3. The Plan Area will provide a comprehensive circulation system for vehicular traffic, cyclists, and pedestrians, and will accommodate both bus service and the potential for future light rail service.
4. The Plan Area will provide a mix of employment and housing opportunities that achieve a portion of the jobs/housing balance within the south Placer region.
5. The Plan Area will provide residential neighborhoods that are:
  - Safe for residents, particularly for children
  - Quiet and buffered from noise and other nuisance factors
  - Convenient in terms of access to services and shopping
  - Pedestrian-friendly
  - Protected from through vehicular traffic
  - Clearly defined with a central focus on neighborhood identity
  - Responsive to significant natural features
  - Adequately buffered from existing and proposed industrial uses west of Washington Boulevard.
6. The Plan Area will provide visual landmarks in the form of prominent buildings, formal landscape corridors along major arterial streets, and permanent views through open space corridors. These landmarks are intended to provide visual orientation within the community, and to provide an identity of the North Central Plan Area as a major entry to the City of Roseville.

## 2.2 General Land Use Concept

The North Central Specific Plan provides a broad mix of uses, including a regional commercial center, a major employment center, and a distinctive residential area in a setting that preserves and incorporates substantial open space. The Plan Area land use pattern is organized into a series of intensity bands radiating from a commercial core and the Highway 65 Bypass. The most intense band is along the northern and eastern portions of the Plan Area, and includes regional commercial, business-professional and light industrial uses. A band of secondary intensity, including multi-family and commercial uses, acts as a transition in the central portion of the Plan Area. The least intensive band includes primarily single-family uses along the southern and western portions of the Plan. The Plan places particular emphasis on pedestrian access and on the ease of movement from one land use to another. The purpose of this emphasis is to have a community where residents, workers, and other visitors are provided a viable alternative to the use of their cars.

Ease of movement is addressed on two levels in the Plan Area. The first level is primarily a matter of having the proper location of land uses relative to one another, and to the circulation system. For example, the locations of multi-family housing sites are along major arterial or collector streets to provide ease of access to public transit. In addition, multi-family housing is generally located near major employment, and retail or service centers. This proximity of appropriate land uses can help to reduce vehicle use by simply minimizing trip lengths and, thereby, making walking, cycling, or public transit a

reasonable alternative. The spatial pattern of land uses as they relate to ease of travel, the appropriate mix of uses, and the natural and man-made features that determine the location of land uses are addressed in this element.

The second level of planning addresses the ease of access between uses. Proximity of uses is not sufficient to encourage people to walk, bicycle, or use public transit if there are physical or psychological barriers. Therefore, it is necessary to address the physical relationship between uses, and to ensure that the pathways for pedestrians and cyclists are clearly defined safe and convenient. Planning for pedestrian public access must establish guidelines for the specific details of individual land uses and individual project development. For example, providing clear, safe pedestrian access to storefronts separate from auto circulation and parking is a basic feature that must be incorporated in a retail shopping center. The more detailed features of land use characteristics, and the specific relationships of land use interface to ensure adequate access, are addressed in Section 3, the Community Form and Design Element.

### 2.3 Principles and Policies That Determine the Land Use Pattern

The North Central Specific Plan is based on a solid foundation of planning concepts and policies well established in the City of Roseville, as affected by the particular physical conditions found in the Plan Area.

The Roseville General Plan incorporates policies designed to ensure that the diverse land uses in the community are highly accessible to one another. Furthermore, the General Plan establishes a hierarchy of land use organization that recognizes the scale and character of individual uses.

The relationship of land uses to one another is specified in the General Plan Land Use Element. These policies define the appropriate location of residential and commercial land uses relative to major arterial streets, and relative to the surrounding land use mix. The General Plan Transportation and Open Space Elements also specify the connection between land uses for public transportation, and for vehicular and pedestrian circulation.

The neighborhood and community commercial areas in the North Central Specific Plan are designed and located relative to the residential areas to meet the normal daily needs of the resident population within the immediate service area. A majority of the residents in the Plan Area are within a one-fourth to one-half (1/4 to 1/2) mile radius of a commercial use. The land use relationships applied in the North Central Specific Plan encourage pedestrian access and public transportation linkages. For example, the Plan Area provides a close proximity between the more intense residential areas and the major employment centers. In addition, residential land use is planned so that seventy to eighty percent (70 to 80%) of the population will be within 1,300 feet of a potential transit (bus) stop.

Pedestrian access is fulfilled in the pedestrian pathways, bikeways, pedestrian access points, and open space corridors linking land uses in the Specific Plan.

### 2.4 Conditions That Determine the Land Use Pattern

In addition to the fundamental concepts of providing appropriate sizing and relationships between uses, the pattern of land use in the North Central Specific Plan Area is determined by a number of natural and man-made features, both within the Plan Area, and in the surrounding community.

The key features illustrated in Figure 2-1, Constraints and Opportunities Diagram, include:

- The primary vehicular circulation system, including Highway 65, and the other major arterials which transect the Plan Area as part of the City's overall circulation master plan (see Figure 1-4, Plan Area Map);
- The regional access and high visibility afforded by Highway 65, I-80 and the proposed major arterials in the Plan Area;
- The adjacent land uses, both existing and planned;
- The natural land features, including the Mehrten geologic formations, the drainage patterns, and topography;
- The native vegetation and other natural features of special character including the native oak trees, the vernal pools and other natural wetlands; and
- View corridors and urban design considerations.

Each of these factors is briefly discussed below:

#### 2.4.1 Primary Street System

The primary street system through the Plan Area is a reflection of the city-wide circulation master plan. This system is designed to provide connecting linkages between various sections of the City. The planned system includes: Highway 65, Washington Boulevard, Harding Boulevard, Stanford Ranch Road, Pleasant Grove Boulevard, and Roseville Parkway. These major arterial roads divide the entire Plan Area into a series of sub-areas or quadrants.

Where the major arterial streets are flanked by non-residential or multifamily residential uses, such uses are buffered from the roadways by a fifty foot (50') landscaped corridor. In those limited circumstances where a major arterial passes adjacent to a single family neighborhood, a landscaped corridor and soundwall are required to create a buffer between the homes and the adjacent arterial. The landscape corridors also provide for pedestrian/bicycle circulation, and can accommodate light rail in the event it is extended into the Plan Area. Pedestrian access connections from adjacent uses are required at regular intervals to ensure convenient access to the corridors.

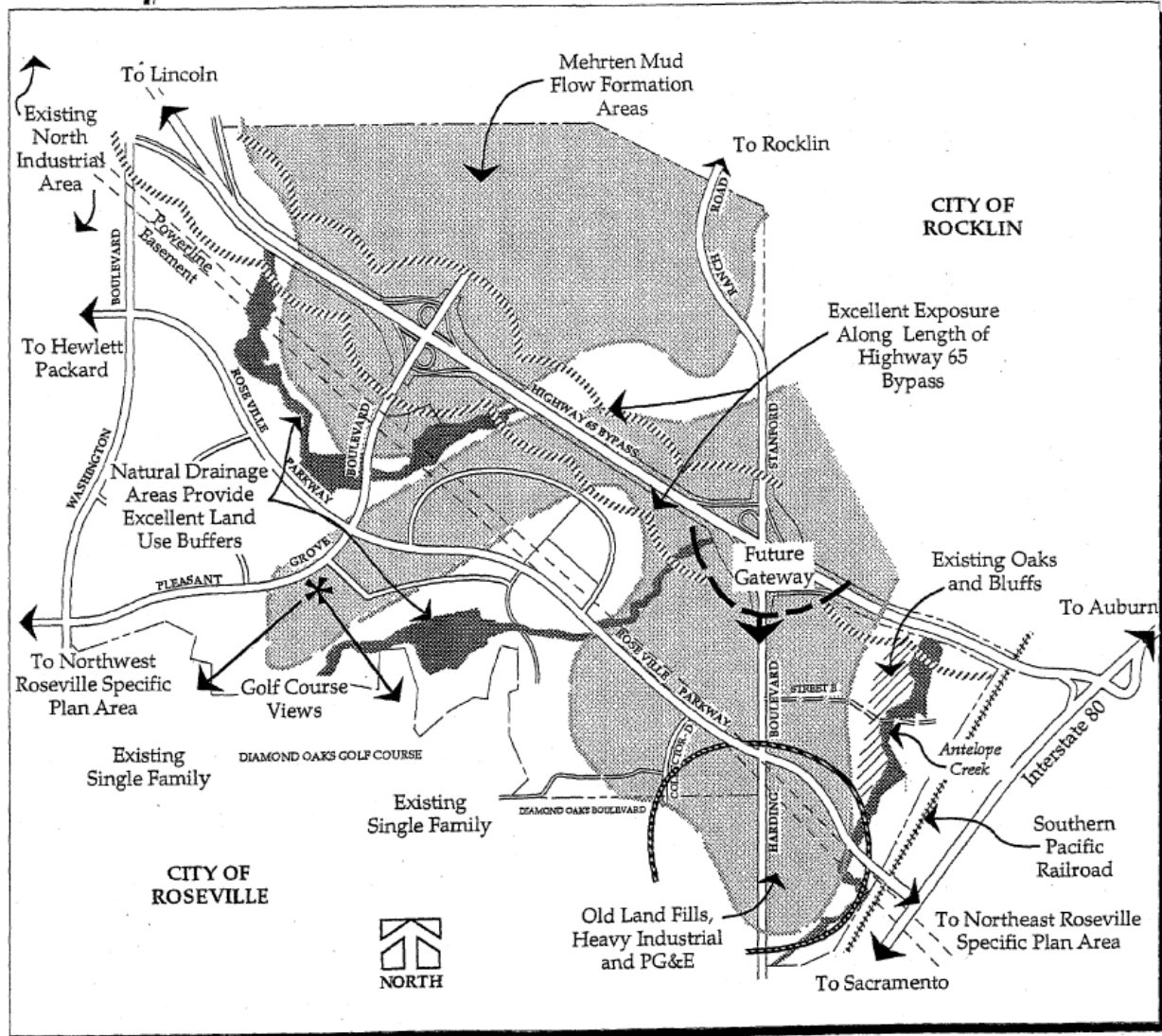


Figure 2-1: Constraints and Opportunities Diagram

### 2.4.2 Regional Access and Visibility

The development of new arterials in the North Central Plan Area will provide additional circulation linkages within the City of Roseville, with the City of Rocklin, and with other areas in the South Placer and Northern Sacramento regions. The result will enhance local circulation, enlarge the retail and service market area for the community, and provide improved access to local employment centers, thereby reducing the average commute distance for people living and working in the South Placer area. The Highway 65 Bypass and other major roadways will provide a high level of regional visibility and access to the adjacent land uses in the Plan Area. The land use allocation and pattern are reflective of this regional orientation. Future connections to Sacramento and Rocklin, as well as Lincoln and other points to the north, may be provided by extension of the light rail system.

Development of the Plan Area will allow an opportunity to clearly define the northern boundary and visual gateway to Roseville. Landscaping along the major arterials, along with the careful treatment of key buildings, will create a unique entry statement as one enters the City.

### 2.4.3 Adjacent Land Use

The uses surrounding the North Central Specific Plan were considered in developing the Plan Area land use pattern. The goal was to minimize any potential conflicts and inconsistencies with surrounding uses. A portion of the southern boundary of the Plan Area is adjacent to existing and planned single family neighborhoods and to the Diamond Oaks Municipal Golf Course. Along this edge, residential neighborhoods have been planned that will relate to the scale and character of the existing neighborhoods. The golf course also provides an opportunity for home sites oriented directly to the course.

The southeastern boundary of the Plan Area abuts old land fill sites, industrial uses and the electric substations operated by the City and PG&E. In these areas, primarily non-residential uses such as commercial, business-professional, and industrial have been allocated. Such uses are compatible with and will not be as adversely affected by the existing uses.

To the north and east, the Plan Area is bounded by the Roseville/Rocklin corporate boundary and the regional transportation corridor which includes the Southern Pacific Railroad (SPRR), Interstate 80, and Taylor Road. Commercial and business-professional land uses are being developed immediately east of this corridor along Taylor Road and within the Northeast Roseville Specific Plan Area. In Rocklin, residential land use extends south of Sunset Boulevard, nearly abutting the Roseville/Rocklin corporate boundary and the Highway 65 Bypass. The intensive regional transportation corridor suggests a relatively intense land use to take advantage of the circulation and good visibility provided. As a result, regional commercial and office uses have been planned for this portion of the Plan Area.

A small area of approximately 50 acres is isolated from the remainder of the Plan Area by the bluff and Antelope Creek at the eastern edge of the Plan. A short connecting road will be provided from the commercial core area atop the bluff. Relatively intensive land use has been allocated to this sub-area due to the proximity of Highway 65, the Southern Pacific Railroad corridor and the intensive land use planned on the adjacent bluff and along Harding Boulevard. However, the natural environment along Antelope Creek requires careful consideration of the design of any use. Business-professional uses in a "garden office" setting oriented to the creek, and multi-family residential uses with barrier landscaping, special unit orientation and design, and other buffering to mitigate the effect of adjacent circulation corridors, have been designated.

The designation of urban land use north of the Highway 65 Bypass is not considered as part of the Specific Plan, with the exception of a commercial site located at the intersection of Highway 65 and Stanford Ranch Road. This site is located to accommodate highway related uses and to serve the adjacent residential areas in Rocklin. The remainder of the area north of the Highway 65 Bypass has been designated as urban reserve. This area may be considered for urbanization in the future subject to amendment of this Specific Plan, the City General Plan and additional environmental analysis.

The northern boundary of the urban portion of the Specific Plan is the Highway 65 Bypass. The adjacent land is highly visible from the Bypass and is influenced by the traffic and the sense of intensity that is associated with a major regional expressway. These conditions suggest land use that benefits from freeway visibility and access, and is not adversely affected by the freeway noise. Such uses include: business-professional, combinations of business-professional and commercial uses, regional commercial, and light industrial uses. All of these uses are included in the Plan as freeway-oriented,

planned developments. The planned uses will provide a buffer between the intense activity along the Highway 65 Bypass and the lower intensity residential uses in the southern portions of the Plan.

To the west of the Plan Area is the Roseville North Industrial Area. Although the majority of this area is undeveloped as of May 1990, the City envisions the development of light industrial and industrial land uses. Businesses located in the North Industrial Area include: Hewlett Packard, NEC, American Olean Tile, Latham Lumber, H.B. Fuller, and Albertsons Market Distribution Center. Along the Highway 65 frontage, north of the Roseville Parkway, light industrial land use has been planned which relates both to the freeway and to the industrial uses to the west. To the south of the industrial area, the planned land use is single family residential. The residential uses will provide a significant work force in close proximity to the large employment base anticipated in the north industrial area. The single family neighborhoods will be buffered from the industrial uses by a fifty-foot (50') wide open space/bike trail corridor and the width of the right-of-way of Washington Boulevard (including soundwalls). Given the distances between the residential and industrial uses and the buffering requirements, a transitional land use is not necessary.

#### 2.4.4 Natural Land Formations

The natural features which have the most significant impact on the Plan Area land use pattern are the Mehrten formations illustrated in Figure 1-7, Mehrten Formation. Due to the difficulties and costs of working with this soil condition, the more intensive land uses are located on the Mehrten areas. Landscaping also requires special consideration due to the poor drainage provided by the soils. It is advantageous to have uses for which the landscaping can be concentrated in selected areas and is likely to receive professional regular maintenance. Such uses, which typically include business professional, retail, and multi-family residential uses, are located generally in the north, central, and eastern portions of the Plan Area on the Mehrten formations. The Mehrten formations do not extend to the west and south end of the Plan Area. In these locations, single family neighborhoods that require grading for individual home sites and the planting of a variety of easily maintained landscape materials can be more readily achieved.

The primary natural drainage swales in the Plan Area are well defined and are designated as permanent open space to be dedicated to the City. Consequently, the drainages provide a visual edge and a potential buffer to the adjacent land use. Pedestrian connections within and across these drainages are to be provided as described in specific policies incorporated into Sections 3 and 5 of the Plan. Drainage sheds generally flow to the west and tend to intersect arterial streets thus effectively transecting the Plan Area into a series of sub areas.

Steep topography is generally not a constraint in the Plan Area. Isolated locations where slopes exceed a twenty percent (20%) gradient occur along portions of the major drainages; however, these locations are generally encompassed within the dedicated open space corridor and are not a significant factor to the adjacent land use. In two locations in the west end of the Plan Area the existing slope in excess of twenty percent (20%) gradient will be modified to construct Roseville Parkway. The most extensive slope areas occur along the bluff at the western portion of the Plan Area overlooking Antelope Creek. Due to the slope condition and the existing oak trees, the potential for development in this area is restricted to a small plateau at the central portion of the slope immediately north of the planned connector to the property east of Antelope Creek. Uses permitted on the plateau may include small low

scale buildings, such as a restaurant or a small clustering of specialty retail or business-professional offices. The remainder of the slope area will be set aside as a non-developable easement.

#### 2.4.5 Vegetation and Other Natural Features

The occurrence of oak woodlands in the Plan Area is very limited. The highest density of oaks occurs at the western portion of the Plan Area along Antelope Creek. A majority of this area has been set aside in open space as lower watershed or as slope easement.

Vernal pools and other wetlands are a significant natural feature in the Plan Area, and a major factor in determining the potential location of the land use. All of the primary natural drainage features are set aside as preservation areas for lower watershed wetland types. The preserved watershed features serve to establish distinct boundaries between most land uses, and also provide an opportunity for pedestrian and bikeway corridors, which are fully realized in the Plan. In addition to the lower watershed preserves, there are four designated wetland preserve areas. Three of the preserve areas encompass the upland vernal pool types. The upland vernal pools occur on the Mehrten formations which are also planned for the more intensive retail, business-professional, and multi-family land uses. The intensive uses are located in response to the Mehrten formations, the primary circulation pattern, and the adjacent land uses. The environmental factors that contribute to the location of the more intensive land uses also coincide with the natural location of the wetland concentrations. To ensure the preservation of the most significant pool concentrations, the preserve areas were identified early in the planning process and are reflected in the land use pattern. In some cases the preserves provide a buffer between the higher and lower intensity uses.

#### 2.4.6 View Corridors and Urban Design Considerations

Currently undeveloped, the Plan Area provides views to distant landmarks such as the Sierra Nevada Mountains, the Coast Range, the Sutter Buttes, and Mount Diablo. Short range views include the Diamond Oaks Golf Course, and the residential neighborhoods to the south in Roseville and to the north in Rocklin. Urban development will impede a totally unobstructed view of these features; however, the location of land uses can serve to maintain certain view corridors to distant vistas, and to enhance the aesthetics of the urban environment. The arterial streets, notably Roseville Parkway and Highway 65, will maintain views to the Sierra Nevada and the Coast ranges. The watershed preserve areas will maintain mid-range views to the golf course and the neighborhoods to the north and south. The business-professional, light industrial, and retail uses planned along the major drainages will, to a limited extent, help to retain the open character and views along the major arterial streets. Such uses, if properly designed, are generally more open, have large landscaped areas, and cover a smaller proportion of their sites than residential uses.

#### 2.4.7 Summary of the Land Use Pattern

The North Central Roseville Specific Plan incorporates residential neighborhoods of varying densities, a business-professional park, a light industrial park, a commercial core, and secondary commercial areas oriented both to local and regional needs. A significant portion of the Plan Area is devoted to open space wetland preservation areas, parks, schools, and acreage designated as urban reserve.

The cumulative effect of all factors that determine the land use is a pattern of a higher intensity core in the northeast portion of the Plan Area. The land use intensity is relatively high along the Highway 65 frontage and adjacent Mehrten formations, but diminishes in intensity moving south and west in the

Plan Area into the single family neighborhoods. The basic land use pattern is illustrated in Figure 2-2, Conceptual Land Use Pattern.

The Plan Area land uses and the circulation network are designed to facilitate walking and other non-auto transportation. The higher density housing is located adjacent to the employment centers to encourage and provide an opportunity for people to walk to work, shopping, and leisure activities. The regional commercial core is envisioned as a pedestrian oriented "satellite" urban area that will provide an opportunity for shopping, recreation, and business, all within the mixed-use center. The Plan Area will be served by buses and, perhaps ultimately, by light rail.

The individual neighborhoods are designed to facilitate pedestrian and bicycle access to parks, schools, commercial, and day care services. Residents will have the opportunity to live, work, and pursue leisure activities without relying solely on their auto. Pedestrian linkages and proximity of uses have been provided in the Plan to facilitate a pedestrian-friendly environment. Specific detail relating to these linkages and interfaces between uses is specified in Section 3, Community Form and Design Element.

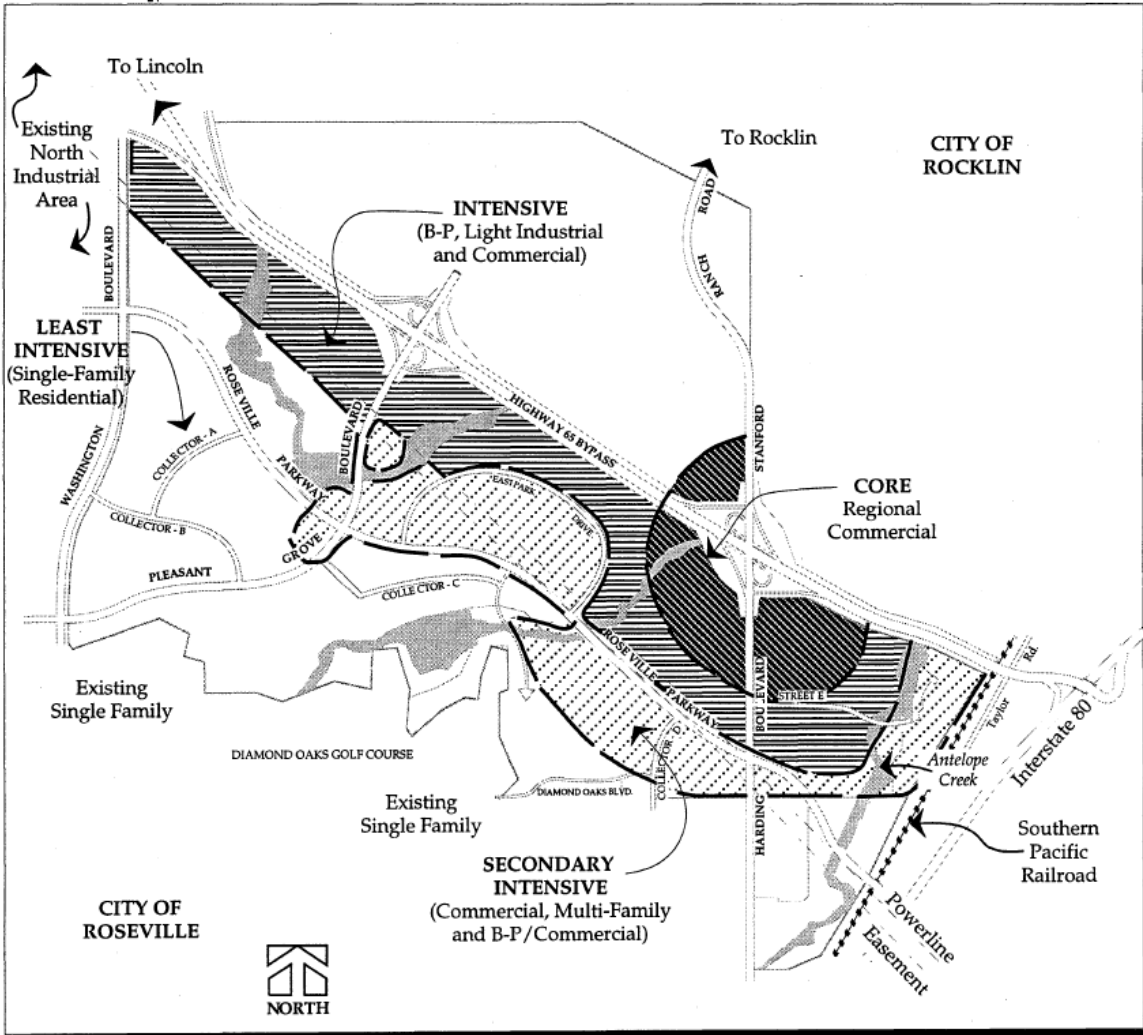


Figure 2-2: Conceptual Land Use Pattern

### 2.5 Land Use Summary

The North Central Specific Plan encompasses a total of approximately 1,823 acres. The land use distribution by category is summarized in Table 2-1, and graphically illustrated in Figure 2-3, Land Use by Category, and Figure 2-4, Land Use Map. Precise acreage and unit counts for each parcel are indicated on Table 2-2, Land Use by Parcel Number.

Table 2-1

GENERAL PLAN LAND USE (Specific Plan Land Use)	ACRES	% OF TOTAL ACRES	UNITS	% OF TOTAL UNITS
<b>RESIDENTIAL</b>				
LDR (Residential)	496.96	27.25%	2,110	44.79%
MDR (Residential)	46.57	2.56%	488	10.36%
HDR (Residential)	112.22	6.16%	2,113	44.85%
<b>Subtotal</b>	<b>655.75</b>	<b>35.97%</b>	<b>4,711</b>	<b>100.00%</b>
<b>SERVICE AND EMPLOYMENT</b>				
NC (Neighborhood Commercial)	2.23	0.12%	0	0.00%
CC (Commercial)	137.35	7.53%	0	0.00%
RC (Regional Commercial)	199.57	10.94%	0	0.00%
BP (Business Professional)	59.99	3.29%	0	0.00%
BP (Business Professional/Commercial)	101.55	5.57%	0	0.00%
LI (Light Industrial)	46.54	2.56%	0	0.00%
LI (Light Industrial / Reserve)	21.25	1.17%	0	0.00%
IND (Industrial)	9.84	0.54%	0	0.00%
<b>Subtotal</b>		<b>31.70%</b>		<b>0.00%</b>
<b>OPEN SPACE AND PUBLIC</b>				
OS (Lower Watershed)	120.96	6.63%	0	0.00%
OS (Open Space / Recreation)	38.41	2.11%	0	0.00%
OS (Slope Easement)	18.59	1.02%	0	0.00%
OS (Wetland Preserve)	4.90	0.27%	0	0.00%
PR (Golf Course)	0.88	0.05%	0	0.00%
PR (Park)	50.06	2.75%	0	0.00%
PR (Park / Preserve)	48.74	2.68%	0	0.00%
PQP (Corp Yard)	1.48	0.08%	0	0.00%
PQP (Substation)	1.18	0.6%	0	0.00%
PQP (Elementary School)	15.98	0.88%	0	0.00%
PQP (Middle School)	18.46	1.01%	0	0.00%
PQP (Daycare)	1.04	0.06%	0	0.00%
PQP (Fire Station)	2.97	0.16%	0	0.00%
PQP (PG & E)	33.72	1.85%	0	0.00%
ROW (Right of Way)	232.04	12.72%	0	0.00%
<b>Subtotal</b>				
<b>Total</b>	<b>1,823.48</b>	<b>100.00%</b>	<b>4,711</b>	<b>100.00%</b>

North Central Roseville Land Use Summary

Last Updated December 10, 2014



# NORTH CENTRAL LAND USE PLAN

## Figure 2-4

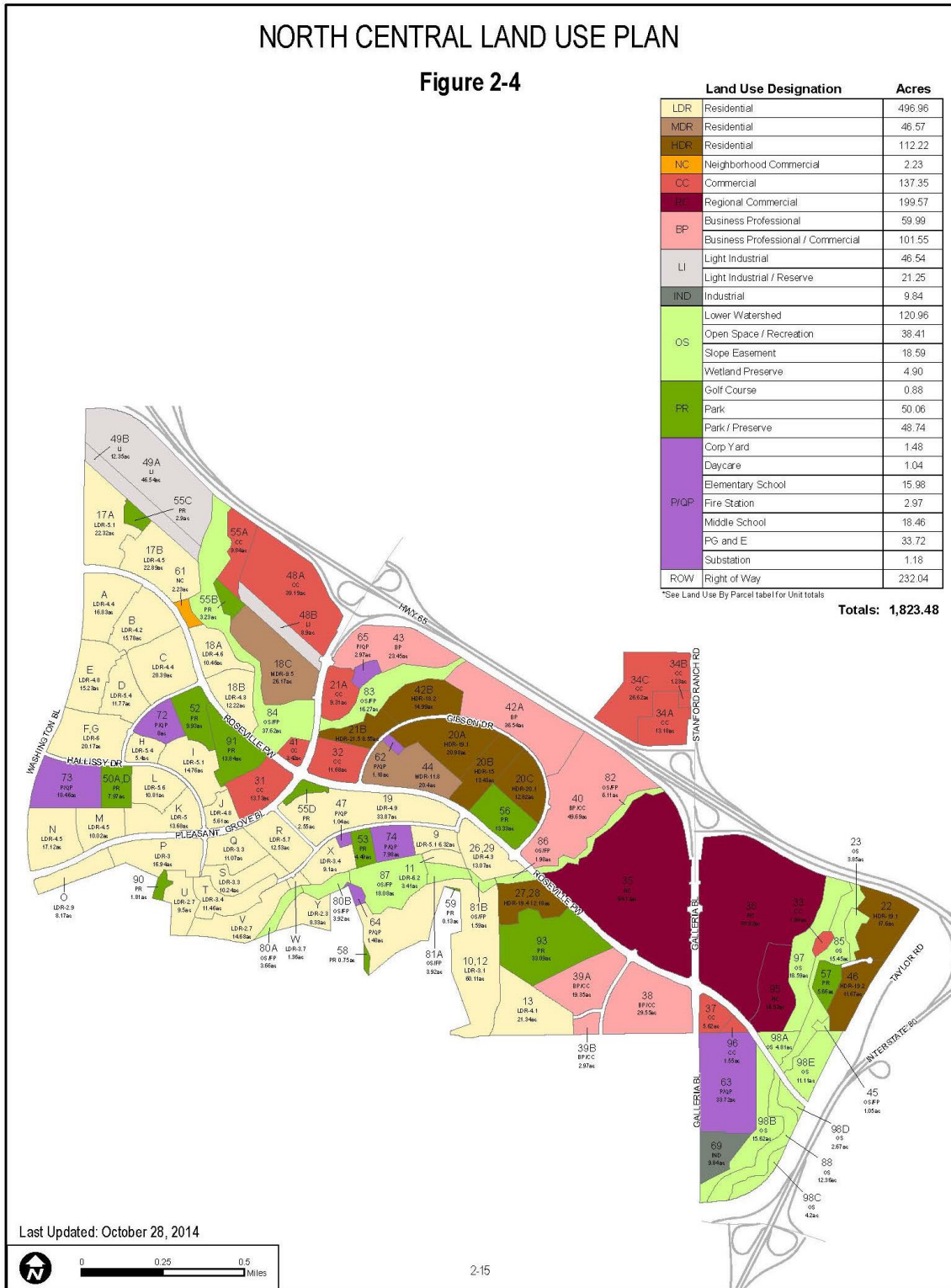


Figure 2-4: North Central Land Use Plan

**Table 2-2  
North Central Land Use by Parcel Number**

Parcel	General Plan Land Use (Specific Plan Land Use)	Zoning	Acres	Allocated Units	Finaled Units	Available Units	Density
A	LDR (Residential)	R1/DS	16.83	79	74	0	4.7
B	LDR (Residential)	R1/DS	15.78	73	67	0	4.6
C	LDR (Residential)	R1/DS	20.39	94	90	0	4.6
D	LDR (Residential)	R1/DS	11.77	63	63	0	5.4
E	LDR (Residential)	R1/DS	15.23	73	73	0	4.8
F, G	LDR (Residential)	RS/DS	20.17	131	121	0	6.5
H	LDR (Residential)	RS/DS	5.40	29	29	0	5.4
I	LDR (Residential)	RS/DS	14.76	75	75	0	5.1
J	LDR (Residential)	RS/DS	5.61	27	27	0	4.8
K	LDR (Residential)	RS/DS	13.68	68	68	0	5.0
L	LDR (Residential)	RS/DS	10.81	61	61	0	5.6
M	LDR (Residential)	R1/DS	10.02	45	45	0	4.5
N	LDR (Residential)	R1/DS	17.12	78	77	0	4.6
O	LDR (Residential)	R1/DS	8.17	24	24	0	2.9
P	LDR (Residential)	R1/DS	16.94	50	50	0	3.0
Q	LDR (Residential)	R1/DS	11.07	36	36	0	3.3
R	LDR (Residential)	RS/DS	12.53	71	71	0	5.7
S	LDR (Residential)	R1/DS	10.24	34	34	0	3.3
T	LDR (Residential)	R1/DS	11.46	39	39	0	3.4
U	LDR (Residential)	R1/DS	9.50	26	26	0	2.7
V	LDR (Residential)	R1/DS	14.68	40	40	0	2.7
W	LDR (Residential)	R1/DS	1.36	5	5	0	3.7
X	LDR (Residential)	R1/DS	9.10	31	31	0	3.4
Y	LDR (Residential)	R1/DS	8.33	19	19	0	2.3
9	LDR (Residential)	RS	6.32	38	32	0	6.0
10, 12	LDR (Residential)	R1/DS-NC	60.11	185	185	0	3.1
11	LDR (Residential)	RS	3.41	21	21	0	6.2
13	LDR (Residential)	RS/DS	21.34	90	88	0	4.2
17A	LDR (Residential)	RS	22.32	119	113	0	5.3
17B	LDR (Residential)	RS	22.89	106	104	0	4.6
18A	LDR (Residential)	RS	10.46	48	48	0	4.6
18B	LDR (Residential)	RS	12.22	53	53	0	4.3
18C	MDR (Residential)	R3	26.17	248	248	0	9.5
19	LDR (Residential)	RS	33.87	165	165	0	4.9
20A	HDR (Residential)	R3	20.98	602	400	0	28.7
20B	HDR (Residential)	R3	13.43	202	202	0	15.0
20C	HDR (Residential)	R3	12.82	258	258	0	20.1
21A	CC (Commercial)	CC	9.31				
21B	HDR (Residential)	R3	8.55	149	184	0	17.4
22	HDR (Residential)	R3	17.60	340	336		19.3
23	OS (Wetland Preserve)	OS	3.85				
26,29	LDR (Residential)	R1/DS	10.07	56	56	0	4.3
27,28	HDR (Residential)	R3/DS-NC	12.18	246	236	0	20.2
31	CC (Commercial)	CC/SA-NC	13.73				
32	CC (Commercial)	CC/SA-NC	11.68				

Parcel	General Plan Land Use (Specific Plan Land Use)	Zoning	Acres	Allocated Units	Finalized Units	Available Units	Density
33	CC (Commercial)	CC/SA-NC	1.98				
34A	CC (Commercial)	GC/SA-NC	13.18				
34B	CC (Commercial)	GC/SA-NC	1.23				
34C	CC (Commercial)	GC/SA-NC	26.62				
35	RC (Regional Commercial)	RC/SA-NC	94.13				
36	RC (Regional Commercial)	RC/SA-NC	88.92				
37	CC (Commercial)	CC/SA-NC	5.62				
38	BP (Business Professional /Commercial)	CC/SA-NC	29.55				
39A	BP (Business Professional /Commercial)	CC/SA-NC	19.35				
39B	BP (Business Professional /Commercial)	CC/SA-NC	2.97				
40	BP (Business Professional /Commercial)	CC/SA-NC	49.69				
41	CC (Commercial)	CC/SA-NC	3.42				
42A	BP (Business Professional)	BP/SA-NC	36.54	0	0		0.0
42B	HDR (Residential)	R3	14.99	300	273	0	20.0
43	BP (Business Professional)	BP/SA-NC	23.45	0	0		0.0
44	MDR (Residential)	R3/DS/SA- NC	20.40	240	0	0	11.8
45	OS (Wetland Preserve)	OS	1.05	0	0		0.0
46	HDR (Residential)	R3/SA-NC	11.67	224	0	0	19.2
47	P/QP (Daycare)	P/QP/SA- NC	1.04				
48A	CC (Commercial)	CC	39.19				
48B	LI (Light Industrial /Reserve)	M1/SA-NC	8.9				
49A	LI (Light Industrial)	PD/SA-NC	46.54				
49B	LI (Light Industrial)	M1/SA-NC	12.35				
50A,D	PR (Park)	PR	7.97				
52	PR (Park)	PR	9.93				
53	PR (Park)	PR	4.49				
55A	CC (Commercial)	CC	9.84				
55B	PR (Park)	PR	3.23				
55C	PR (Park)	PR	2.90				
55D	PR (Park)	PR	2.55				
56	PR (Park)	PR	13.33				
57	PR (Park)	PR	5.66				
58	PR (Golf Course)	PR	0.75				
59	PR (Golf Course)	PR	0.13				
61	NC (Neighborhood Commercial)	NC/SA-NC	2.23				
62	P/QP (Substation)	P/QP	1.18				
63	P/QP (PG and E)	M2	22.72				
64	P/QP (Corp Yard)	PR	1.48				
65	P/QP (Fire Station)	P/QP	2.97				
69	IND (Industrial)	M2	9.84				
72	P/QP (Elementary School)	P/QP	8.00				
73	P/QP (Middle School)	P/QP	18.46				

Parcel	General Plan Land Use (Specific Plan Land Use)	Zoning	Acres	Allocated Units	Finalized Units	Available Units	Density
74	P/QP (Elementary School)	P/QP	7.98				
80A	OS (Lower Watershed)	OS	3.66				
80B	OS (Lower Watershed)	OS	3.92				
81A	OS (Lower Watershed)	OS	3.92				
81B	OS (Lower Watershed)	OS	1.59				
82	OS (Lower Watershed)	OS	6.11				
83	OS (Lower Watershed)	OS	16.27				
84	OS (Lower Watershed)	OS	37.62				
85	OS (Lower Watershed)	OS	15.45				
86	OS (Lower Watershed)	OS	1.98				
87	OS (Lower Watershed)	OS	18.08				
88	OS (Lower Watershed)	OS/FW	12.36				
90	PR (Park / Preserve)	PR	1.81				
91	PR (Park / Preserve)	OS	13.84				
93	PR (Park / Preserve)	OS	33.09				
95	RC (Regional Commercial)	RC/SA-NC	16.52				
96	CC (Commercial)	CC/SA-NC	1.55				
97	OS (Slope Easement)	OS/SA-NC	18.59				
98A	OS (Open Space /Recreation)	M2	4.81				
98B	OS (Open Space /Recreation)	M2	15.62				
98C	OS (Open Space /Recreation)	M2	4.20				
98D	OS (Open Space /Recreation)	OS	2.67				
98E	OS (Open Space /Recreation)	OS	11.11				
ROW	ROW (Right of Way)	ROW	232.01				
<b>Total</b>			<b>1,823.48</b>	<b>4,711</b>	<b>4,247</b>	<b>0</b>	

Last Updated: December 10, 2014

## 3. Community Form and Design Element

### 3.1 Purpose and Objectives

The Community Form and Design Element establishes standards and guidelines to serve as an evaluative aid to project developers, design professionals, City staff, the Project Review Commission, Planning Commission and City Council in the review and design of individual developments within the North Central Plan Area. All development projects will require design review by the City. The intent of this section is to establish standards and general guidelines which ensure consistent quality while supporting flexibility and the ability to provide unique solutions which may fulfill the intended design.

The Community Form and Design Element has been organized into two primary sections. The first section, General Design Policies, focuses on elements which are common to several different land uses. The second section, Use Specific Design Policies, is divided by land use type and concentrates on the specific detail unique to each use.

The Community Design Policies presented in this element relate directly to the land use concepts discussed in the Land Use Element, Section 2. The Land Use Element and the Community Form and Design Element are inter-related; however, the emphasis in the Land Use Element is oriented to the relationship of land uses to one another and to the design of land use as a distinct pattern. The emphasis in the Community Form and Design Element is on the specific details of individual land uses. This section also overlaps with the Plan policies relating to woodland preservation, erosion control, and the use of open space presented in the Resources Management Element, Section 6.

Design is the fabric that binds together the physical elements of the built community, both the man-made environment and the natural environment. The Community Form and Design Element provides a sense of continuity and identity throughout the Plan Area by addressing the common features of many of the proposed land uses. The goal is to create a consistent level of quality within development throughout the North Central Specific Plan Area.

The objectives of this Community Form and Design Element are to achieve:

1. An urban core area that provides superior regional automobile access to those driving to or from the Plan Area, as well as pedestrian circulation that ties the separate uses together and encourages people to walk rather than use their automobiles within the Plan Area.
2. A built environment that minimizes disruption of existing natural features, and to the extent practicable, blends with and takes advantage of the natural topography.
3. Integration of the built-environment and open space to enhance living and working spaces.
4. Maximum potential for energy conservation through building and landscape designs and orientations which recognize the climatic conditions in the area.
5. High quality and aesthetically superior development that relates architectural style, landscaping, signage, lighting, circulation and street furniture to produce environments that are pleasing in form, scale, texture, color and variety.
6. Safety and convenience for all residents, employees, and visitors in the Plan Area.
7. Building densities and designs which reflect the more intense urban character in the east and north portions of the North Central Specific Plan Area, and the less intense suburban character in the south and west portions of the Plan Area.

8. Building densities and designs that provide a transition between the more intensive land uses and the residential neighborhoods in the Plan Area.
9. Interfaces between various uses which provide a visual break, yet allow ample opportunity for pedestrian cross access.

## SECTION 1: GENERAL DESIGN POLICIES

### 3.2 General Design Policies

The General Design Policies focus on the themes and design features typical in many of the land uses found throughout the Plan Area. Policies are included which detail the treatment of common elements or issues found in a number of different land use types. A primary focus is on the interface between individual uses that will provide for pedestrian access throughout the Plan Area. These design features include:

- project site design
- pedestrian circulation
- architecture
- landscaping
- screening/fencing
- lighting
- signage
- street furniture
- artworks

The following design guidelines are generally applicable to all land uses within the Plan Area:

#### 3.2.1 Project Site Design

1. Each increment of a phased project shall be designed to be complete in its function, circulation, drainage, infrastructure, landscaping, and visual aspects.
2. Parking should be located to the rear or side of buildings where practical.
3. Non-residential uses shall have a comprehensive parking plan designed to maximize shared parking facilities, establish efficient circulation, promote the visual quality of the site, and accommodate pedestrian circulation. Angled parking with one-way circulation is to be utilized whenever feasible.
4. Non-residential land use projects adjacent to residential land use may be subject to performance conditions as part of the project review process. Such standards may include, but are not limited to, noise generation, type of use, and hours of operation.
5. Non-residential buildings shall be set back a sufficient distance and be designed to minimize visual impacts on adjacent residential units to the extent practicable. The setback will vary depending upon building height and bulk, type of use, and topography. Non-residential land use adjacent to single family residences shall provide a minimum setback of thirty-five feet (35') for single story buildings and fifty feet (50') for two story buildings. Trash enclosures, noise generating equipment, and other nuisance shall be located away from adjacent residential units. The sole exceptions shall be the neighborhood day car center (Parcel 47) which shall be set back a minimum of twenty feet (20') from the adjacent single-family use, and buildings on Parcel 61,

which shall be set back a minimum of twenty-five feet (25') from the north and south property lines (inclusive of easements).

6. Buildings shall be sited with regard to the physical features of each project parcel and adjacent parcels. Such features shall be considered as primary design determinants.
7. Buildings, roadways, and other improvements shall conform with the natural topography and minimize grading where feasible. Finished slopes should taper to blend with existing on-site topography and contours on adjacent sites and roadways. Excessive cuts and fills shall be avoided.
8. Projects adjacent to open space areas and corridors should incorporate such corridors into project design.
9. Site design and architecture shall consider solar access, wind protection, shade, and seasonal considerations, to enhance the quality of outdoor space.
10. All multi-family and non-residential uses shall be landscaped such that within a period of 15 years of planting, a tree canopy will shade no less than fifty percent (50%) of the parking area. The shading requirements shall be as measured at noon on August 15.

### 3.2.2 Pedestrian Circulation

A primary focus of the North Central Specific Plan is site design features that will provide for pedestrian access throughout the Plan Area. Specific connections between land uses are detailed in the Use Specific Design Guidelines. General policies that are applicable to all land use include the following:

1. Land uses shall be easily accessible by public transportation, pedestrian, and bicycle routes.
2. All land uses shall be designed to facilitate pedestrian cross-connections to adjacent uses and access to the area-wide trail system.
3. Pedestrian walkways shall provide access from sidewalks into projects separate from major vehicular driveways and circulation. . Connections between private and public pathways shall be the responsibility of the project developer.
4. Bike racks and lockers for employees are encouraged within industrial, commercial, and business-professional land uses to promote walking and cycling to work. Bike parking shall be provided in highly visible areas.
5. To ensure pedestrian access within the community, clear connections to the pedestrian pathways shall be provided at regular intervals in the landscape corridors, along the adjacent roadways, and open space corridors. Pedestrian pathway access connections shall occur along the perimeter of a project at an average interval of every 600 linear feet, with the distance between individual access points not to exceed 1000 linear feet along the perimeter of a project. In residential areas, a neighborhood street intersecting a collector street may be counted as an access connection for the purposes of meeting the above requirement. The location of pedestrian access openings shall be coordinated with the location of transit stops to facilitate the use of public transit.

Additional policies are set forth in the Use Specific Design Guidelines and the Circulation Element of this Specific Plan.

### 3.2.3 General Architectural Guidelines (Building Form and Style)

It is not possible to anticipate all the architectural themes that may be applied in the Plan Area. Architecture is a subjective matter, and design standards that specify the use of standardized materials

or forms over the entire Plan Area are not appropriate. However, it is desirable to ensure consistent architectural treatment within individual projects or complexes, and to create a visual flow between separate projects throughout the Plan Area so that incompatible architectural styles and materials are not placed immediately adjacent to one another. The North Central Plan Area provides intensive land uses that are likely to include large buildings. These structures will be gateway landmarks for the City and for travelers along I-80 and Highway 65. The climatic and topographic conditions, in the Plan Area, suggest that adherence to basic design guidelines is in order for all buildings within the Plan Area. These guidelines include:

1. The architecture of buildings visible from Highway 65, Interstate 80, Roseville Parkway, Pleasant Grove Boulevard, and Harding Boulevard shall be distinctive in form, lighting, and detailing to establish a strong identity for these primary entries and regional routes into the City.
2. Building height shall relate to the building location within the Specific Plan Area according to the following categories:
  - Buildings adjacent to major arterial streets and highways, or part of a corporate park, may be of any height acceptable to the City pursuant to design review. The intent is to provide buildings of notable architectural scale and quality that will be landmarks for the community.
  - Neighborhood commercial, professional, and other non-residential buildings adjacent to single family neighborhoods shall not exceed more than two (2) stories in height or 35 feet. Such structures may be limited to one story per the City design review process.
3. Pedestrian plazas with landscaping, seating, drinking fountains, and points of interest, such as water elements and art, should be incorporated in project design.
4. All exterior architectural materials shall be selected in accordance with the seasonal variation in temperatures and precipitation that occur in the area.
5. Architectural treatment, detail, and materials shall be consistent among the buildings in multi-family and non-residential projects, but should create visual interest through orientation, form, and alignment of individual buildings.
6. The architectural detail and treatment of adjacent development should be considered in the design of projects to create a visual flow and prevent incompatible architectural styles from being placed adjacent to each other.
7. Parapet firewalls, when required for building separation, shall be treated as an integral part of building design and should not appear as unrelated visual elements.
8. Where the rear or side of a non-residential building is visible from a public thoroughfare or public space, such elevations shall be treated with the same materials, colors, and details as the primary frontage.
9. All primary building and project entries shall be well-defined by varied textures, forms, materials, colors, and landscaping, to afford a sense of entry.
10. Trademark buildings dictated by chain or franchise businesses are generally discouraged. The architectural design of such businesses shall be consistent with the overall project design.
11. Untextured, untreated concrete slab tilt-up buildings lacking detail and appreciable architectural style and form are prohibited.
12. All accessory structures shall be compatible in material, color, and texture with the main buildings.

13. Showers, lockers, and bike racks may be required in all non-residential land uses, as an option to satisfy the requirements of the City's Rideshare Ordinance.

### 3.2.4 Streetscape Landscape Guidelines

The North Central Roseville Specific Plan Area presents an unusual challenge in landscaping, due to the extent of the Mehrten formation and shallow soils. Conventional landscaping approaches may need to be augmented by special approaches that include over-excavation of landscape areas and importation of soil. The Plan Area, with a notable exception along Antelope Creek, has few trees. The long boulevards provide periodic views into the open areas of the Plan and offer an opportunity for visually interesting interfaces between the man-made environment and the natural environment.

Landscaping along the major arterial streets and the collector streets will establish a common identity and image for the North Central Specific Plan Area. The major roads will rise and fall as they follow the gently rolling terrain in the Plan Area to afford a variety of viewpoints that add interest to the road system. The major boulevards will have a one hundred foot (100') wide right-of-way with landscaped medians and fifty foot (50') landscaped corridors on both sides of the roadways. Where soundwalls are provided adjacent to single family uses on Pleasant Grove Boulevard, a landscape corridor at least thirty-five feet (35') wide is required. Washington Boulevard, along the west edge of the Plan Area, will be subject to the same standards of landscaping as the other major boulevards within the Plan Area. The landscaped corridor along major arterials is measured from the back of the curb of the ultimate road pavement width, as illustrated in the Circulation Element, Section 5. Landscape improvements for all roadways are the responsibility of the owner of the property abutting the back of curb.

The major roadways, including Highway 65, Roseville Parkway, and Pleasant Grove Boulevard, will cross the lower watershed preserve areas at a number of points throughout the Plan Area. The wetland preserve areas cannot have non-indigenous landscaping introduced into them, and it is important to maintain the primary view sheds throughout the Plan Area. At these locations low level landscaping will be used to soften the short range, foreground views. Street trees will be used to frame the open space corridor views in the preserve areas, as illustrated in Figure 3-1, View Corridors Analysis.

Landscaping the frontage and within the interchange rights-of-way for Highway 65 is an aesthetic consideration for the Plan Area. The Community Facilities District for the North Central Specific Plan Area has included funding for the landscaping of Caltrans right-of-way along Highway 65.

Landscape Design Guidelines to be adopted for the Specific Plan Area will establish specific methods for landscaping and a palette of plant materials. The following general landscaping standards are applicable to the Plan Area.

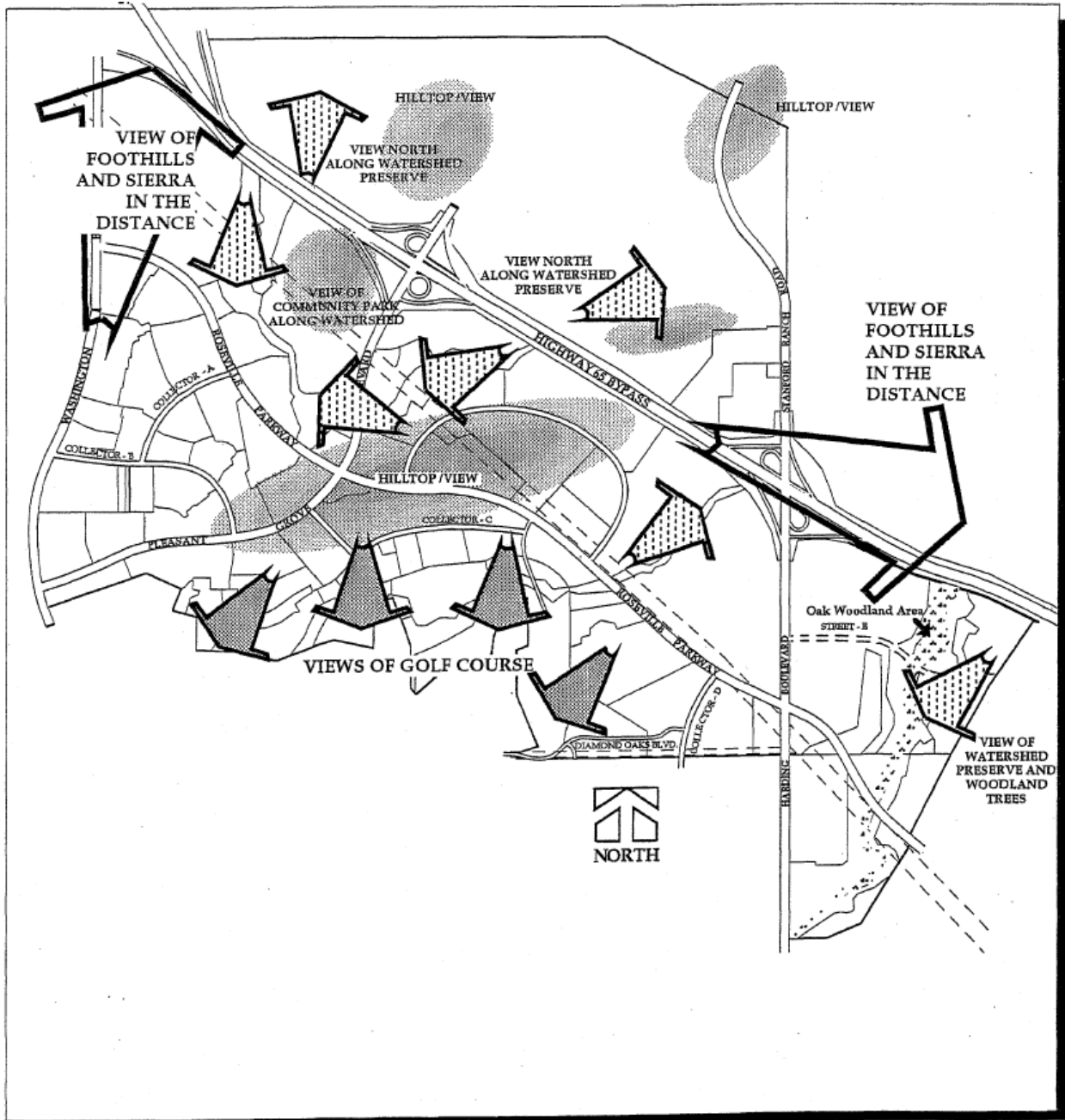


Figure 3-1: View Corridors Analysis

1. All landscaping in the North Central Plan Area shall be designed and implemented consistent with the Landscape Design Guidelines.
2. Landscaped setbacks shall be created along all arterial streets to shield private spaces and help create a sense of unity along the street and within the community. Corridor width requirements are illustrated in Figure 3-2, Landscaped Corridor Widths Along Major Arterials, and specified in the Circulation Element, Section 5.
3. Street trees shall be located along major thoroughfares to provide shade and foliage, soften the hard streetscape, and help define the public space and pedestrian scale. An example of the potential landscape treatment is illustrated in Figure 3-3, Arterial Landscape illustration. A

dominant species of tree will be specified for each roadway in the Landscape Design Guidelines to provide visual continuity and harmony.

4. Landscaping shall include undulating irregular earth berms or other design features as a method of adding visual interest and providing sufficient soil for mature plant growth. The intent is to avoid an unbroken visual plane along the roadway corridors. Such berms may not interfere with traffic visibility or drainage to natural features.
5. Local residential and residential collector streets will provide a five-foot (5') landscape strip between the back of curb and the sidewalk, as illustrated in Figure 5-6 in the Circulation Element. Street tree treatment within this strip will be consistent along any single street for visual continuity and harmony, and may include canopy street trees or other trees scaled to the pedestrian character of the street. Accent planting shall be used at neighborhood entries. The maintenance of groundcovers and shrubs within the planter strips shall be the responsibility of the adjacent property owners. Street trees will be maintained by the City through the establishment of a lighting and landscape district. Appropriate landscape materials will be specified in the Landscape Design Guidelines.
6. The continuing availability of water for landscaping is a major concern to the City of Roseville. Landscaping materials shall be selected with consideration for water requirements over the lifetime of the plants. The use of materials with low water requirements, particularly plants that are considered drought tolerant, and the use of efficient irrigation systems is strongly recommended and may be required. The Landscape Design Guidelines shall address the long-term water demand for all plant materials and shall indicate the means of conserving water in the recommended plantings and irrigation.
7. All public uses shall be subject to the same landscape and design standards as adjacent private uses.

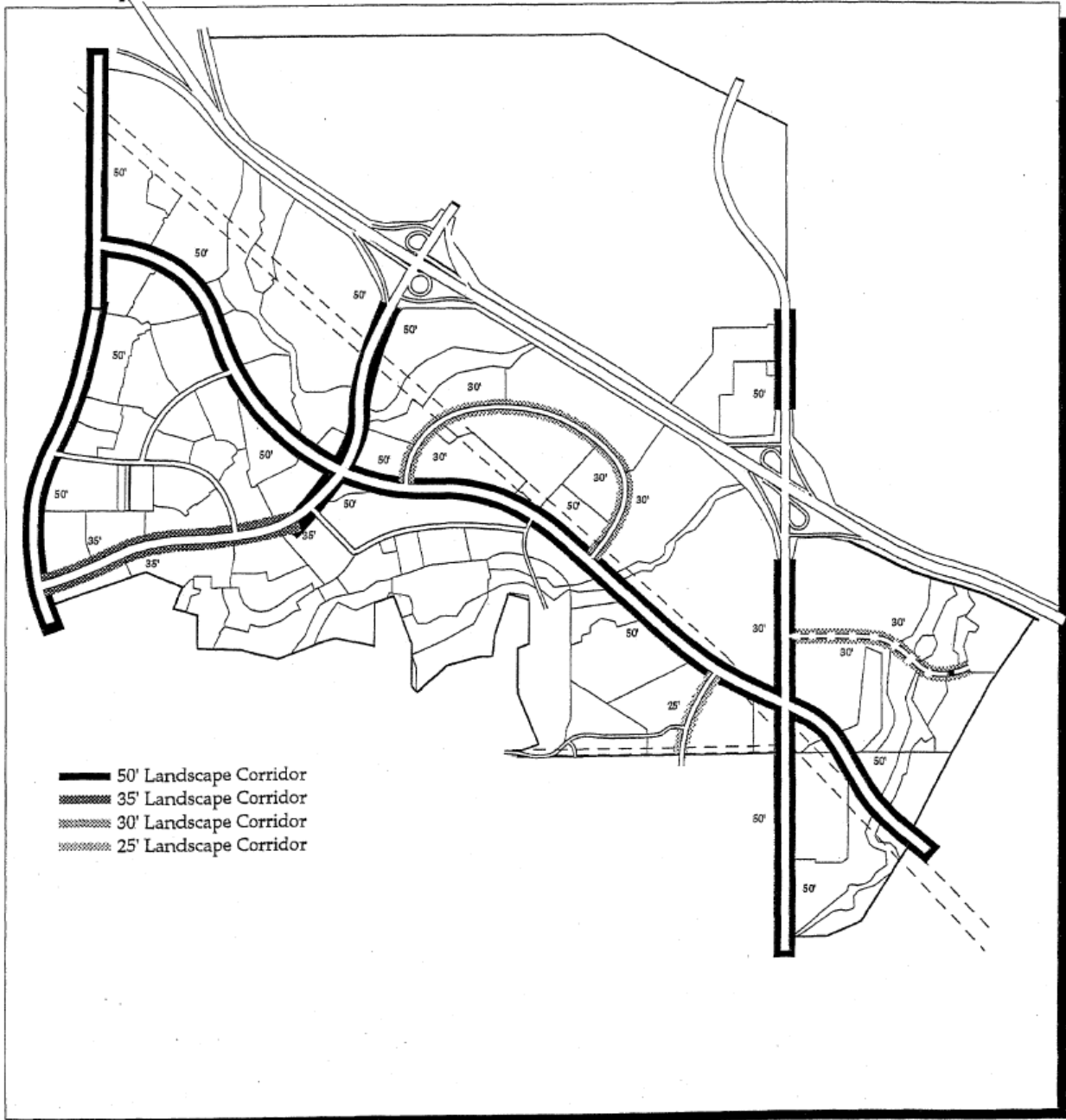
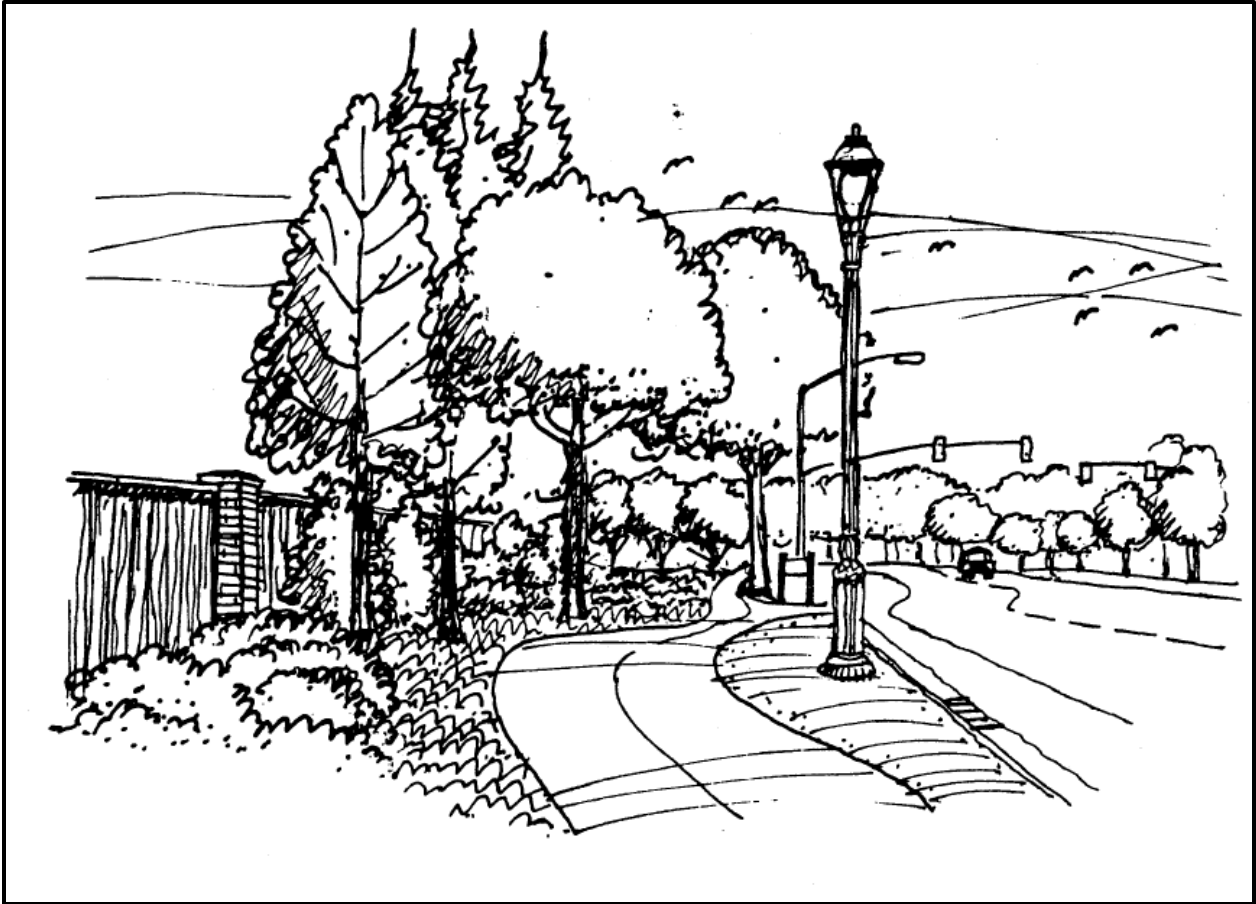


Figure 3-2: Landscaped Corridor Widths Along Major Arterials



*Figure 3-3: Arterial Landscape Illustration*

### 3.2.5 Landscaping Adjacent to Natural Open Space Areas

Designated open space areas in the Specific Plan occur in the wetland/park preserve areas, lower watershed wetlands preserve areas, and in the riparian area along Antelope Creek. The natural landscape is relatively sparse in the vernal pool and lower watershed areas. However, the wetland areas will remain as a network of permanent open space which provides an excellent opportunity to blend the natural landscape with the urban landscape. In most instances the open space corridors contain areas of environmental sensitivity which cannot be modified through formal landscaping.

The objective of the open space landscape guidelines is to ensure the environmental integrity of the natural habitat, provide the continuity of view corridors through the Plan Area, and provide a subtle transition between natural and created environments. The landscaping of areas adjacent to open space is addressed in detail in the Landscape Design Guidelines. The following general policies apply to landscaping adjacent to natural open space areas:

1. Formal landscaping adjacent to the natural open space areas, including oak woodlands and wetland preserves, will require erosion and water quality control techniques to avoid run-off into the open space area. Of particular concern for wetland preservation areas is run-off carrying herbicides, pesticides, fertilizers, and eroded soils. Such control measures need to be integrated with the overall landscape design for the proposed project. The City may require documentation of such measures as part of the design review process. The specific approaches

recommended to minimize siltation and erosion problems and to maintain the quality of water flowing into the natural areas are described in Sections 6.3 and 6.4 of the Resource Management Element, the use specific guidelines found in this section, and the Landscape Design Guidelines. Drainage constraint areas are shown on Figure 7-13, Proposed Drainage Basin Sites.

2. Drainage from adjacent land uses discharging into lower watershed wetland areas and perennial streams will be limited to selected areas as determined by the more restrictive of any U.S. Army Corps of Engineers 404 permit and/ or City of Roseville review process.
3. Where feasible, the formal landscaping of adjacent land uses should provide a visual transition to the informal landscape character of the open space area. Landscaping adjacent to wetland preserves shall be typified by low shrubs and groundcover, with the exception of trees compatible with the street trees used in the adjacent streets or adjacent private landscaping. Landscaping adjacent to open space along Antelope Creek shall blend with the oak woodland by incorporating oaks and complementary species. A discussion of the potential landscape materials is presented in the Landscape Design Guidelines.
4. Landscape materials within planting areas adjacent to the wetland preserve areas shall be non-invasive species compatible with the natural habitat of the preserve areas.
5. Earth berms and/ or swales may be used to separate and delineate the natural open space from the formal urban landscaping in order to maintain the security and privacy of the adjacent land use. Visual and pedestrian connections between the use and the open space should be maintained where feasible and as mandated in the Use Specific Guidelines.
6. The project developer, owners' associations and City shall take whatever steps are necessary to prohibit the disposal of lawn clippings, rubbish, or any other foreign material in open space areas. A landscape and lighting district will be established to maintain all open space areas.

Additional guidelines for the protection of sensitive plant habitats and woodland areas are set forth in Section 6, Resources Management Element.

### 3.2.6 Screening and Fencing Guidelines

Walls and fencing within the Plan Area are intended to screen facilities, and to provide sound barriers, privacy, and security. To a significantly lesser extent they may be utilized to buffer land use boundaries. Policies relating to the interface between uses are addressed in the appropriate use specific guideline discussions. In general, the walls are to be kept to a minimum to avoid blocking views or creating a sense of fragmentation among the land uses in the Plan Area. The following guidelines shall apply:

1. No outside, unscreened storage is permitted. Loading, service, equipment, and trash enclosure areas shall be fully screened by a combination of fencing, masonry walls, grade separation, and/ or dense landscaping.
2. Mechanical equipment, satellite dishes, antennas, and other similar structure shall be ground-mounted when feasible. If not ground-mounted, such equipment shall be screened from the view of streets, adjacent properties, and areas open to the general public through the use of parapet walls, roof wells, or other means incorporated as an integral part of building design.
3. All screens, fencing, and retaining walls shall be compatible in material, color and texture with related buildings.

4. All noise generating mechanical devices shall be of low emission design, and/or located and screened with noise reduction barriers so that their potential as a nuisance to abutting properties is minimized.
5. Six foot (6') high masonry soundwalls will be provided between single family residential uses and arterial roadways. A six foot (6') high wood and pilaster fence will be provided between single family residential uses and residential Collectors A and B. Wrought iron and pilaster fencing may be substituted to preserve views if approved by the Commission. If a wrought iron fence is approved, CC&R's for the subdivision shall include language prohibiting the modification or replacement of the fence which would create inconsistent treatment along a continuous section of fence. A six foot (6') high masonry sound wall will be provided between single family residential uses and all other collector roadways. Walls/ fences shall have consistent treatment along a given roadway and will include materials and designs which are visually attractive. Wall/fence treatment will be described in detail in the Landscape Design Guidelines. Periodic breaks in the wall/fence shall be provided to facilitate pedestrian access per the use specific guidelines.
6. Walls may be utilized between single family residential uses and nonresidential uses as a buffer. Such walls shall allow pedestrian cross access as specified in the use specific guidelines. Alternatives to walls are encouraged. Such alternatives may include building orientation, setbacks, mounding, and landscaping.
7. All utility lines and connections are to be placed underground. Transformers, terminal boxes, meters, fire risers, backflow preventers, and other similar facilities shall be screened to the extent practicable, while still maintaining minimum clear areas around the equipment as required by the utilities and state law.

### 3.2.7 Lighting Guidelines

Exterior lighting within the Plan Area is intended to provide for safety and security, as well as to enhance building design and landscaping. The character of the lighting will differ from location to location within the North Central Specific Plan Area. It is intended that the intense commercial areas will be brightly lit in a manner that complements the architecture and level of activity anticipated. In residential areas the lighting will be more subdued and in character with the other residential neighborhoods in Roseville. The following lighting guidelines are designed to encourage creative use of lighting while avoiding nuisances and minimizing energy demands.

1. The dramatic and creative use of exterior lighting to enhance the architecture of non-residential buildings and grounds is strongly encouraged.
2. Project lighting shall not create glare for project occupants or neighboring properties.
3. Lighting in pedestrian areas shall be of lower scale and less intensity than in major public streets to enhance the pedestrian character. Specific design standards, including lighting intensity, fixture type and height will be as set forth in the Landscape Design Guidelines.
4. The design of exterior lighting shall, in all cases, consider the long-term energy demand of the lighting program.
5. High pressure sodium vapor lights with cutoff-style fixtures shall be used on major arterial streets, collector streets in non-residential areas, in parking areas, and along public sidewalks adjacent to arterial streets to improve energy efficiency and reduce glare impacts.

6. The style and design of lighting fixtures shall be compatible with building design and consistent within individual projects.

### 3.2.8 Signage

Signage design guidelines are incorporated in the North Central Roseville Specific Plan Sign Guidelines adopted as a Planned Sign Permit Program separately from this Specific Plan. All signs within individual projects shall be coordinated and shall be consistent with the Sign Guidelines.

### 3.2.9 Street Furniture

All street furniture including trash receptacles, benches, bus shelters, signage and lighting shall utilize a standard or complementary design theme as set forth in the Landscape Design Guidelines.

### 3.2.10 Artwork

The provision of fine artwork such as sculptures, murals, water elements, carvings, frescoes, mosaics, and mobiles is strongly encouraged. Such work should relate in terms of form and concept with the architecture and environment of the subject site. Outdoor artwork should be located to be visible to the public. Design of the artwork should be durable against vandalism and weather, and not require excessive maintenance. The artwork is to be considered a permanent asset to the property. Artwork is to be considered as part of the design review process for individual projects in accordance with applicable City ordinances.

## SECTION 2: USE SPECIFIC DESIGN POLICIES

The Use of Specific Design Guidelines set forth standards that are applicable to certain land uses and are applied in addition to the General Design Guidelines. The Use Specific Guidelines have been organized by land use category.

### 3.3 Residential Neighborhoods

The North Central Roseville Specific Plan Area is a logical location for a mix of housing types and intensities ranging from single family residential to multifamily apartments. The Land Use Map (Figure 2-4 and Table 2-1) designates the location of 4,711 dwelling units of various densities on 655.75 acres. The North Central area is transected by a number of major arterial routes with direct access to the Highway 65 Bypass. The Plan Area is immediately adjacent to the North Industrial Area and other major employment centers along Highway 65, and will be readily accessible to the new employment opportunities in the Northeast and Southeast Roseville Plan Areas.

Conventional single family detached homes, at 0 to 5 dwellings per acre; account for a total of 2,110 units or approximately forty-four percent (44.79%) of dwelling units in the Plan Area. The western southern portions of the Plan are highly suitable for single family dwellings due to the relatively gentle terrain and good quality soils for development. Detached dwelling units will provide flexibility in unit siting and design. Homes overlooking the Diamond Oaks golf course will typically be single family units in the lower density ranges.

Small lot, or attached dwelling units, in densities of 5.1 to 10 dwelling units per acre, account for a total of 488 units, or approximately eleven percent (10.36%) of the units in the Plan Area. This mid-range density contributes to a diverse housing mix and is often utilized as a transitional density. Examples of

the type of dwelling unit configurations that may be applied in the medium density residential category are illustrated in Figure 3-4, Typical Medium Density Single Family Residential Configuration.

The North Central Roseville Specific Plan also includes a substantial number of multi-family residential sites. A total of 2,113 dwelling units, or approximately forty-five percent (44.85%) of the total housing stock in the Plan Area is in this higher density category. Typically, the multi-family residential uses are to be developed at densities in the range of 14 to 20 dwelling units per acre. The large percentage of higher density housing is a reflection of the significant nearby employment base, the proximity of shops and services, the extensive circulation system and potential for light rail, the intensity of adjacent uses, and the relative difficulty of developing on the Mehrten formations. The soil conditions in the northern and eastern portions of the Plan Area will require special landscape methods. Common landscape areas typical to multi-family and attached units will be more feasible in these areas.

The allocation of residential units by density category in the North Central Specific Plan Area is summarized in Table 3-1.

**Table 3-1  
Residential Allocation by Density Category**

	Dwellings	Percent of Total	Avg. HH Size	Estimated Population
Low Density Residential (R1 to R5/conventional lots)	2,110	44.79	2.61	5,507
Medium Density Residential (R5+/- to R10/small lots)	488	10.36	2.61	1,274
High Density Residential	2,113	44.85	2.61	5,514
TOTAL:	4,711			12,296

The average density of all land designated for residential use in the Plan Area is approximately 6.0 dwelling units per acre. The ultimate residential population is estimated to be approximately 12,296 residents based on the assumed average household population of 2.61 for single family, 2.61 for medium density, and 2.61 for multi-family housing (Source: City of Roseville Planning Department).

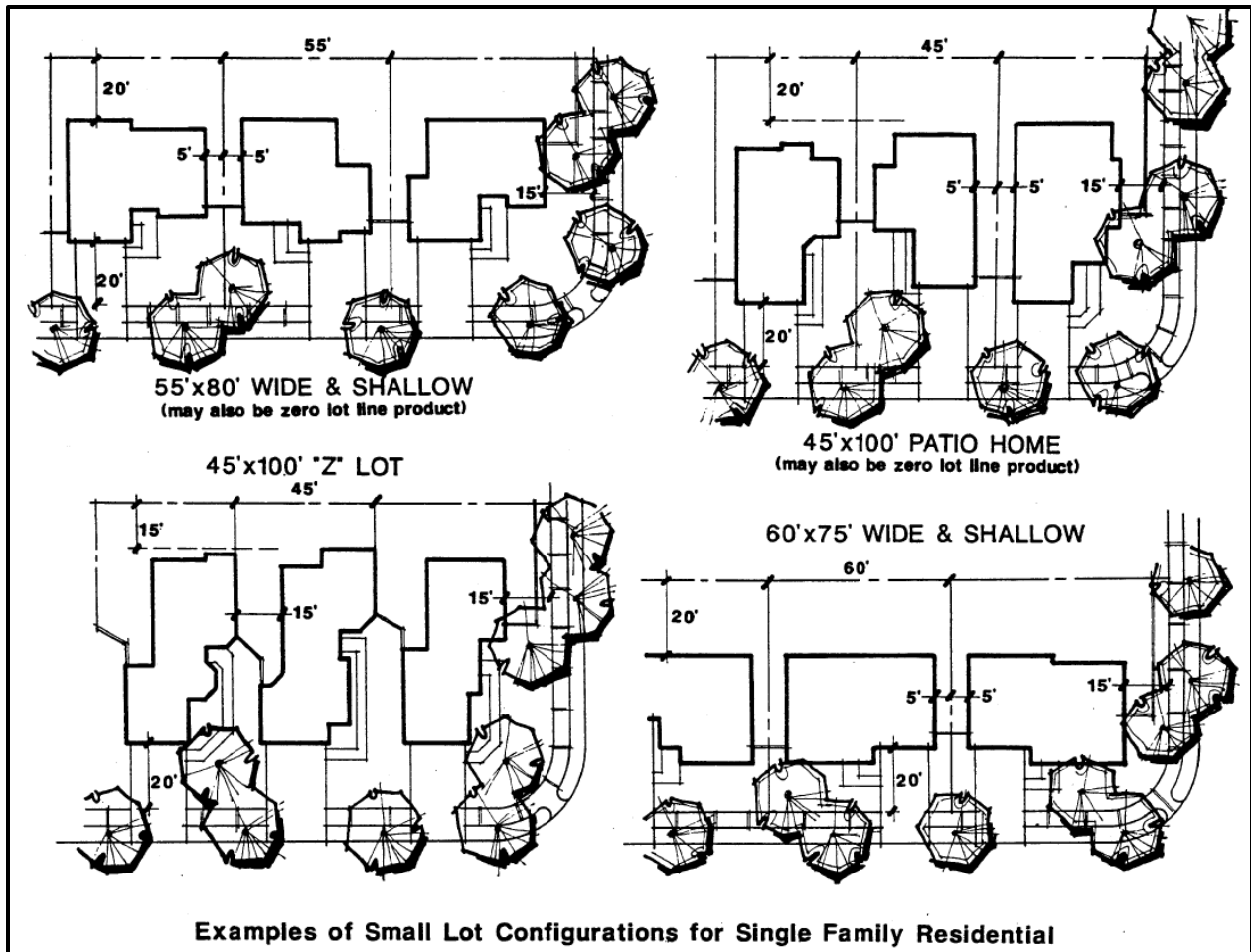


Figure 3-4: Typical Medium Density Single Family Residential Configuration

The residential land use within the Plan is organized as a series of "neighborhoods". The residential neighborhoods encompass all of the single family and mid-range density residential use in the Plan Area, as well as a portion of the multi-family housing.

The residential neighborhoods are defined and identified by the following elements:

*1. Clearly Defined Boundaries*

Boundaries are defined through a combination of collector and arterial streets, open space, and public uses such as schools and parks. Such elements create clear neighborhood edges.

*2. Limited Size*

Residential areas are typically arranged in neighborhood clusters of approximately 100 to 250 acres, depending on the configuration of the road network and the natural features. The limited size allows the opportunity to create a common identity for each neighborhood.

*3. Neighborhood Center*

Within each neighborhood, or shared with an adjacent neighborhood, combined school and park facilities are planned. These facilities are intended to provide focal points for neighborhood identity and social interaction. The schools and parks will be the centers of activity for neighborhood children and adults. The neighborhood center facilities have been located to be within one-half mile of all residents within a neighborhood, and are served by a comprehensive pedestrian and bicycle pathway system that includes both the local residential streets and a separate trail system. As a result, neighborhood center facilities will be easily accessible to adjacent residents.

There are four residential neighborhoods identified in the Specific Plan Area, as illustrated in Figure 3-5, Neighborhood 'Delineation Plan. The special characteristics and a summary of the land use for each neighborhood are presented in Figures 3-6, 3-7, 3-8, and 3-8a. Multi-family project areas may be included within a neighborhood, but are more commonly identified as a single project area oriented to non-residential land use. This orientation is consistent with the City's General Plan land use siting policies. Multi-family projects tend to develop as stand alone neighborhoods with their own recreation and open space facilities. The multi-family projects have, however, been carefully integrated into the fabric of the Plan Area and policies have been included detailing interfaces and connections with adjacent uses.

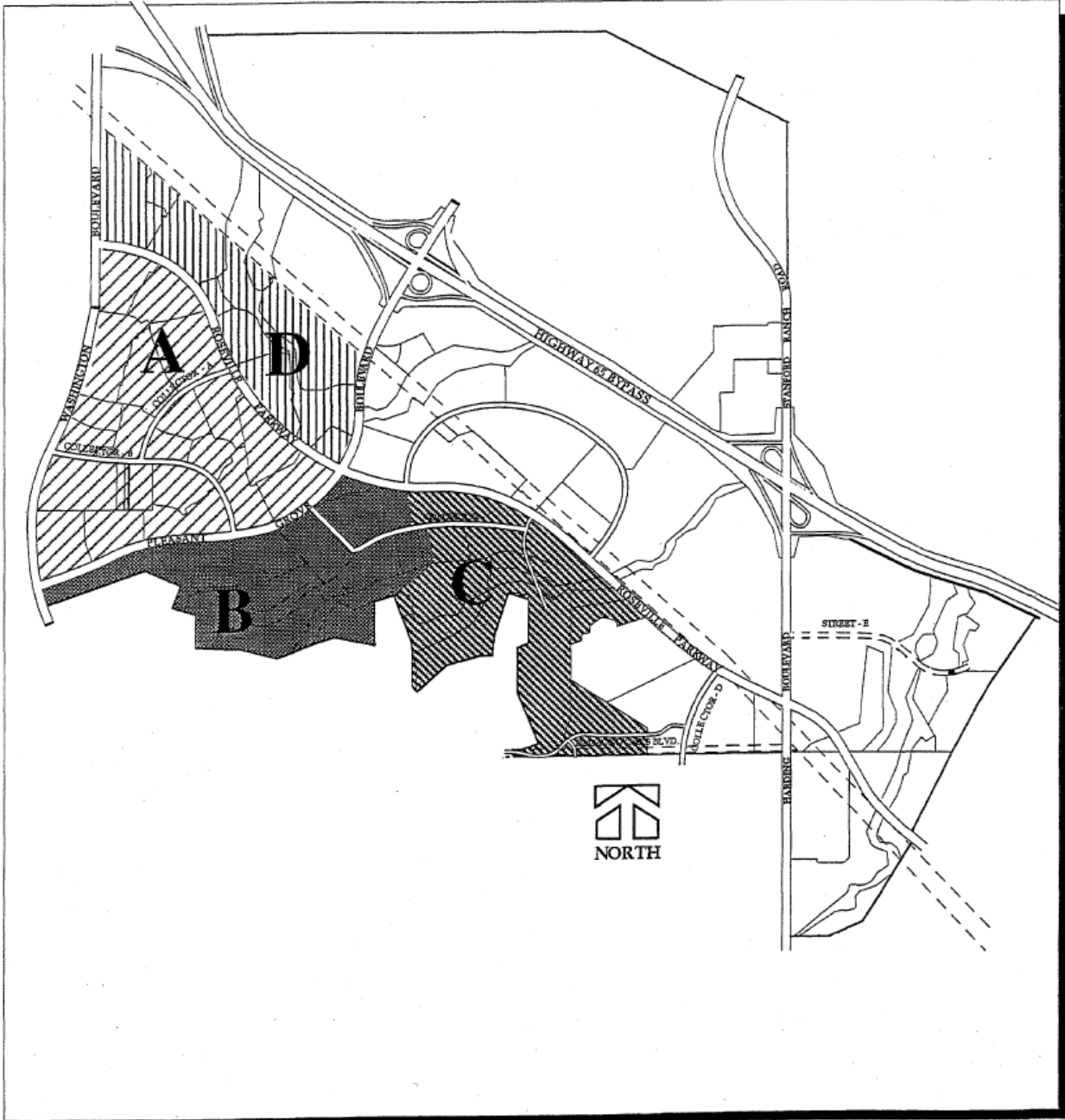


Figure 3-5: Neighborhood Delineation Plan

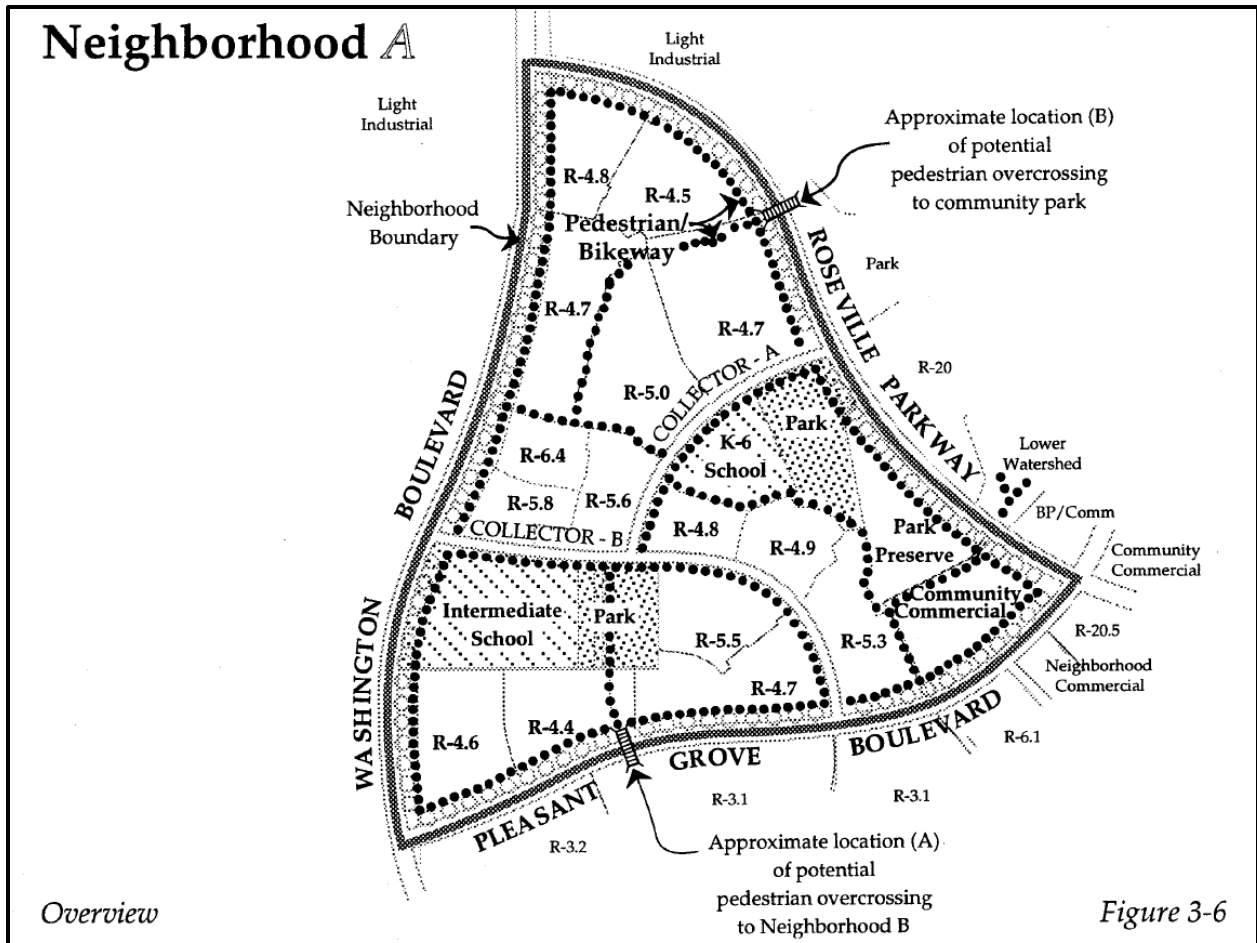


Figure 3-6: Neighborhood A

<i>Land Use</i>	<i>Acres</i>	<i>Units</i>
Low Density	108.00	449
Medium Density	73.80	444
Schools	26.42	
Park	18.06	
Park/Preserve	13.67	
Commercial	13.96	
<b>Total</b>	<b>253.91</b>	<b>893</b>

*Overview*

Neighborhood A is bounded by Washington Boulevard, Roseville Parkway, and Pleasant Grove Boulevard. The neighborhood includes low and medium density residential areas, with adjacent schools, parks, and shopping.

The residential areas are served by nearby community commercial at Pleasant Grove Boulevard and Roseville Parkway. A park/preserve separates the shopping area from the elementary school and its adjacent park.

The intermediate and elementary school sites are located adjacent to parks in the neighborhood, and are linked to nearby residential areas by the pedestrian pathway/bikeway system running throughout

the Plan. Pedestrian connections are provided from the neighborhood to the area south of Pleasant Grove Boulevard via a potential pedestrian overcrossing, and on-street or separated pathways to the neighborhood park and intermediate school.

A similar potential pedestrian overcrossing (B) connects from neighborhood A to the community park north of Roseville Parkway.

The terrain and soils within the neighborhood A will be easily developed for lower density residential use.

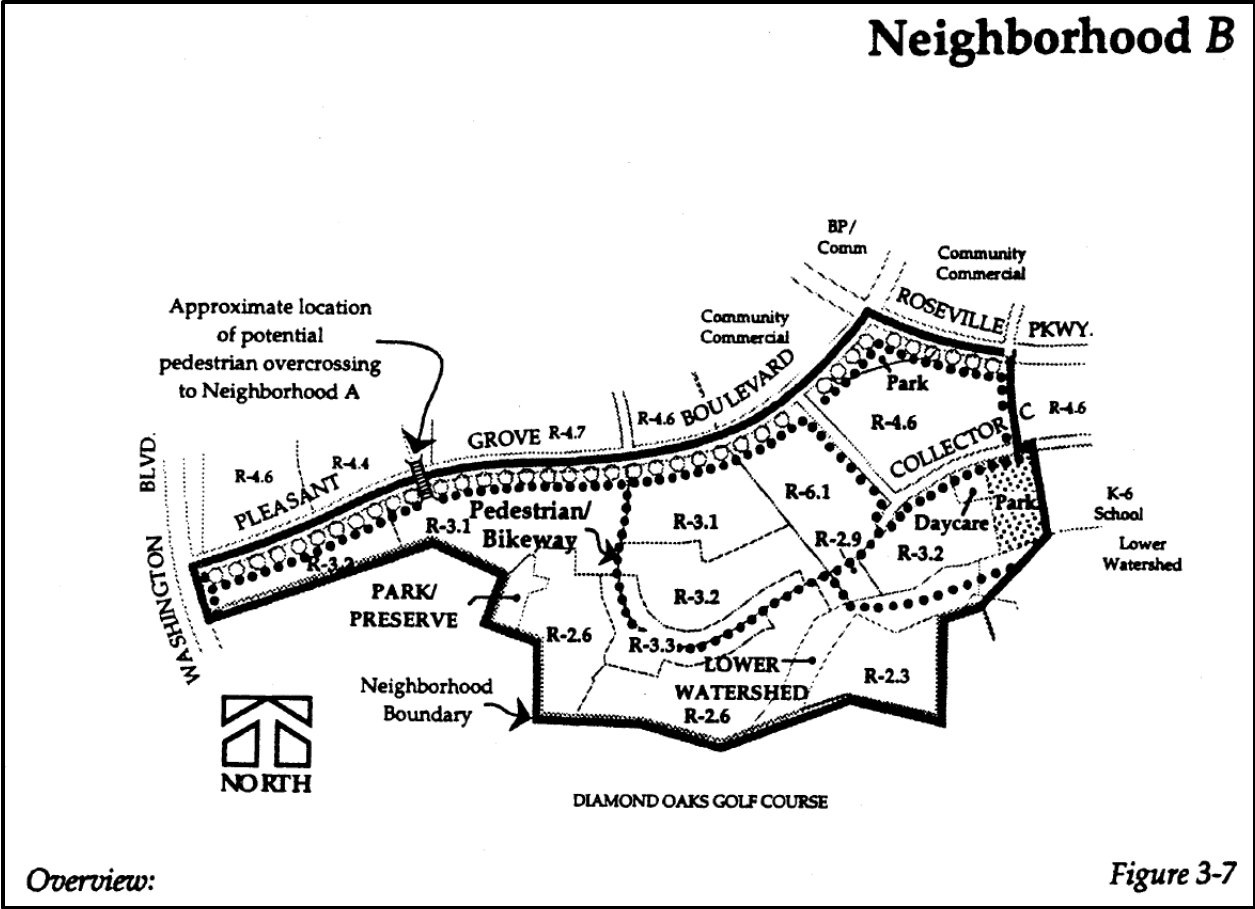


Figure 3-7: Neighborhood B

<i>Land Use</i>	<i>Acres</i>	<i>Units</i>
Low Density	122.34	399
Medium Density	12.90	79
Park	5.38	
Park/Preserve	1.81	
Day Care	1.09	
Lower Watershed	7.70	
<b>Total</b>	<b>151.22</b>	<b>478</b>

*Overview*

Neighborhood B consists of a mix of residential densities overlooking the Diamond Oaks Golf Course, with nearby parks and shopping.

The neighborhood includes a park located adjacent to the elementary school shared with neighborhood C to the east as well as a small neighborhood park located at the southeast corner of the Pleasant Grove/Roseville Parkway intersection. In addition, a small park/ preserve is located adjacent to the golf course near the western edge of the neighborhood. This park/ preserve may be improved as a pocket park with a small turf area or other improvements. In addition, a day care center is located in the neighborhood to serve residents and workers in the area. Community shopping areas are located across Pleasant Grove Boulevard and Roseville Parkway.

A pedestrian pathway /bikeway through the open space corridor and along the local streets connects the neighborhood with commercial and business-professional across Roseville Parkway. A potential pedestrian overcrossing over Pleasant Grove Boulevard connects this neighborhood with Neighborhood A. The pedestrian route connecting to the overcrossing may be on-street or a separated pathway.

# Neighborhood C

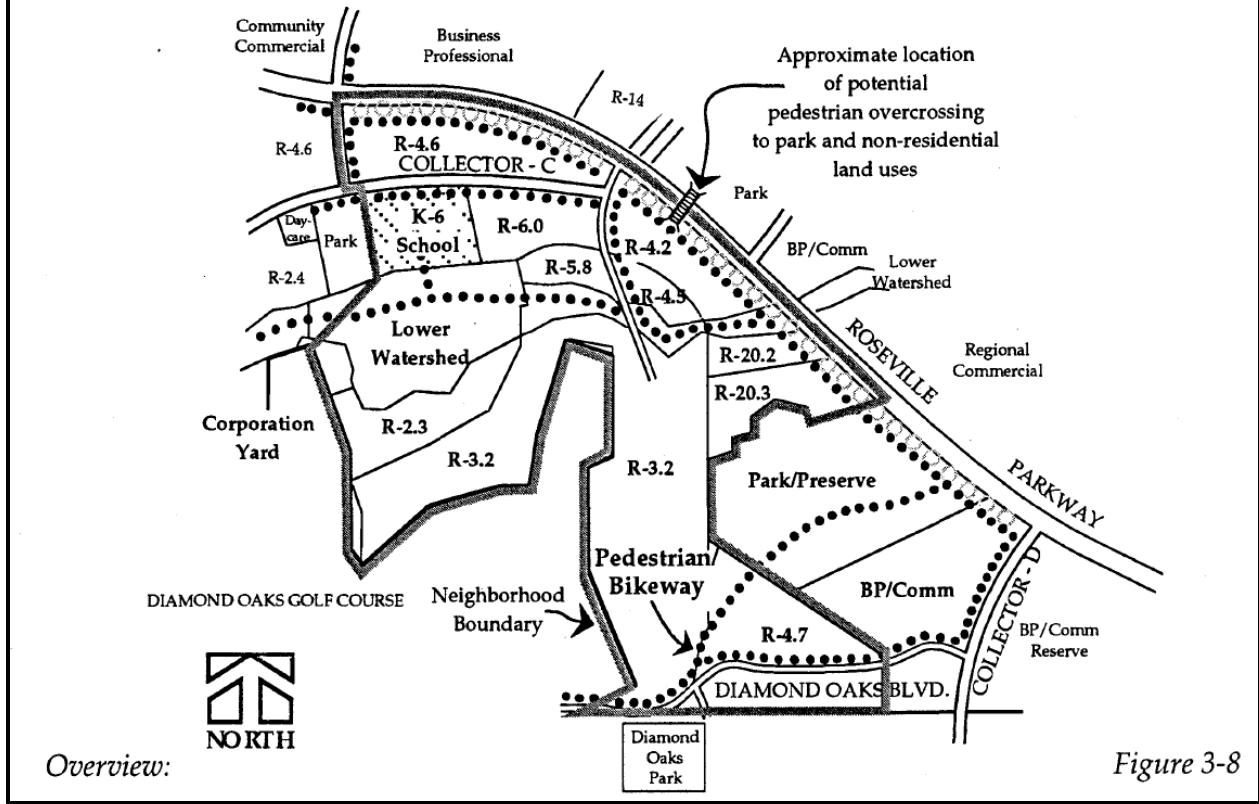


Figure 3-8: Neighborhood C

<u>Land Use</u>	<u>Acres</u>	<u>Units</u>
Low Density	94.48	345
Medium Density	9.75	58
High Density	25.46	519
School	8.00	
Lower Watershed –	18.03	
Park/Preserve		
Lower Watershed	7.44	
Golf Course	0.88	
Corporation Yard	2.46	
<b>Total</b>	<b>165.50</b>	<b>922</b>

## Overview

Neighborhood C consists of a mix of low, medium, and high density housing, a portion of which is adjacent to Diamond Oaks Municipal Golf Course. The high density housing, including elderly housing, lines a portion of the northern boundary of the neighborhood along Roseville Parkway. This high density housing acts as a transition to the more intense uses north of the Parkway. An elementary school site, park, and neighborhood day care center are shared with Neighborhood B to the west. Additional commercial services are anticipated in the BP /Commercial land use adjacent to the eastern edge of Neighborhood C.

A major element of the neighborhood is the intermittent drainage running in an east/west direction. A portion of this lower watershed includes a park/preserve and an adventure play area with nature trails. The residents will also have access to Diamond Oaks Park immediately south of the neighborhood.

A pedestrian pathway /bikeway links the neighborhood to adjacent business-professional services and the regional mall through the open space corridor. A potential overcrossing may connect Neighborhood C to the community park and other uses north of Roseville Parkway.

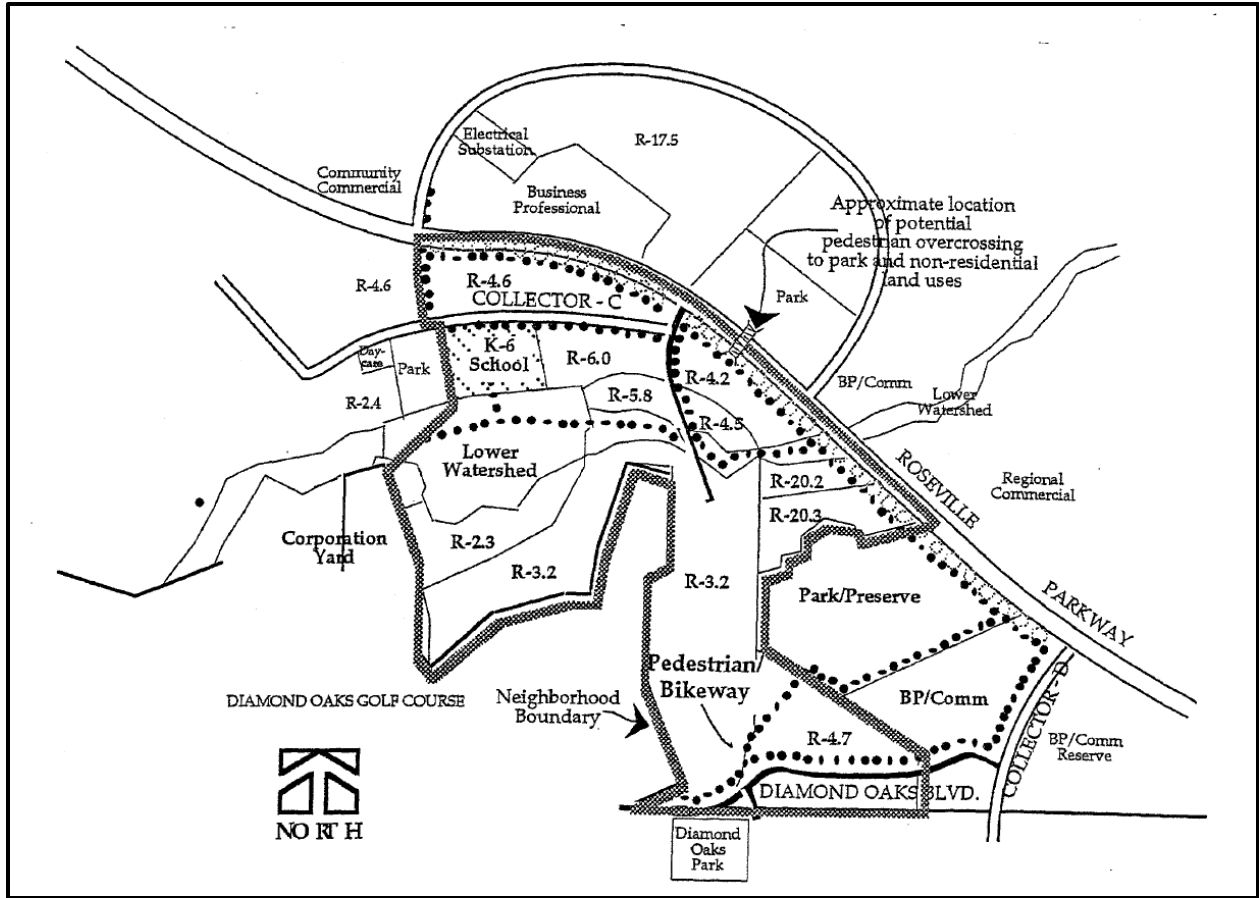


Figure 3-8a: Neighborhood D

<u>Land Use</u>	<u>Acres</u>	<u>Units</u>
Low Density	44.82	207
Medium Density	49.36	367
Neighborhood Commercial	2.28	
Park	6.11	
Lower Watershed (portion)	27.87	
Business-Professional/Commercial	3.48	
<b>Total</b>	<b>133.92</b>	<b>574</b>

*Overview*

Neighborhood D consists of predominantly LDR residential bounded by Washington Boulevard on the west, Roseville Parkway on the south, Pleasant Grove Boulevard on the east and a fifty-foot (50') wide open space/bike trail corridor on the north.

Key elements within Neighborhood D include 2 park sites intended to provide neighborhood focal points and recreation facilities for the surrounding residential parcels.

A large open space/lower watershed corridor which bisects the neighborhood is also located in Neighborhood D. This open space corridor links the various LDR parcels and will provide direct pedestrian/bike routes to nearby business-professional/commercial uses located adjacent to Roseville Parkway and Pleasant Grove Boulevard.

Neighborhood D also provides a business-professional/commercial site which encompasses 3.48 acres located at the intersection of Pleasant Grove Boulevard and Roseville Parkway, and a neighborhood commercial site of 2.28 acres between low density residential parcels south of the watershed and north of Roseville Parkway.

Residences located adjacent to the Diamond Oaks Golf Course in Neighborhoods B and C contain setback zones to mitigate the potential hazard of stray golf balls driven off the course. Figure 3-9, Golf Course Special Precaution Areas, illustrates the locations of these zones.

### 3.3.1 Single Family Village Development Guidelines

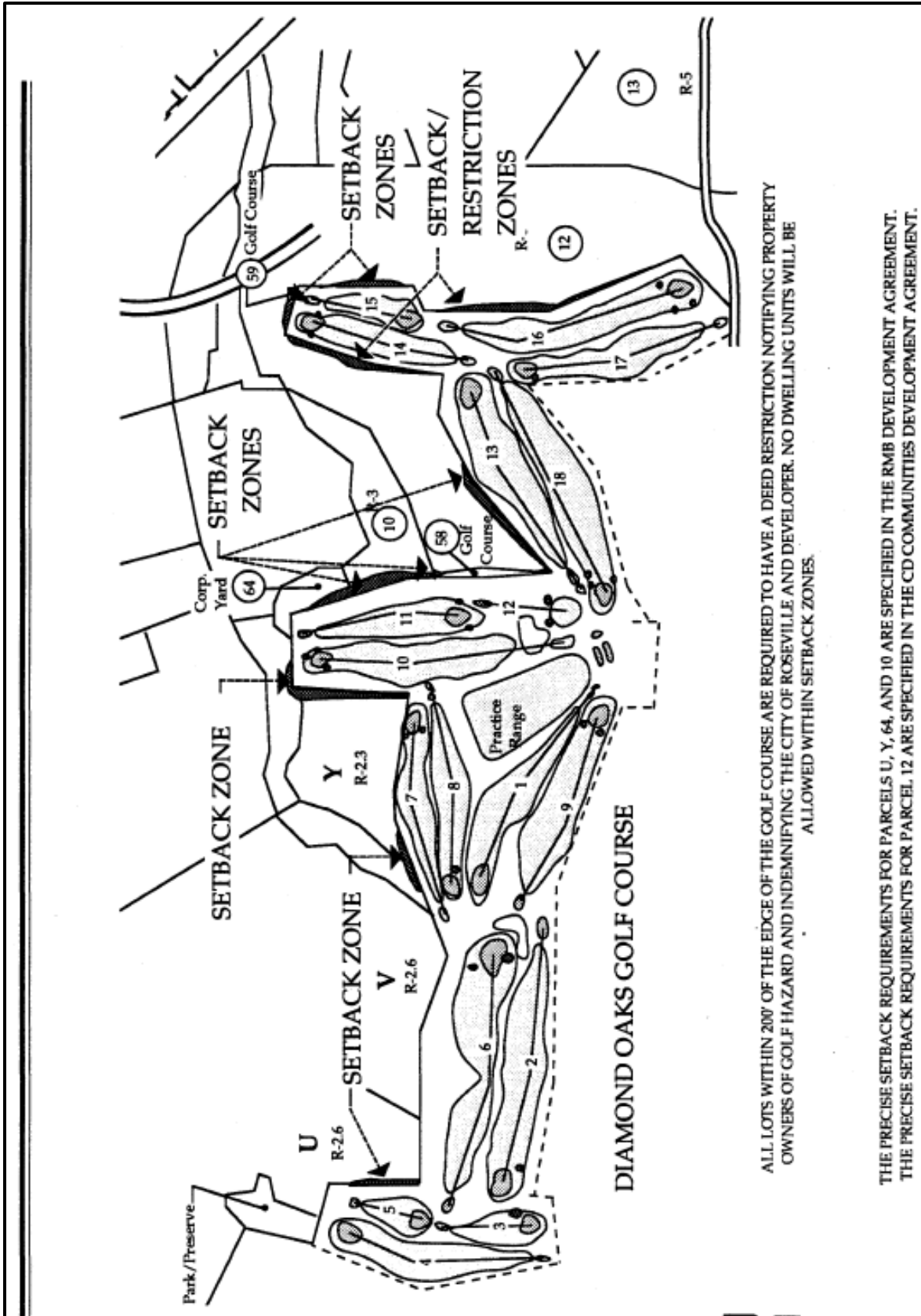
The following guidelines apply to the development of single family residential uses within neighborhoods; additional policies in the General Design Guidelines Section of this element also apply:

1. Each residential area shall be oriented with the dwelling units facing toward a local residential street with only the rear or side property lines of individual residences abutting arterial and collector streets. Walls or fences are used to provide visual privacy and to attenuate traffic noise where appropriate. A six-foot masonry wall shall be provided adjacent to arterial roadways. A six-foot (6') wood and pilaster fence shall be provided adjacent to residential Collectors A and B. Wrought iron and pilaster fencing may be substituted to preserve views if approved by the Commission. If a wrought iron fence is approved, CC&R's for the subdivision shall include language prohibiting the modification or replacement of the fence which would create inconsistent treatment along a continuous section of fence. A six-foot (6') masonry wall shall be provided adjacent to all other collector roadways. The design of such walls shall be as specified in the Landscape Design Guidelines.
2. A six-foot masonry wall shall be provided as a visual screen and acoustical barrier where single family residential neighborhoods abut commercial or business-professional uses, as illustrated in Figure 3-10a, Typical Single Family Residential to Business-Professional/Commercial Interface.

Such conditions are limited to only four locations in the entire Plan Area. One of these locations, Parcel X, is adjacent to a day care facility designed as part of a neighborhood center. The other three locations, Parcels I, J and 13, abut commercial and/ or business-professional/ commercial uses. In some instances a pedestrian walkway from the interior of the neighborhood will connect to pedestrian trails in the adjacent park preserves (such as Parcels 91 and 93), which will then connect to the non-residential use. A plan view of the typical pedestrian connection is shown in Figure 3-10b, Typical Single Family Residential to Business-Professional/ Commercial Plan View Interface.

Single family residential will be separated from open space and pedestrian corridors by a fence and/ or "barrier" landscaping. Alternatives to solid fencing are encouraged. Access between the neighborhood streets and the open space corridor will be provided by pedestrian connections throughout the neighborhood.

3. Pedestrian walkway access connections including, but not limited to, a break in the adjacent fence or soundwall, shall occur at an average interval of every 600 linear feet with the distance between individual access points not to exceed 1000 linear feet along the perimeter of a project. A neighborhood street intersecting the collector or arterial street may be counted as an access connection for the purposes of meeting the above requirement. The location of pedestrian access connections shall be coordinated with the location of transit stops to facilitate the use of public transit. It is the developer's responsibility to make the path connection from the neighborhood project to the adjacent pathway system.



ALL LOTS WITHIN 200' OF THE EDGE OF THE GOLF COURSE ARE REQUIRED TO HAVE A DEED RESTRICTION NOTIFYING PROPERTY OWNERS OF GOLF HAZARD AND INDEMNIFYING THE CITY OF ROSEVILLE AND DEVELOPER. NO DWELLING UNITS WILL BE ALLOWED WITHIN SETBACK ZONES.

THE PRECISE SETBACK REQUIREMENTS FOR PARCELS U, Y, 64, AND 10 ARE SPECIFIED IN THE RMB DEVELOPMENT AGREEMENT. THE PRECISE SETBACK REQUIREMENTS FOR PARCEL 12 ARE SPECIFIED IN THE CD COMMUNITIES DEVELOPMENT AGREEMENT.

Figure 3-9: Golf Course Special Precaution Areas

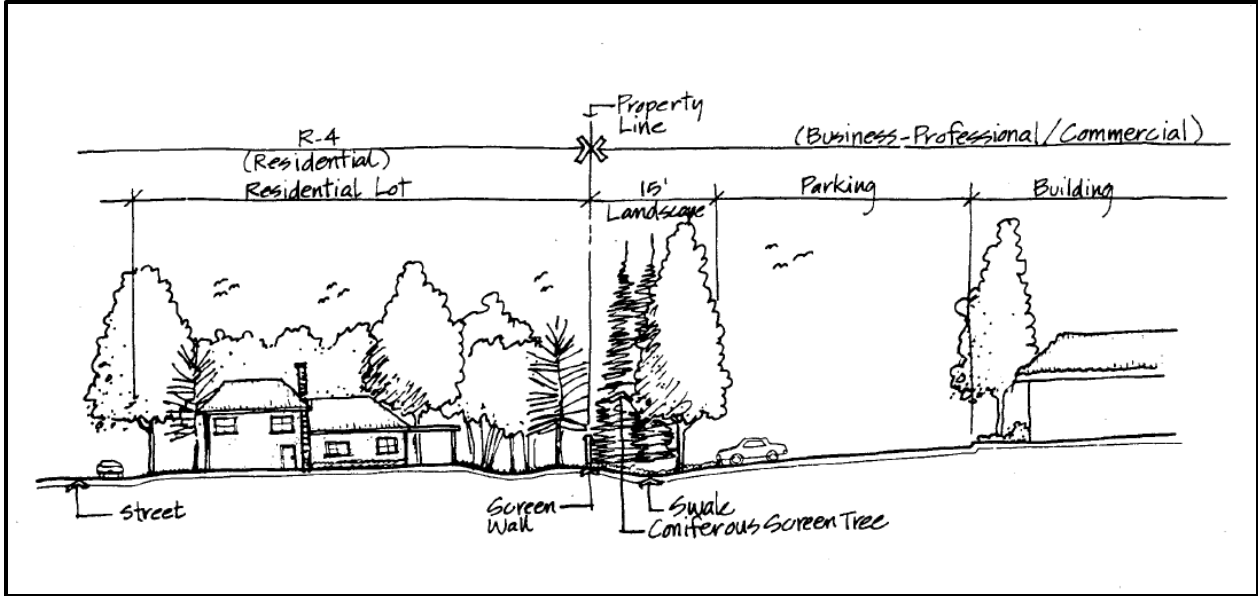


Figure 3-10a: SECTION VIEW - Typical Single Family Residential to Business-Professional/Commercial Interface

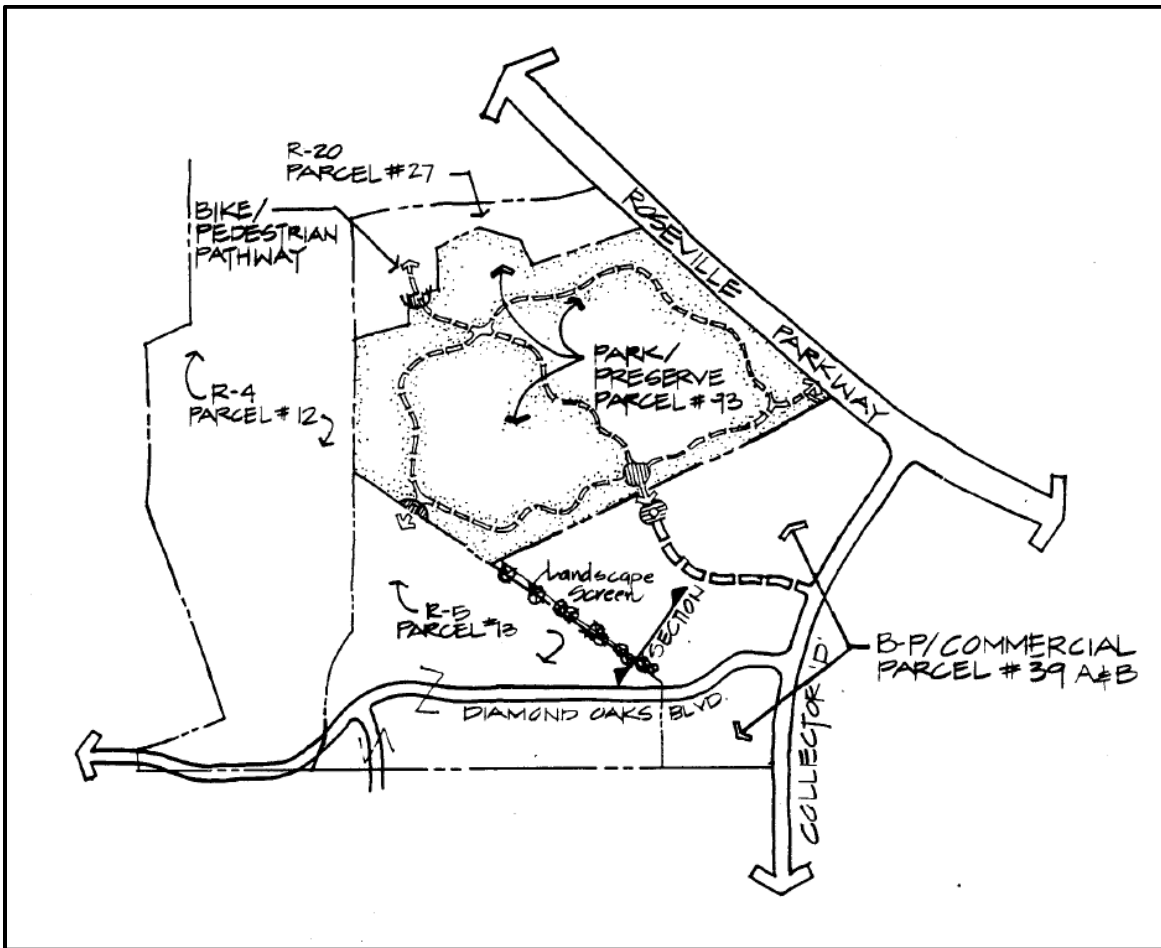


Figure 3-10b: PLAN VIEW - Typical Single Family Residential to Business Professional/Commercial Interface

Side lot orientation is recommended at key locations adjacent to breaks in fencing or soundwalls to provide pedestrian and cyclist access from the interior local street to the adjacent bikeway system. An example of this orientation is illustrated in Figure 3-11, Side Lot and Pedestrian Access Schematic. Side lot and rear lot pedestrian pathway corridors may also be used to accomplish pedestrian access through a neighborhood to an adjacent pathway system.

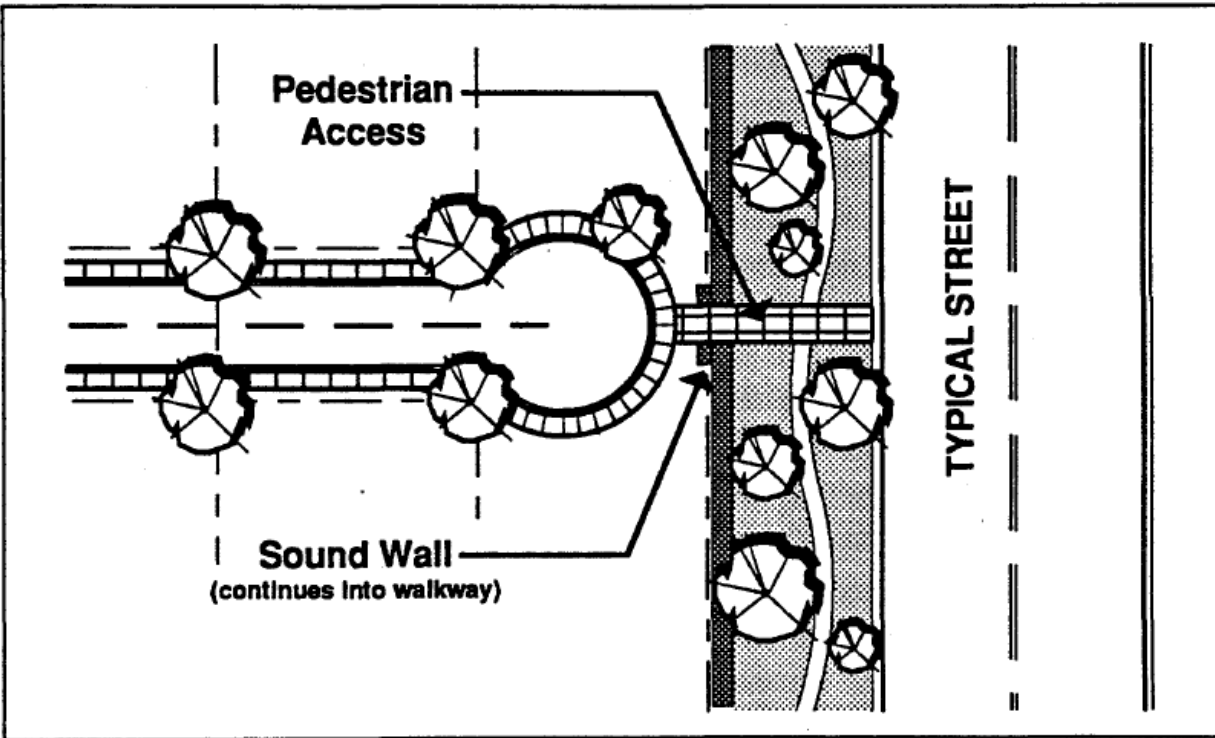


Figure 3-1: Side Lot and Pedestrian Access Schematic

4. The comprehensive pedestrian/bikeway circulation system that runs throughout the entire Plan Area is thoroughly integrated in the residential neighborhoods. The components of the pathway system include the trail systems running through the lower watershed open space corridors, the park preserve areas, and the roadside corridors. The system connects residential neighborhoods to the shopping and employment centers.

Within the neighborhoods the pedestrian system will consist of five foot (5') wide sidewalks separated from the residential street by a five foot (5') wide planting strip. All streets will have sidewalks and a landscaped setback. With limited exceptions no pedestrian walkways throughout the entire Plan Area will be located immediately adjacent to the back of curb. The only exceptions will be immediately adjacent to intersections and transit stops.

Where appropriate, a separate pedestrian pathway corridor may be utilized to provide a connection between streets. The intent is to create a reasonably direct route to the schools and parks. The separate pedestrian pathway corridors are to be limited to areas where the street would not provide a sufficiently direct route. In no case should a separate pedestrian walkway flanked on both sides by a fence be longer than the depth of a single lot. The corridors are to be

landscaped, provide an eight foot (8') wide path connection, and shall have minimum corridor widths as follows:

<u>Length</u>	<u>Minimum Corridor Width</u>	<u>Pavement Width</u>
25' or less	15'	8'
26 to 100 ft.	20'	8'

The general location of the primary pedestrian route within each neighborhood are schematically illustrated on the neighborhood diagrams, Figures 3-6, 3-7, 3-8 and 3-8a, Neighborhoods A, B, C, and D.

A fifty-foot (50') wide open space/bike trail corridor located along the northeastern boundary of Parcels 17A, 17B and 18C will provide a pedestrian/bikeway connection between the bikeway in the lower watershed (Parcel 84) and the bikeway in the landscape corridors adjacent to Washington Boulevard and Pleasant Grove Boulevard. This eight-foot (8') wide pedestrian/bikeway is to be installed as each residential project abutting the fifty-foot (50') wide open space/bike trail corridor is built.

All pedestrian pathways are to be maintained by a Lighting and Landscape Maintenance District. A minimum lighting level of 0.5 foot candles is to be provided along the paved portion of the pathway by adjacent street lighting, lighting standards, or bollards located within the easement. The lighting fixtures shall be placed so as to not impact adjacent residential properties. Figure 3-12a and 12b, Pathway Lighting and Landscape illustrations, illustrates the typical lighting and landscaping along a separate pedestrian pathway.

Where pedestrian paths adjacent to residential properties run in corridors separate from the street, the public reports for the adjacent subdivisions shall include notification of the intent to install such public walkways.

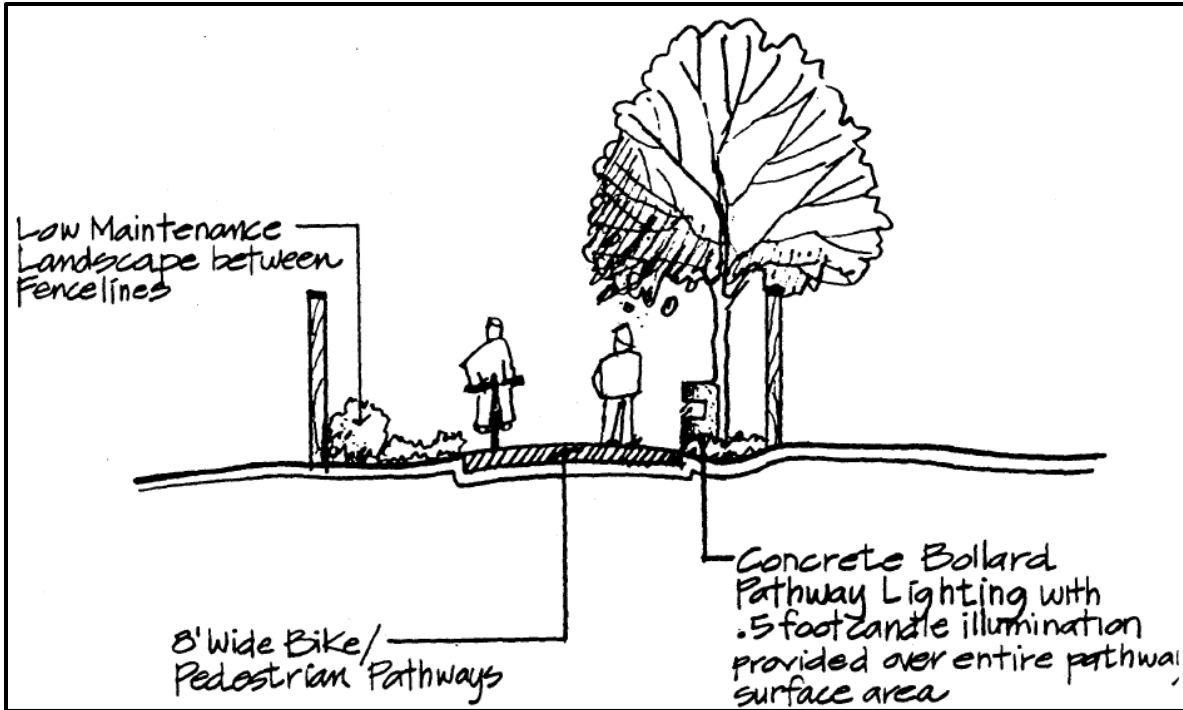


Figure 3-12a: SECTION VIEW - Pathway Lighting and Landscape Illustration

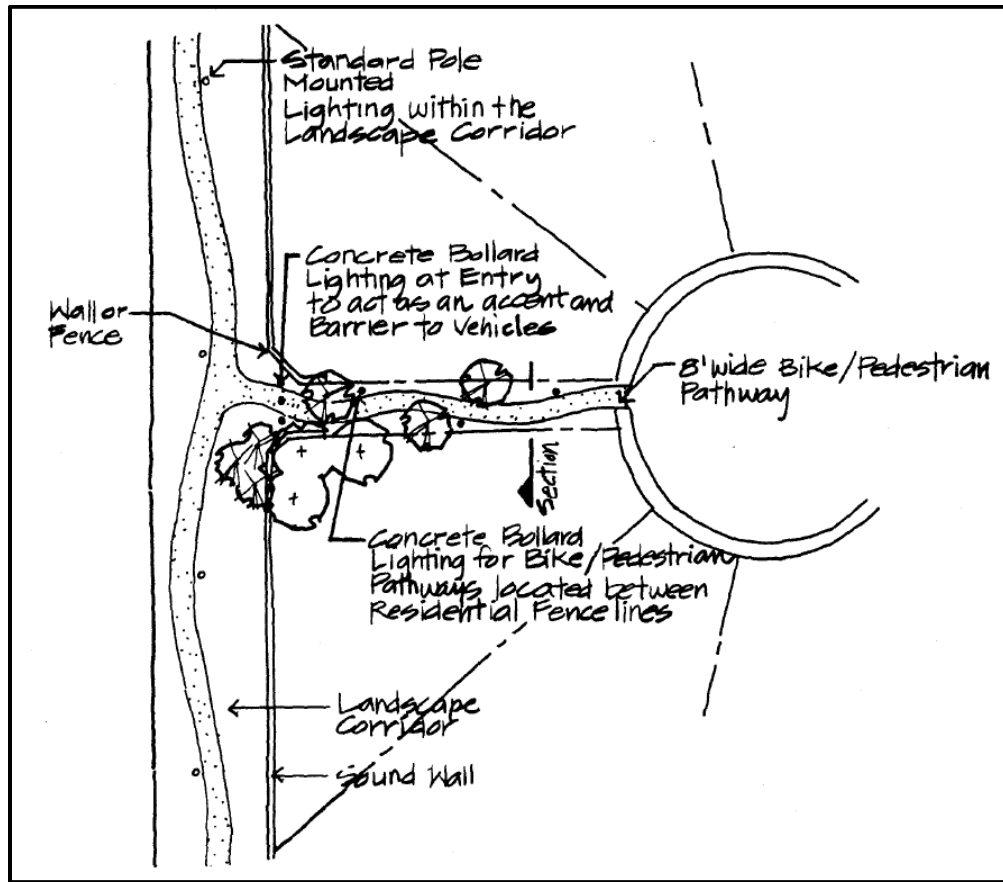


Figure 3-12b: PLAN VIEW - Pathway Lighting and Landscape Illustration

Pedestrian overcrossings are possible at three locations, as illustrated in the neighborhood diagrams. These overcrossings could allow pedestrians to move from one neighborhood to another, and to the neighboring parks, employment, and shopping areas, without having to cross major arterial streets at grade. The precise location and design of the overcrossings will be subject to the City design review process.

5. Arterial streets are not allowed to penetrate neighborhoods, and points of intersection between arterial boulevards and interior residential street shall be limited to reduce safety problems and congestion. However, local residential streets shall intersect adjacent collector streets generally at intervals of not more than every 800 linear feet to ensure that there is sufficient access for residents to the collectors and neighborhood center facilities. (Facilities Section 5.8.1, Policy 5.) The intent is to provide a sufficient number of local street access points to the collectors so that trips are spread and vehicular traffic is not concentrated at a minimal number of entries to a subdivision. Local residential streets within neighborhoods are designed to be narrow and to emphasize internal circulation rather than accommodate through-traffic. Street right-of-way requirements are contained in Section 5.1 of the Circulation Element.
6. Primary vehicular and pedestrian entries to a neighborhood shall be enhanced to present a formal entrance design that identifies a point of arrival. Accent tree planting, special paving, special lighting and entry signage, may combine to communicate the distinctive residential character of the neighborhood. Entry signage may be incorporated into the wall or fence, behind the landscaped corridor adjacent to a primary entry street. Such signage shall be consistent with the City of Roseville Sign Ordinance and the North Central Roseville Specific Plan Signage Guidelines.
7. It is intended that a common identity be established for each neighborhood as a means of establishing a sense of place and scale within the Plan Area. Common identity elements shall include such items as dominant street tree to be used on the local residential streets throughout a neighborhood, coordination among the project entry signage, common perimeter wall or fence design, and consistent landscaping treatment along public areas.
8. The following information shall be provided with the first project within any of the four identified neighborhoods:
  - A neighborhood pedestrian/bicycle system plan which illustrates how the neighborhood system links with the larger backbone system and activity centers, such as parks, schools, retail, and employment centers.
  - A schematic landscape plan for the entire neighborhood describing the general landscape treatment, materials, and identifying street trees.
  - A preliminary grading plan for the entire neighborhood indicating the location of sensitive natural features, the resulting drainage patterns and the balance of materials within the neighborhood.
  - A plan coordinating project entry signage and fence design and materials for the entire neighborhood.
9. Buildings within neighborhood groupings shall include a variety of roof lines, building heights, color schemes, and setbacks to achieve visual interest. The architectural style shall be compatible among all buildings within each residential neighborhood.
10. Where the center-line of a street alignment does not vary by more than ten feet (10') along the frontage of six or more adjacent single family parcels, the front yard setbacks of not less than

forty percent (40%) of the dwellings on each side of the street shall vary by a minimum of three feet (3').

11. Single Family structures shall minimize the amount of visible building frontage occupied by a garage. Except as provided below, a maximum of fifty percent (50%) of the building frontage facing a street shall be devoted to garages, carports or open parking. For lots of 6,000 square feet or more, the garage frontage as a percentage of the total building frontage may be increased to 60% to accommodate three car garages. In all such cases, either the garage frontage will be recessed, not less than five feet, relative to the frontage of a major building feature \* or the livable portion of the dwelling or at least one of the three garage doors shall be recessed, not less than five feet, relative to the frontage of the two remaining garage doors. Rear, side loaded, and recessed garages are encouraged. For lots of 6,000 square feet or more, the minimum setback for a garage to the front property line is twenty feet (20'). The setback for a major building feature<sup>1</sup> or the livable portion of the dwelling may be reduced to fifteen feet (15') in order to recess the garage frontage.
12. Parcel 12 shall provide pedestrian and vehicular access for Parcel 10.
13. Residential lot configurations, which will accommodate zero lot line, cluster and other flexible designs, may be used to maximize land use efficiency and to preserve natural features.
14. Buildings shall be sited with respect to topography, vegetation and other natural features of the building site. Such features shall be considered as primary site design determinants.
15. Parcel 4B shall retain the steep portion of the slope adjacent to the golf course through a natural ungraded area in its detailed grading plan. The boundaries of the ungraded area will be defined through the subdivision review process.
16. Access and infrastructure extensions shall be provided to non-residential uses within neighborhoods such as schools and parks in the same time frame as those services are provided to adjacent residential lands.
17. Traditional and alternative neighborhood designs utilizing elements such as grid street patterns, alleys or greenbelts may be considered on a planned development basis and are not prohibited by this Specific Plan.
18. Development on Parcel 44 shall be subject to parcel specific development standards adopted with the R3/DS Zoning and on file with the City of Roseville Planning Department.

### 3.4 Attached and Multi-Family Housing

Multi-family housing in the North Central Specific Plan Area is located in close proximity to the major employment centers, and the major services and shopping areas, in order to facilitate pedestrian connections and provide a resident population to support these activities. In general, densities of 10 dwelling units per acre and up are considered multi-family housing. The proximity of multi-family housing to the light industrial and business-professional uses will provide a substantial component of the jobs /housing balance within the City. Furthermore, the location of multi-family housing in proximity to Roseville Parkway provides a population base within easy walking distance to support public transit and potential light rail service.

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<sup>1</sup> For the purposes of implementing this guideline a major building feature shall be integral to the design of the dwelling and shall include horizontal and vertical elements of the dwelling inclusive of the roof line. A major building feature must also be successful in de-emphasizing the garage frontage of the dwelling. The determination of whether a building feature fulfills this guideline shall be made by the Planning Director.

Parcels 20, 21a and 21b are located on the Mehrten formation adjacent to the community park, the lower watershed preserve area, the business-professional center, and across from the commercial core area. In addition these sites are served by arterial and/ or collector streets. For these reasons, these three sites are particularly well suited for high-density multi-family housing.

The multi-family housing sites are generally located on the Mehrten mud flow formations. The landscaping associated with multi-family housing is typically concentrated in small, intensively landscaped areas where the level of design, site preparation, and on-going maintenance can be expected to sustain the plant materials.

In addition, multi-family sites are typically located adjacent to open space or park preserve which allow for pedestrian access directly to the Class I bike trail system. The housing units shall be oriented to take advantage of the open space, and direct pedestrian connections must be provided from the interior of the housing project to the adjacent open space. The following policies apply to multi-family development within the North Central Specific Plan Area. Additional policies in the General Design Guidelines Section of this element also apply:

1. Single story apartment or townhouse buildings shall be set back a minimum of twenty feet (20'), and buildings of two stories or more in height shall be set back a minimum of seventy-five feet (75') from the boundary of any adjacent single family residential land use.
2. Separate vehicular and pedestrian circulation systems shall be provided within multiple family projects which minimize auto and pedestrian conflicts.
3. Common open space areas shall be planned with specific functions in mind. Such areas should be more than just "left-over" spaces after building design.
4. Open space areas shall be connected with on-site pedestrian circulation systems. Common areas shall be readily accessible from all buildings.
5. Open space areas shall be used to preserve existing natural features where appropriate.
6. Multiple family residential units adjacent to open space corridors should be oriented towards open space and shall incorporate such corridors into project design. Parking areas adjacent to parks and open space shall not exceed fifty percent (50%) of the common boundary frontage. The minimum setback from multiple family buildings to the edge of the park or open space area shall be twenty feet (20'). Balconies, patio and parking area setbacks shall be a minimum of fifteen feet (15'). Berms, landscaping, and setbacks may be used rather than walls when a separation between the multiple family and park or open space uses are required. The relationship between open space and multi-family housing is illustrated in Figures 3-13a and 3-13b, Park Interface at Multi-Family Residential.
7. Private open space shall be directly accessible from adjacent units, should be large enough to allow outdoor activities and shall consider wind and sun as factors of orientation.
8. Buffering between multi-family development and major roadways, or non-residential uses, shall be provided by setbacks, soundwalls, or fencing, landscaping and berming, or a combination of all the above. The use of setbacks, landscaping and berming, rather than soundwalls or fencing, is encouraged.
9. To ensure pedestrian access within the community, clear connections to the pedestrian pathways shall be provided at regular intervals in the landscape corridors, along the adjacent roadways, and open space corridors. Pedestrian pathway access connections shall occur along the perimeter of a project at an average interval of every 600 linear feet, with the distance

between individual access points not to exceed 1000 linear feet along the perimeter of a project. In residential areas, a neighborhood street intersecting a collector street may be counted as an access connection for the purposes of meeting the above requirement. The location of pedestrian access openings shall be coordinated with the location of transit stops to facilitate the use of public transit.

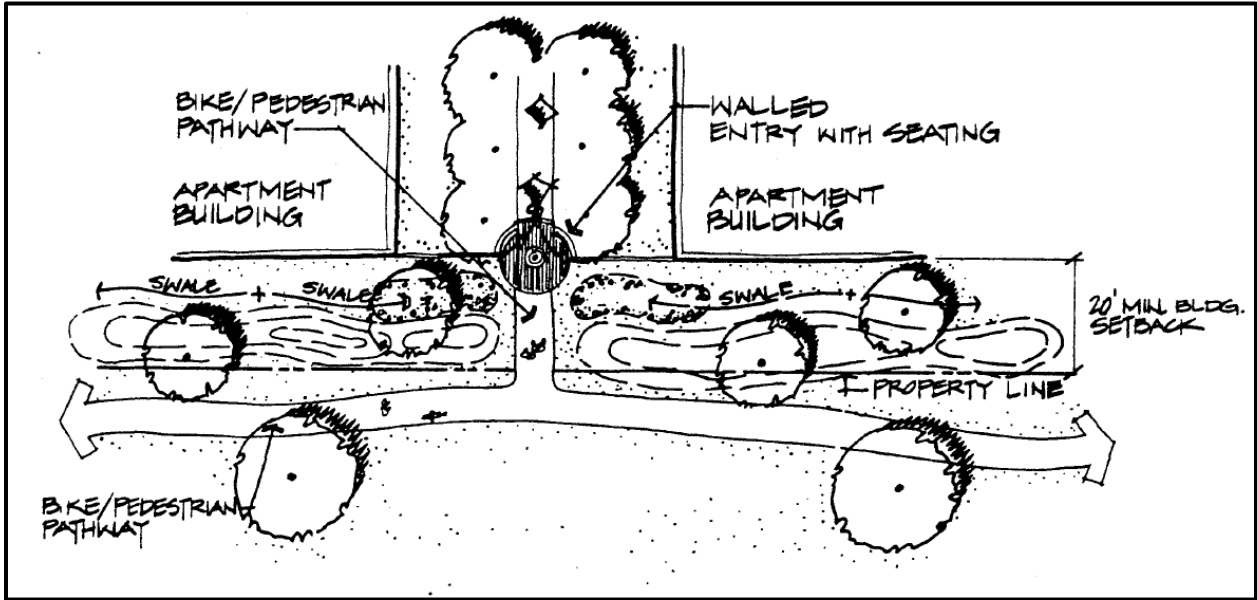


Figure 3-2a: PLAN VIEW - Park Interface at Multi-Family Residential

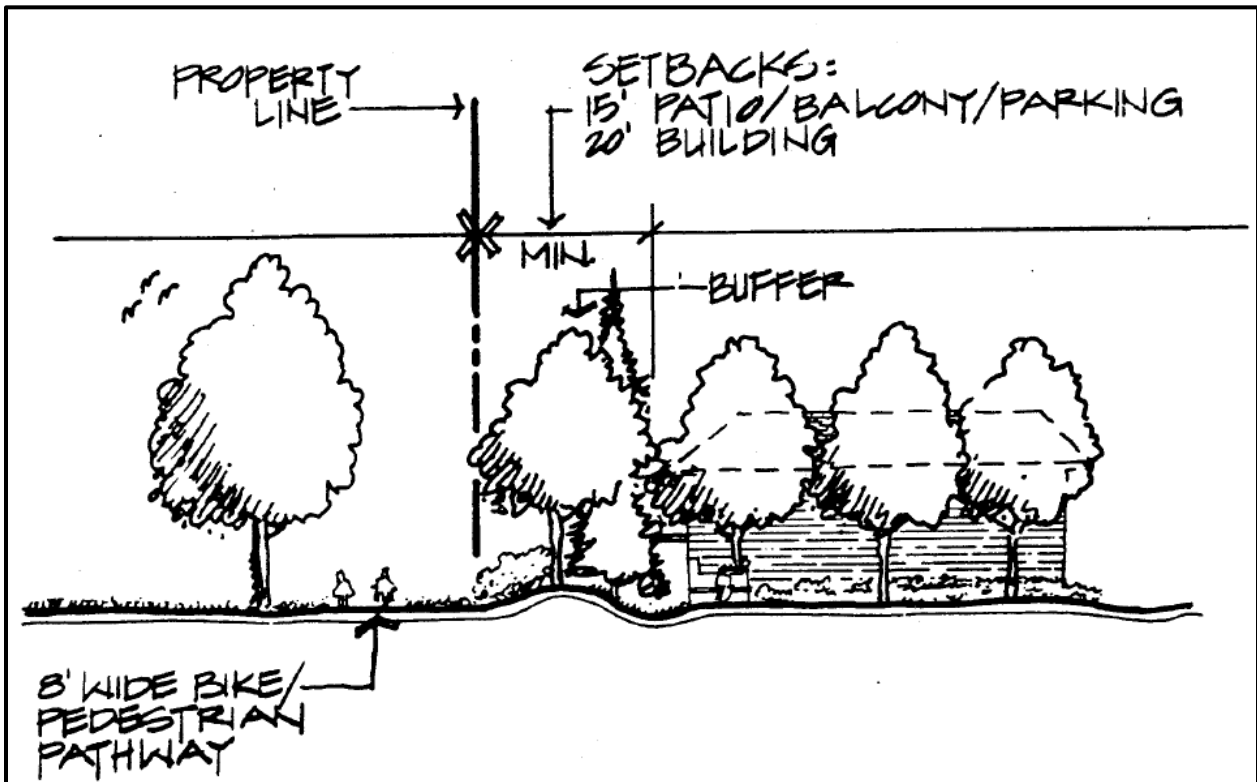


Figure 3-13b: ELEVATION - Park Interface at Multi-Family Residential

10. The design of multi-family uses shall consider the possible future extension of light rail including right-of-way and station needs. On-site transit stops, including those for buses, shall be considered during the design review stage.
11. Detailed noise analysis will be required at the design review stage for all multi-family projects adjacent to major roadways and the Southern Pacific Railroad. Where multi-family dwellings provide private exterior living areas, the noise levels in such areas shall not exceed 60 dB Ldn. Sound attenuation may include soundwalls, berms, landscaping, orientations, and setbacks or a combination thereof.
12. Recreational facilities in addition to those provided by the City park system, such as swimming pools, tot lots, and open space areas, should be provided in sufficient quantity to accommodate the estimated project population.
13. The design of Parcel 21b shall carefully consider its interface with community commercial Parcel 32. Parcel 21b shall provide pedestrian connections and orientation to Parcel 32 and Parcel 32 shall provide screening for any service areas. The pedestrian connections between the parcels shall be incorporated into the design of all buildings.

#### 3.4.1 Special Policies for Parcels 22, 23, and 46

Parcels 22, 23 and 46 present unique conditions due to the proximity of the Highway 65 Bypass, the Southern Pacific Railroad corridor, and limited site access. These parcels are accessible only from a public roadway connector (Street E) across Antelope Creek through the retail commercial area on the east side of Harding Boulevard (Parcel 36). Consequently, additional policies are required.

1. The alignment of Street E will be dependent upon the development of Parcel 36 and final delineation of wetlands to be preserved within the parcel. The alignment of Street E will be located at the time of development review of Parcel 36 and may impact the time frame that Parcels 22 and 23 may develop.
2. A twenty-five foot (25') landscape setback shall be provided from the property line adjoining the railroad corridor along Parcel 22 and 46. The corridor shall incorporate berms, landscaping, and a soundwall designed as noise barriers in accordance with an acoustical analysis based on project layout. Such analysis shall be submitted as part of the project review process. Parking (including carports) should be provided immediately adjacent to the setback so as to maximize distance from railroad to residential buildings. Landscaping may be required on the eastern side of the soundwall to soften the view of the wall from Taylor Road.
3. The location and design of outdoor activity areas shall be planned to provide the maximum practical amount of shielding within the 60 dB Ldn contour from Highway 65.
4. Where specified by the required noise analysis, building design should provide noise level reduction measures to meet interior noise level standards.
5. Vehicular emergency access shall be provided and maintained between Parcel 22 and Springview Road to the north under the Highway 65 Bypass overcrossing. The construction of such connections shall be the responsibility of the project developer.
6. Buildings shall be designed to relate to the adjacent park (Parcel 57) and watershed area (Parcel 85). Pedestrian linkages from the high-density residential uses to the park, watershed, and pedestrian pathways shall be provided.

## COMMERCIAL LAND USE

### 3.5 Day Care Center

A day care center site of approximately one acre (Parcel 47) is located adjacent to a neighborhood park (Parcel 53). The site is conveniently located to provide day care service for the residents of the neighborhood. The site has excellent access from Roseville Parkway and will be convenient to the employees in the employment centers to the north of Roseville Parkway. The day care center is intended to be compatible with the residential neighborhood.

The following policies shall apply to the development and operation of a day care center:

1. It is intended that the buildings present a pedestrian scale facade at the street edge. The minimum building setback is twenty-five feet (25') from back of the curb. Not less than sixty percent (60%) of the building facade must be located at the minimum street frontage setback. The building must provide direct access from the sidewalk.
2. The primary parking area for the day care center is to be located at the side or rear of the parcel away from the adjacent park.
3. Buildings may be no closer than twenty feet (20') from any residential property.
4. A minimum ten foot (10') setback, landscaped with screen trees, "barrier" shrubs and a masonry wall, shall be provided between the day care center and any adjacent residential property.
5. Hours of operation and delivery may be limited pursuant to a use permit to avoid disturbance of the adjacent residential areas.
6. The buildings shall be compatible in scale and architecture with a typical single family residential neighborhood.
7. The play area for the day care should be oriented to the park. Any fence between the park and day care should be an open material to allow a view from the day care center to the park.

### 3.6 Neighborhood Commercial

A neighborhood commercial site of 2.23 acres (Parcel 61) is located adjacent to residential Parcels 17b and 18a to the northwest and southeast, respectively. The site is conveniently located to provide neighborhood serving commercial uses to residents of the adjacent neighborhoods. Development of Parcel 61 is intended to provide the adjacent residential community with convenient, compatible and low-intensity commercial uses in proximity to homes and accessible by pedestrians, bicyclists and vehicles via access from Roseville Parkway.

The following uses conditionally permitted in the Neighborhood Commercial zone in the City Zoning Ordinance shall be prohibited on Parcel 61: gasoline sales, veterinary clinics and heliports.

In addition, the following uses permitted in the Neighborhood Commercial zone in the City Zoning Ordinance may be permitted on Parcel 61 subject to the approval of a use permit by the City: convenience eating and drinking establishments, and maintenance and repair facilities.

The following policies shall apply to the development of the Parcel 61 Neighborhood Commercial property:

1. Buildings shall be no closer than twenty-five (25) feet from the side property lines, with rear setbacks a minimum of ten (10) feet for both building and parking on the parcel. Parking shall be set back fifteen (15) feet from the side property lines.
2. The front yard setback for building and parking (inclusive of the landscape easement) shall be fifty (50) feet.
3. Other development standards for parking, lighting, loading and landscaping shall be set forth in the City Zoning Ordinance or the Community Design Guidelines, unless otherwise provided for in this Specific Plan.
4. Buildings shall be of a residential scale. Specifically, roof lines should be predominantly one story, with opportunities for higher design elements.
5. A solid six (6) foot high masonry wall along the western and eastern property lines abutting residential properties shall be provided.
6. No loading/unloading areas shall be allowed behind buildings on Parcel 61 adjacent to the open space parcel.
7. No trash containers/dumpsters shall be allowed behind buildings on Parcel 61 adjacent to the open space parcel.
8. Lighting on the sides of buildings facing adjacent residential parcels shall be shielded so as to prevent the lighting source on Parcel 61 from being visible beyond Parcel 61 's boundaries with adjoining residential users.
9. Landscaping with the 25 foot wide landscape setback area adjacent to residential properties to the east and west shall include both deciduous and evergreen trees.

[TEXT DELETED 1/96]

[FIGURE 3-14 DELETED 1/96]

[FIGURE 3-15 DELETED 1/96]

[TEXT DELETED 1/96]

### 3.7 Community Commercial Land Use

The community commercial land use is intended to provide a concentration and mix of shops and services large enough to meet the daily shopping needs of the residents and employees in the North Central Specific Plan Area. Placement of community commercial in close proximity to residents and employees, and providing pedestrian access connections, will help reduce the number and length of shopping and leisure trips, and the related effects of automobile use. A substantial percentage of the residents within the Specific . Plan will be within a one-half (1/2) mile radius of a community commercial center. This equates to, at most, about a ten to twelve minute walk for most people, and is expected to encourage people to walk for many of their limited purchase, single purpose trips.

[FIGURE 3-16 DELETED 1/96]

Typical uses permitted in the community commercial centers may include:

- Drug store
- Variety store
- Supermarket
- Restaurant/Coffee shop/Fast Food
- Professional Services such as bookkeeper, attorney
- Post Office
- Hardware without outside storage
- Personal services such as flower shop, shoe repair, barber
- Gas station (subject to Policy 6)
- Bakery
- Specialty apparel
- Sporting goods
- Banks

Care is to be taken in the siting of buildings, the ease of public access, and the physical interface with adjacent land uses, to ensure that community commercial land use can be successfully integrated at the edge of a neighborhood or adjacent employment area. The shopping areas will be designed to provide safe and convenient pedestrian access from the surrounding uses. This will be partially achieved by providing pedestrian pathway systems from the interior of the neighborhoods or employment centers to the stores, and by providing a clear pedestrian pathway from the street front sidewalk to the shops. The typical relationship between a commercial site and the surrounding neighborhood or employment areas is illustrated in Figure 3-17, Park Preserve to Commercial Interface.

Two sites, encompassing a total of 25.41 acres (Parcel 31, 13.73 acres; Parcel 32, 11.68 acres) are located at the intersection of Pleasant Grove Boulevard and Roseville Parkway. Parcel 31 abuts an open space areas which provides an unusual settings and opportunity for orientation of the centers. It is 'intended that the Parcel 31 commercial centers provide some portion of the building space oriented toward the open space to encourage a pedestrian approach from the adjacent neighborhood or employment uses, and to allow for a restaurant or other public gathering place to look out onto the open space areas. A conceptual illustration of this concept is shown in Figure 3-18, Commercial/Open Space Overlook Illustration.

In addition to pedestrian oriented shopping areas, there is a need for retail and service commercial uses that can be easily accessed by automobiles. These include large stand alone retail uses, such as furniture and appliance sales, auto service centers, and highway oriented commercial uses. A third community commercial site is located on Parcel 48a, adjacent to Pleasant Grove Boulevard and south of the Pleasant Grove/Route 65 interchange. The site is intended to provide commercial uses normally situated

at highway interchanges near adjacent employment uses. Immediately to the west of Parcel 48a is Parcel 55a, a 9.84 acre site, also designated for Community Commercial land use. The 41.03 acre community commercial site (Parcels 34a, 34b, and 34c) on the northwest corner of the Harding Boulevard/Stanford Ranch Road interchange with Highway 65 and the community commercial sites (Parcel 48a and further to the west, Parcel 55a) on the southwest corner of the Pleasant Grove Boulevard interchange with Highway 65, have been targeted for such uses.

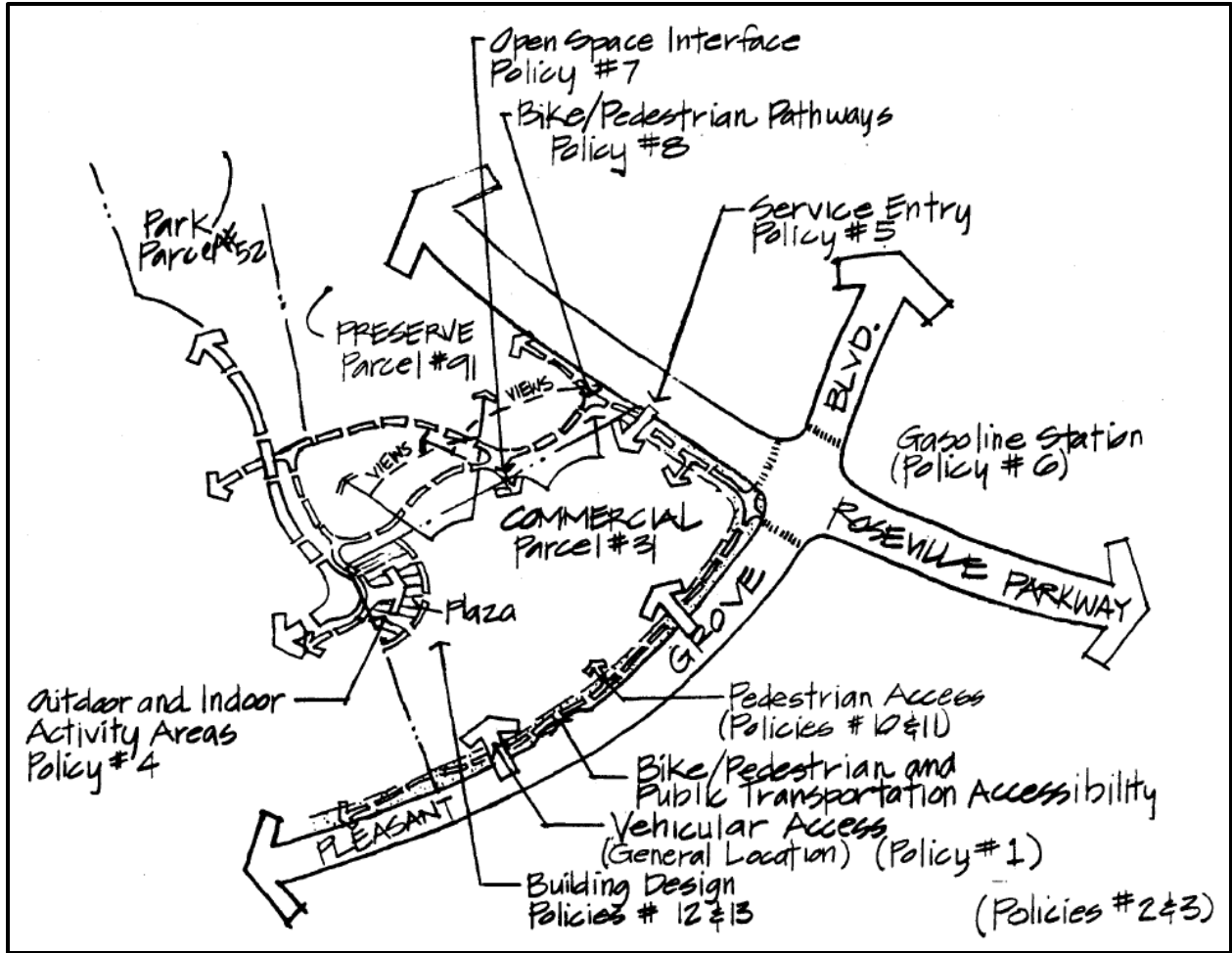


Figure 3-17: Park Preserve to Commercial Interface

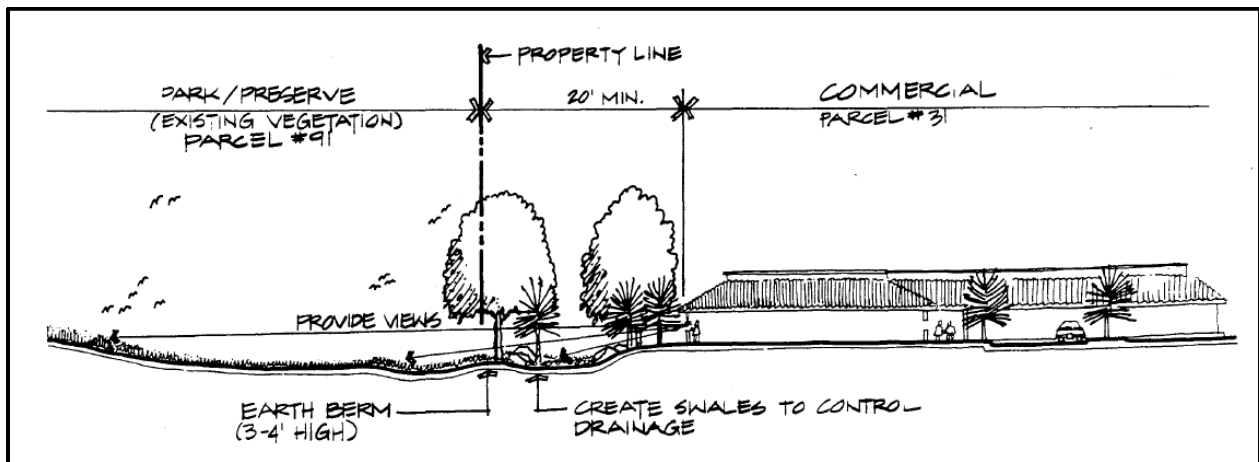


Figure 3-18: Commercial/Open Space Illustration

the uses previously specified community commercial, the following uses may be permitted for Parcel 34:

- discount outlet
- hotel
- junior department store
- furniture or appliance store
- sporting goods
- movie theaters and other entertainment facilities
- auto service center

The following policies shall apply to the community commercial land use. Additional policies contained in the General Design Guidelines section of this element also apply:

1. Each community commercial area shall be accessible from at least one major collector of arterial street, with sufficient design capacity to accommodate traffic generated by the businesses as well as other local traffic.
2. Community commercial areas shall be accessible by public transportation, and from pedestrian sidewalks and bicycle routes. Consideration shall be given at the design review stage to on-site transit stops, including but not limited to bus stops.
3. Bicycle parking racks shall be provided in highly visible locations at each community commercial parcel. The provision of lockers and showers for employee use may be considered pursuant to the City Rideshare Ordinance.
4. Opportunities should be provided for outdoor and indoor public activity spaces such as seating areas, fountains, and plazas for special promotional events.
5. Provide visually separated service entries for the delivery of merchandise.
6. Gas stations, providing gasoline sales and limited auto service and repair, may be located subject to a use permit in any community commercial land use designation, except for Parcel 48a, which shall be permitted those uses set forth as permitted or conditionally permitted in the CC zone. Gasoline service stations and hazardous materials handling shall be prohibited on the Community Commercial portion of Parcel 43 (Parcel 43a) immediately to the west of the proposed fire station. Consideration should be given to orienting the service islands away from the street. The stations are to be designed as an integral part of the commercial site in terms of architectural treatment, materials, colors, building location and orientation, access, and parking lot circulation.
7. Buildings should incorporate, to the extent feasible, adjacent open space as a visual amenity. A minimum twenty foot (20') setback shall be provided from the edge of the open space areas. Such setback shall be landscaped and may include berms and swales to create a boundary and control drainage.

Fencing between commercial and open space use is discouraged. When necessary, such fencing should be open type to allow for continuous view to the open space area, except where screening is desired. Building design shall consider views from the adjacent open space areas. In general, architectural treatment and materials for those frontages visible from adjacent open space areas shall be the same as those utilized on the main frontages of the buildings. All delivery and storage areas shall be screened.

8. Pedestrian access between Parcel 31 and the adjacent residential Parcels I and J shall be provided through the park preserve (Parcel 91 ).
9. A six foot (6') masonry wall with a minimum fifteen foot (15') planter strip shall be provided on Parcel 31 adjacent to Parcels I and J. Buildings shall be set back a sufficient distance and be designed to minimize impacts on adjacent residential units to the extent practical. The building setback will vary dependent upon building height and bulk, type of users and topography, and shall be a minimum of thirty-five feet (35') for single-story buildings and fifty feet (50') for two-story buildings. Trash enclosures, noise generating equipment, and other nuisances shall be located away from Parcels I and J.
10. Parcel 32 shall provide pedestrian connections and orientation to Parcel 21 b and shall provide screening for all service areas. Additionally, the pedestrian connection between the parcels shall be incorporated into the design of the buildings.
11. Although the surrounding uses are urban reserve, the design of Parcel 34 shall consider potential pedestrian connections from Parcel 101, should this area ever develop.
12. Pads or buildings directly fronting roads shall provide direct connections from the landscape corridor pathway along the adjacent street.
13. A clear pedestrian pathway separated from vehicle travelways shall be provided along each street frontage at regular intervals. The pathway shall be treated in such a manner as to guide the pedestrian to the main building accesses as shown in Figure3-19, Pedestrian Walkway in Commercial Parking Lot. A minimum of one such walkway shall be provided per street frontage, with the interval between walkways not to exceed 600 linear feet along the perimeter of the project. The location of the pathways shall be coordinated with transit stops. Pedestrian pathway access shall also be considered at all vehicular driveway locations. It is the developers' responsibility to make the pathway connection from the project buildings to the pedestrian pathway along the street.

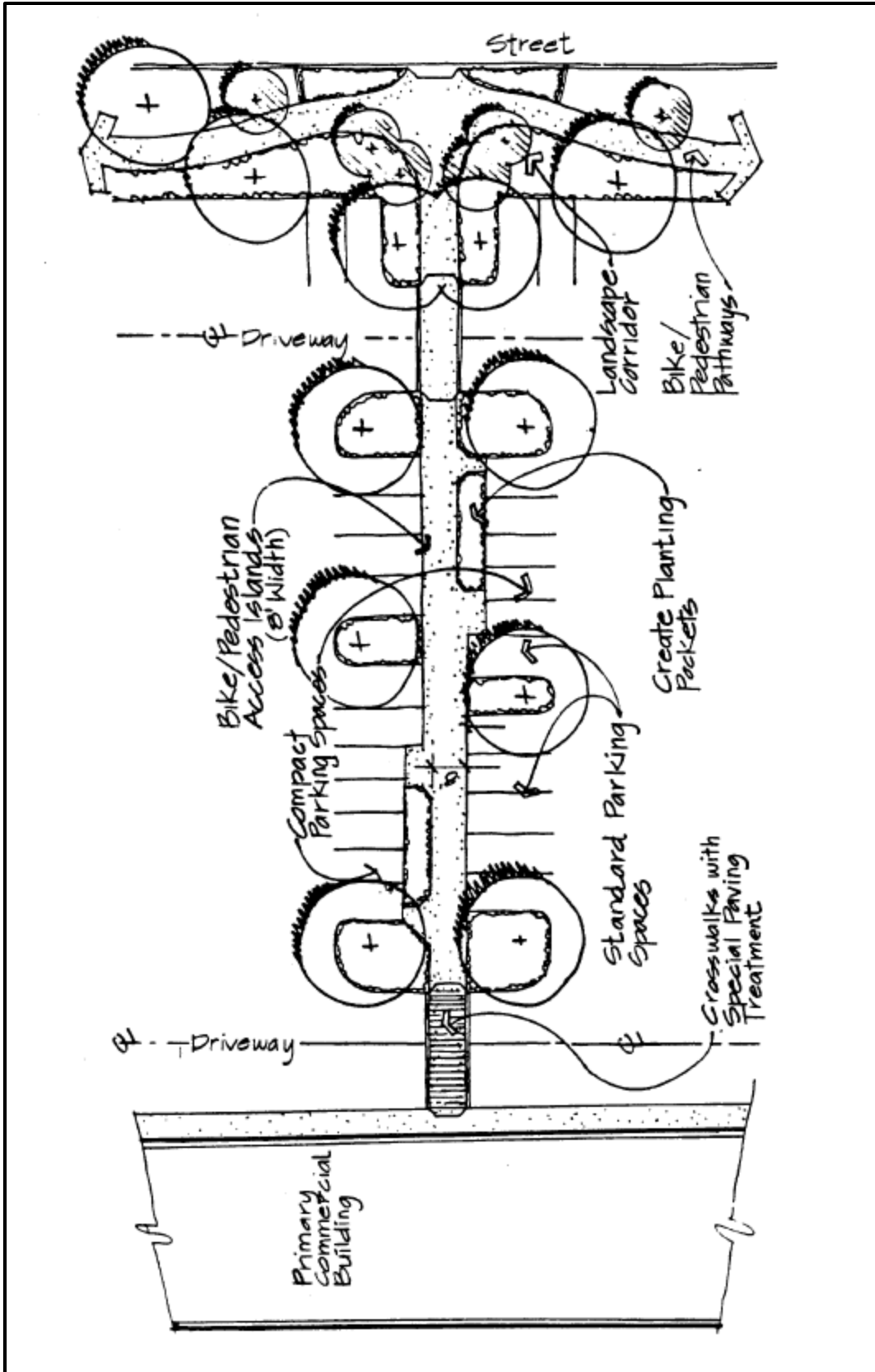


Figure 3-19: Pedestrian Walkway in Commercial Parking Lot

14. Building site design should consider alternatives to the standard "L" shape or strip building configuration. In order to strengthen the streetscape, pad sites or a portion of the main buildings should be located at the street frontage.
15. Large single-user freestanding retail commercial buildings which are not integrated in an overall pedestrian oriented site design, are generally discouraged, and shall not be the dominant form of commercial use on any parcel. Architectural design measure should be incorporated to visually reduce the bulk and large frontages often associated with such uses. All commercial uses are subject to a use permit. All community commercial parcels must be master planned for the entire site.
16. The master plan for all community commercial parcels adjacent to potential light rail corridors shall take into consideration the possible future extension of light rail, including right-of-way and station needs. The design of Parcels 31 and 32 shall consider the possible future construction of an urban interchange at the intersection of Roseville Parkway and Pleasant Grove Boulevard.
17. In addition to the other uses specified for the community commercial land use, Parcel 34 may also accommodate large users such as a junior department store, discount outlet, furniture or appliance stores, sporting goods, hotel, auto service center, movie theaters or other entertainment activities, and similar large facilities. All such uses shall be designed in accordance with the policies in this section.

### 3.7A HIGHWAY ORIENTED COMMERCIAL

*(Parcels 48A and 55A)*

The Highway 65 corridor defines the northern edge of Parcels 48A and 55A. The NCRSP freeway frontage is approximately two miles in length with the existing Harding/Stanford Ranch Road Interchange anchoring the eastern reach and the future Blue Oaks Interchange anchoring the west. At the center of the NCRSP Highway 65 frontage is the planned Pleasant Grove Interchange. This interchange will provide a major link between Roseville and Rocklin by way of Pleasant Grove Boulevard which will become Park Drive in Rocklin, and provide direct access to this highway oriented commercial parcel.

#### 3.7A.1 Parcel 48A and Parcel 55A Development Guidelines

Pleasant Grove Boulevard is designed to provide direct access from the Highway 65/Pleasant Grove Boulevard Interchange to Parcel 48A and Parcel 55A.

Due to freeway proximity, future buildings on Parcel 48A and Parcel 55A may be visible to SR 65 motorists. Although the mass of future buildings may be screened to some degree by the cut slopes along the edge of the right of way, the following design standards are incorporated to further soften the development edge along the SR 65 corridor of Parcel 48A and Parcel 55A:

- Commercial buildings are to be set back a minimum of 50 feet from the SR 65 right of way. Commercial parking areas, including driveways and delivery bay areas, are to be set back a minimum of 40 feet from the SR 65 right of way. Any dumpster enclosures, service entries, and/or loading dock shall be fully screened from view with a combination of block walls and landscaping.

- Setback and open areas between future buildings or parking and the freeway right-of-way are to be landscaped with a mix of trees, shrubs and ground covers. The landscape planter shall have a width of 40 feet.
- The primary tree along the SR 65 frontage shall be the Bloodgood London Plane tree planted in a staggered pattern at 30 feet on center.
- Where permitted by individual site development plans, tree planting pockets abutting the setback area should be developed between adjoining buildings or clusters of buildings or at "dead spaces" occurring within individual site development plans. The tree planting pockets are intended to introduce a rhythm of intermittent tree groves along the freeway edge.
- Approximately one third of the trees planted in the setback and pocket areas should be conifer trees for vertical scale and screening.
- All building elevations visible from the freeway should be designed to incorporate architectural detail that is sensitive to such visibility and that avoids the featureless design typically associated with "backside" facades.
- Where this commercial parcel lies adjacent to watershed open space parcels, a landscape buffer is to be installed along the abutting edge of the non-open space parcel. The minimum buffer width is 20 feet for parking and 25 feet for buildings.
- Although highway commercial parcels are not required to be master planned, considerations of internal function shall be addressed in the context of a conceptual site plan at the time a development proposal is initiated for any portion of the individual site. Where a mix of uses is anticipated for the site, particular attention shall be given to site circulation, access and parking. A clearly defined pedestrian link running parallel to the freeway is encouraged.

### 3.8 Regional Commercial Core Area

The commercial core area is comprised of two regional commercial sites totaling approximately 188.89 acres, (Parcels 35 and 36), and ancillary parcels adjacent to the two primary parcels (Parcels 33, 37, 38, 39, and 40). The specific policies regulating the development of Parcels 38, 39 and 40 are found under the appropriate use sections of this element. The commercial core parcels comprise core retail, service, and leisure activities that serve the entire South Placer region. Population projections indicate that the region will be able to support one large retail mall and substantial additional commercial activity at this location.

The intent is to provide a mix of land uses within the core area that will support greater diversity and more pedestrian activity than is likely to occur at a single-purpose retail center. A higher level of development intensity is encouraged than would otherwise occur at a suburban shopping center. In order to achieve this mix of uses, one of the two regional commercial sites, (Parcel 35 or 36), has been identified as a regional mall, featuring shopping, services, leisure, and other activities in a single complex. The remaining area provides in-line and pad tenant spaces for a complementary mix of uses which may include office, service, retail commercial, restaurants, leisure, and cultural facilities. This area is envisioned as the "hub" of activity in the region. The core area is characterized by tall, landmark buildings, public plazas, a distinctive pedestrian circulation system, and a diverse mix of uses.

The parcel developed as the regional mall may include the following uses:

- retail sales and services (including major department stores)
- eating and drinking establishments

- professional offices
- auto service center
- manufacturer direct-to-consumer automotive sales/leasing, for no more than one auto manufacturer, with a maximum of 5,000 square feet for a showroom within the Galleria containing no more than five (5) display vehicles, and no more than eight (8) vehicles combined for test drives/sale or lease purposes. Reserved parking spaces for such vehicles outside the display space within the Galleria shall be in a location not visible from public streets.
- banks and financial services
- child care
- movie theaters
- cultural centers, such as libraries and museums
- specialized education and training
- personal services
- multi-family residential uses

An alternative land use plan is proposed for the site not developed as the regional mall (Parcel 36). Parcel 36 uses may include up to sixty percent (60%) of the total gross building floor area of the site for retail commercial. The balance of the site will be used for non-retail uses such as business-professional office, hotels, restaurants, banks, child care, theaters, and other office and service commercial uses. Multiple family residential use may be considered subject to City approval of a General Plan Amendment and Specific Plan Amendment for additional units. Parcel 36 is subject to regional commercial policies as well as the business-professional and commercial policies that are appropriate to the ultimate land use mix, when such policies are applicable and do not conflict with the goals and policies of this section.

The eastern edge of Parcel 36 encompasses slope areas ranging from five (5) to twenty-five percent (25%) gradient and oak woodlands overlooking Antelope Creek. Although the site is adjacent to an area designated as a potential regional commercial center, it is not suitable for conventional office or commercial development due to the slope and tree conditions. Consequently, a majority of the slope will be included in a non-developable slope easement. Approximately 2.6 acres, designated as Parcel 33, will be accessible from a future connection from Antelope Creek Drive.

This parcel is illustrated on Figure 3-20, Slope Easement Area. The site is suitable for a restaurant, small office building, or specialty retail. The building placed on the parcel will require pedestrian access from a parking area at the top of the bluff. The building is required to be of a low profile design, not more than two stories in height on its downhill side, and must be visually integrated with the natural terrain. The height shall not exceed thirty-five feet (35') above the natural grade. Regulations relating to the development of Parcel 33 are contained in Policy 19 of this section.

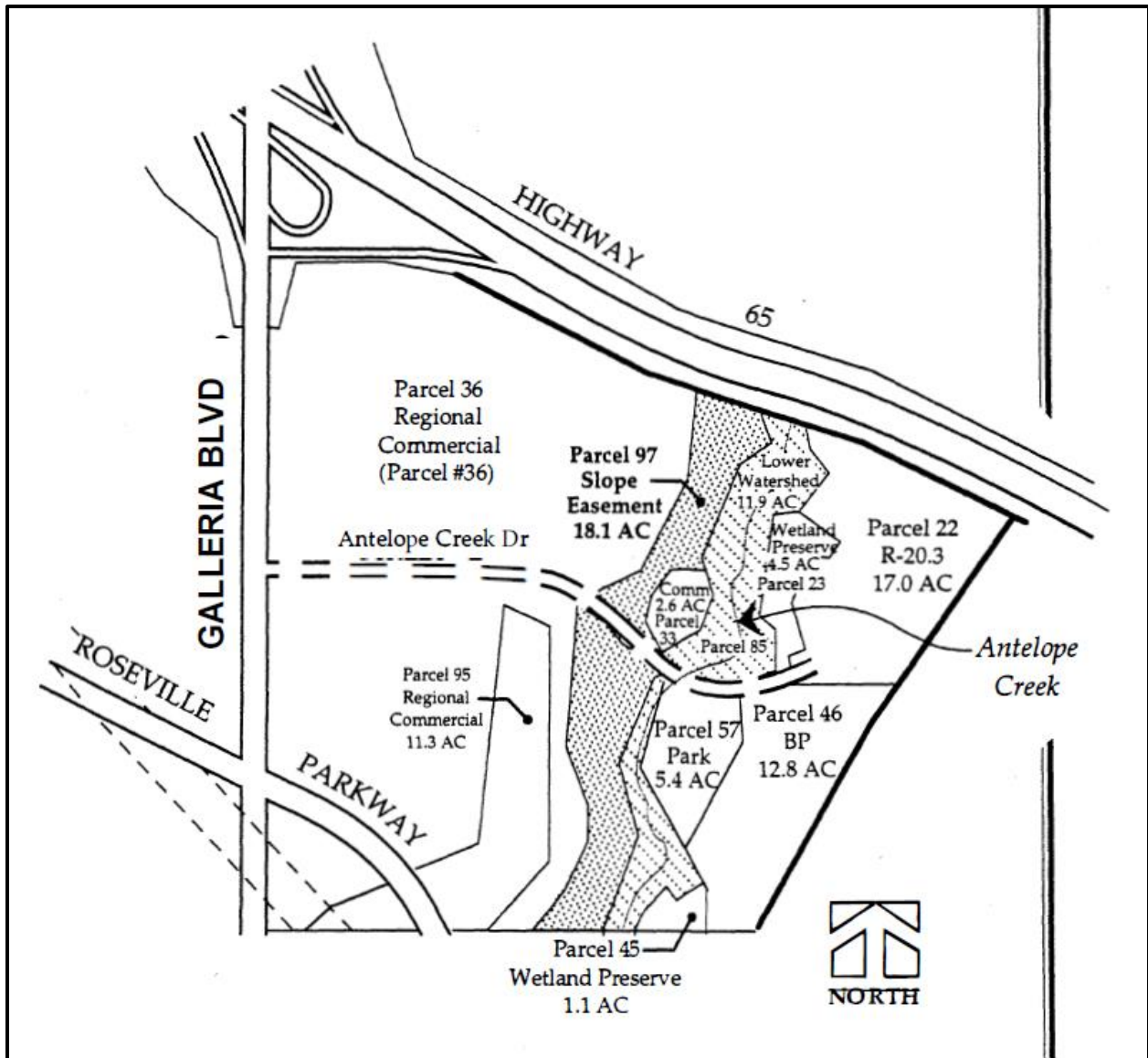


Figure 3-20: Slope Easement Area

Parcel 37 is a small commercial site of approximately five (5) acres separated from the regional commercial sites by Roseville Parkway. This site is suited for small freestanding commercial or service uses such as auto service, bank, restaurant, or small office or retail uses relating to the commercial core area. Much of the site is encumbered by a powerline easement, which limits building locations and coverage. Due to the proximity to the Roseville Parkway /Galleria Boulevard intersection, the property will be limited to right in/ right out vehicular turning movements.

The basic concept of the commercial core area is to make the two sides of Galleria Boulevard work together as a single urban complex. The features of such a complex include a highly efficient circulation system that accommodates pedestrians as well as vehicles and public transportation. A major emphasis of the core area is on the design of pedestrian spaces and connections.

The creation of an urban scale commercial core area is facilitated by the regional access that has been provided. The access is illustrated in Figure 3-21, Regional Commercial Core Circulation Diagram. The roadway system offers superior circulation for the core area via the Highway 65 /Galleria Boulevard, Atlantic Street/Eureka Road and Highway 65 /I-80 interchanges. The interchanges are supplemented by the major arterial linkages including Galleria Boulevard, Stanford Ranch Road and Roseville Parkway. The location also presents opportunities for potential future connections to regional public transit, including light rail. The site offers freeway visibility; proximity to an established and growing population base, a visually dramatic site, and a comprehensive master planning effort by the City. These factors provide a unique opportunity in the Sacramento /South Placer region to establish a "satellite downtown" complex.

The pedestrian circulation system, illustrated in Figure 3-22, Regional Commercial Core Pedestrian Circulation Diagram, will link the entire commercial core area around Galleria Boulevard. In addition, the walkways will extend to the adjacent business-professional/ commercial, business-professional, and multi-family residential uses immediately surrounding the commercial core. A connection will intersect with the planned bike trail along Antelope Creek which will link south into the developed portion of Roseville and north into Rocklin. The pathway system will have the effect of making the commercial core accessible to a larger pedestrian core area by providing safe and convenient connections.

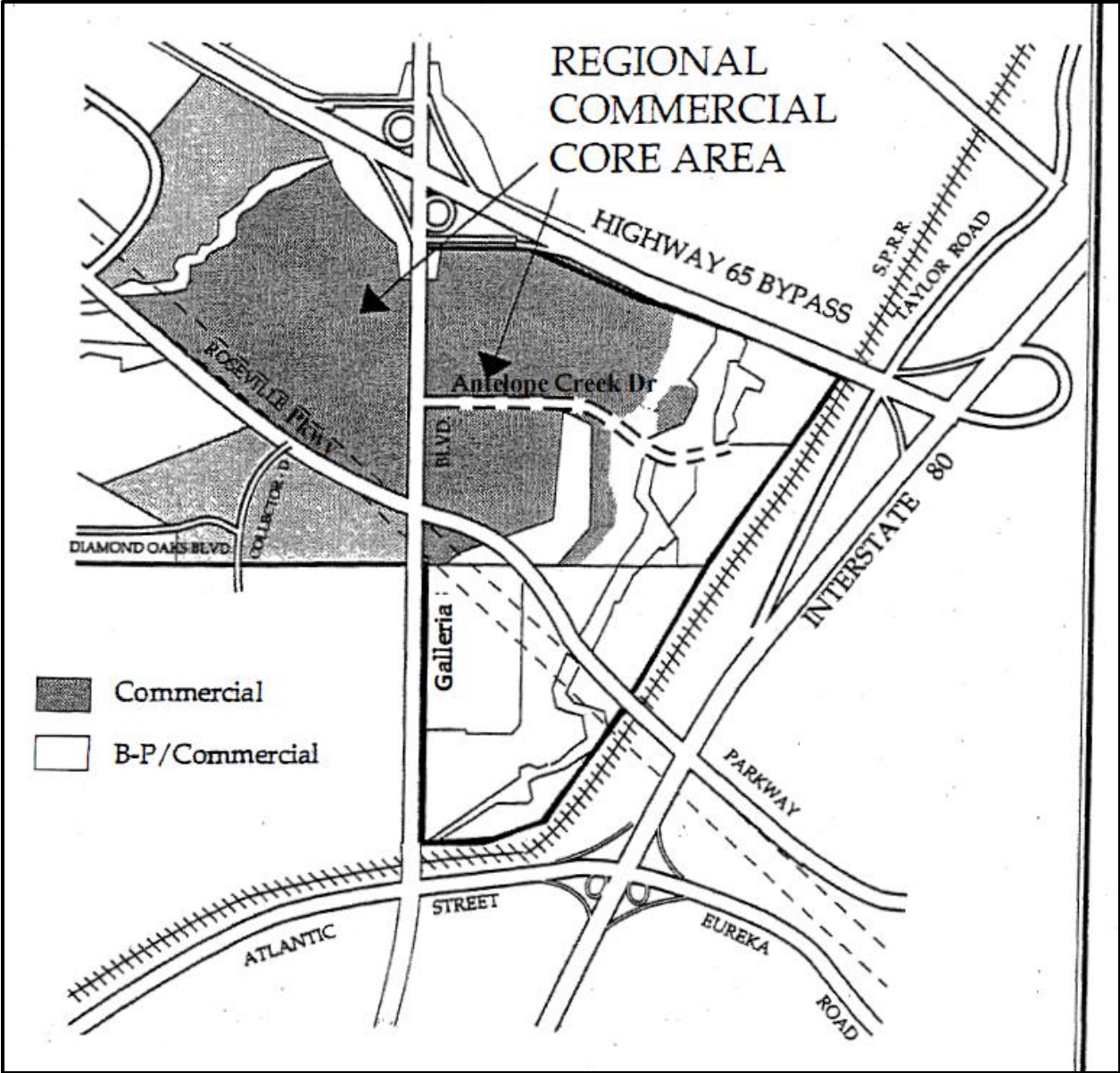


Figure 3-21: Regional Commercial Core Circulation Diagram

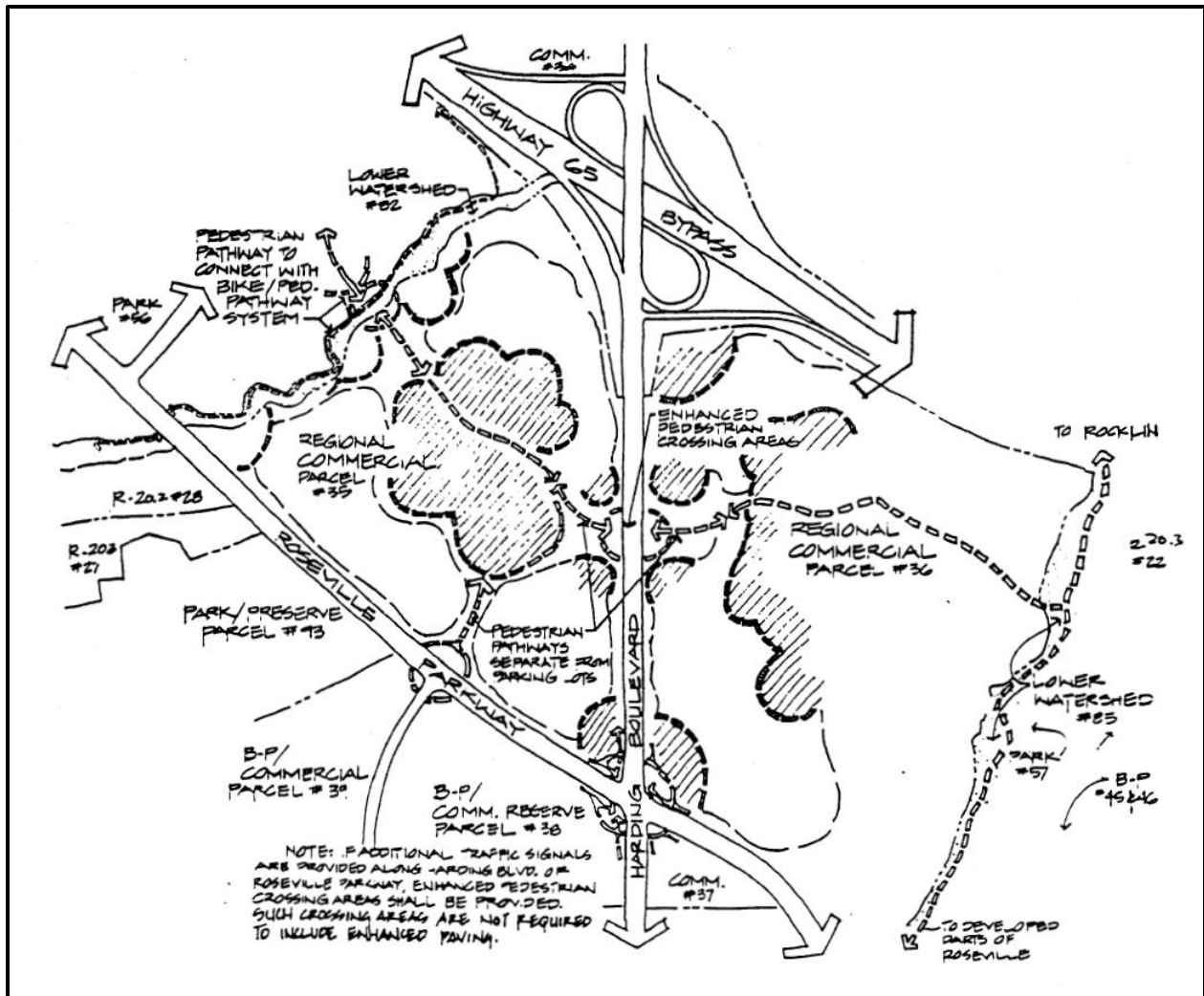


Figure 3-22: Regional Commercial Core Pedestrian Circulation Diagram

The pedestrian linkages connecting the various elements of the core area are conceived as "promenades" delineated by distinctive paving materials with landscaped edges. The intent is to clearly define and visually separate the pedestrian walkways from the surrounding vehicular circulation and parking areas. Figure 3-23, Sectional View of a Promenade, illustrates this concept. Canopy trees will shade the promenades from afternoon sun, and pedestrian scale lighting will make the areas a comfortable place to be in the evening. Small recessed "bays" along the promenades will provide spaces for rest area benches, for performing artists, and for small food vendor carts or other mobile sales activity licensed by the private commercial core management. Small shops providing flowers, fresh baked goods, and other conveniences may be spotted at irregular intervals along the promenades. The promenades will be designed to allow ready access from adjacent parking areas.

As they approach arterial connections, the promenades may be flanked by restaurants or retail activities typically found on pad sites near the street. This will provide additional pedestrian access to these uses and will enhance the character of the promenades. A conceptual illustrative plan diagram for a portion of a promenade is shown in Figure 3-24, Pedestrian Promenade Conceptual illustrative Plan Diagram.

A primary goal of the promenades will be to connect the uses on opposite sides of the adjacent arterials. Such uses are normally only conveniently and safely accessible from one shopping area to another by automobile. All signalized intersections adjacent to the regional commercial core area are required to provide enhanced pedestrian access.

The following policies and guidelines will be used to guide development of the regional commercial parcels. Additional guidelines contained in the general guidelines section of this element also apply:

1. Only one regional commercial mall shall be developed in the North Central Roseville Specific Plan Area.
2. The site developed as the regional mall may include any mix of uses typical to a regional commercial facility as specified in Section 3.8.
3. The land use for Parcel 36 may include up to sixty percent (60%) of the total gross building floor area of the site in retail commercial uses. The balance of the site will be used for business-professional offices, restaurants, banks, savings and loans, child care, hotels, movie theaters, and other office and non-retail service commercial uses. Multi-family residential use may be considered subject to a General Plan Amendment and Specific Plan Amendment resulting in an additional residential allocation by the City of Roseville.

Business-professional and commercial uses on Parcel 36 shall be subject to the business-professional and community commercial policies in this plan respectively, when such policies are applicable and do not conflict with the goals and policies in this section.

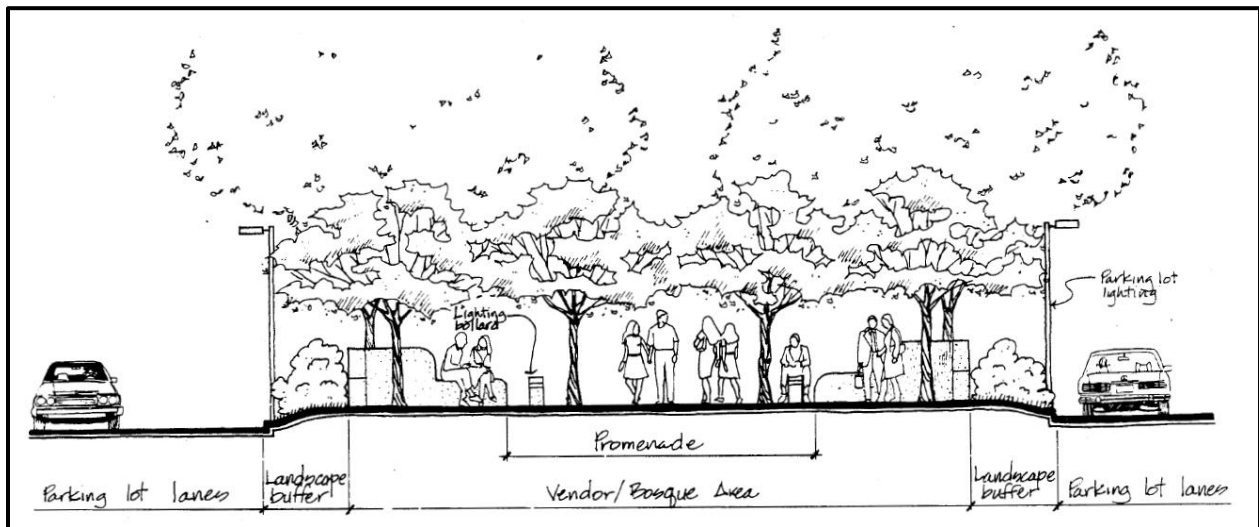


Figure 3-23: Sectional View of Promenade

4. Parcels 35 and 36 shall be master planned prior to receiving a use permit for development of any portion. The master plan shall indicate building locations, vehicular circulation and parking systems, pedestrian circulation systems, general grading and drainage plans, and the architectural signage and landscaping guidelines for the entire parcel.

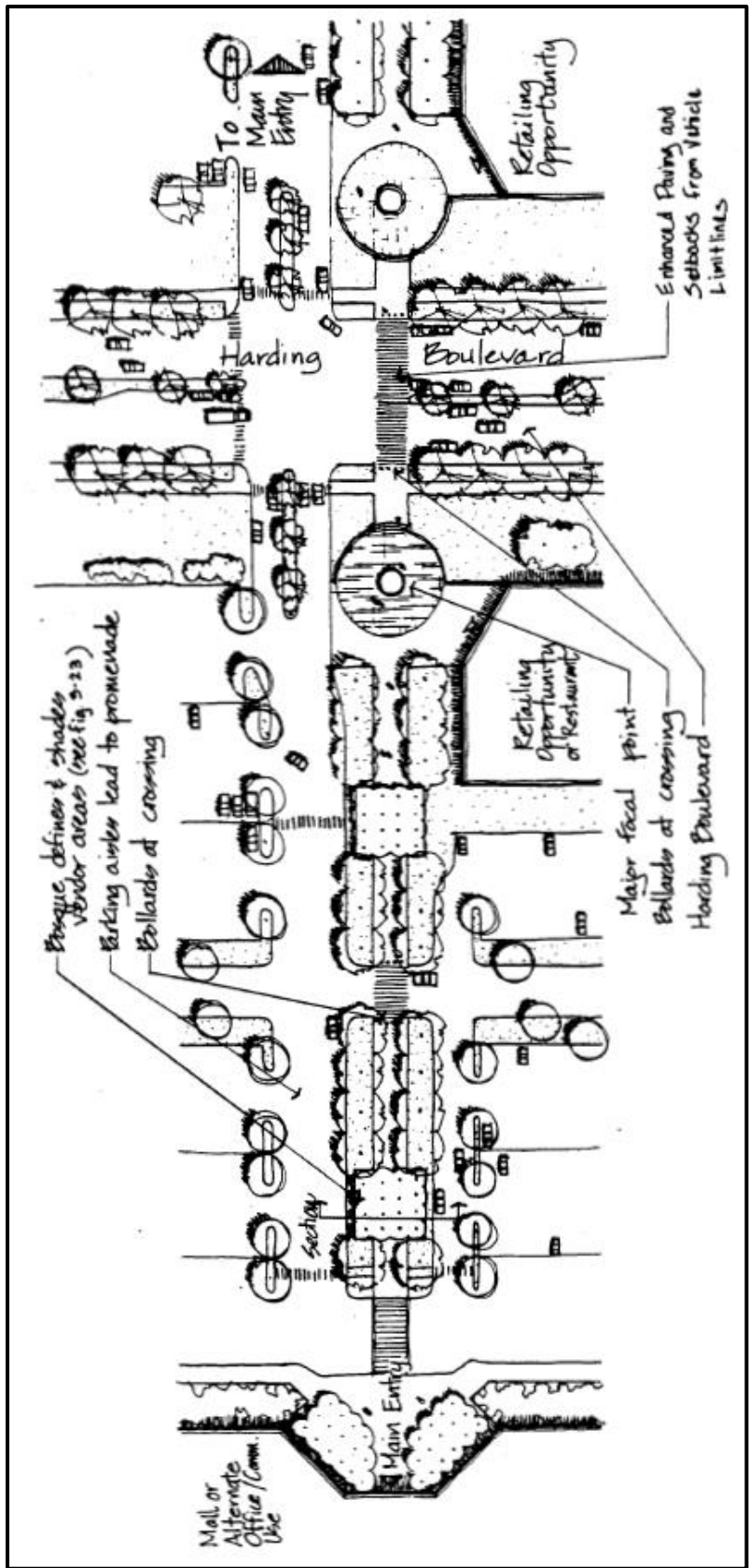


Figure 3-24: Pedestrian Promenade Conceptual Illustrative Plan Diagram

5. Parcels 35 and 36 shall have a single design theme for each parcel that is to be reflected in the primary buildings. Such a theme does not require rigid uniformity in all the buildings but does require compatibility between buildings. The theme elements shall include the use of certain colors, materials, certain architectural elements or style, or consistent landscaping treatment.
6. Building height is to be determined on a case-by-case basis through the Design Review Permit or Major Project Permit process.

Large, single-user freestanding retail commercial buildings which are not integrated in an overall site design are generally discouraged and shall not be the dominant form of commercial use.

Such uses shall be incorporated in an overall site master plan in which design measures are applied to visually reduce the bulk and large frontages typically associated with such uses.

7. Each parcel shall provide pedestrian pathway systems that connect to the adjacent parcels as illustrated in Figure 3-22, the Regional Commercial Core Pedestrian Circulation Diagram; 3-23, Sectional View of a Promenade and 3-24, Pedestrian Promenade Conceptual Illustrative Plan Diagram. The pedestrian pathway systems shall be separated from vehicle travelways and shall be designed in such a manner as to guide the pedestrians to the main building access points. Such access shall be coordinated with transit stops. The pedestrian connections shall be incorporated in the design of buildings.
8. The pedestrian pathways throughout the commercial core area shall utilize common design elements. These elements may consist of common paving colors and textures, design elements, or other methods of indicating that the pedestrian network is part of a continuous pathway that extends beyond the individual project. A common pedestrian scale lighting standard shall be selected for the entire length of the commercial core pedestrian pathway system. Common furniture, drinking fountains, trash receptacles, signage and other elements shall be used.
9. In order to strengthen the streetscape, pad sites should be located at the street frontage. Pads or buildings directly fronting roads shall provide direct pedestrian connections from the landscape corridor pathway along the adjacent street.
10. Bicycle parking racks shall be provided in highly visible locations.
11. The design of the regional commercial uses shall consider the possible future extension of light rail including right-of-way and station needs. Pedestrian connections shall be coordinated with transit stops. On-site transit stops, including those for buses, shall be considered during the design review stage. The site design for Parcels 35, 36 and 37 shall consider the possible future construction of a grade separated intersection at Roseville Parkway and Harding Boulevard.
12. Public plazas, outdoor dining areas, seating areas accessible to pedestrians, fountains, and other outdoor gathering places are strongly encouraged.
13. Gas stations, providing gasoline sales and limited auto service and repair, may be located in any regional commercial land use designation, subject to a use permit. Consideration should be given to orienting the service islands away from the street. The stations are to be designed as an integral part of the commercial site in terms of architectural treatment, materials, colors, building location and orientation, and parking lot circulation.
14. The interface between regional commercial and open space areas, such as the lower watershed (Parcel 82), wetland mitigation area (Parcels 95 and 96), slope easement (Parcel 97), and open space recreation (Parcel 98), shall be carefully considered during design review. The site design should, to the extent possible, incorporate the visual amenity provided by the adjacent open space. A minimum twenty foot (20') setback shall be provided from the edge of the open space

areas. The setback shall be landscaped and may include trees, berms, and swales provided to create a boundary and to control urban run-off. Fencing between the regional commercial use and the open space areas is generally discouraged, except as required by the permit issued by the U.S. Army Corps of Engineers pursuant to Section 404 of the Federal Clean Water Act. Where necessary, such fencing should be open type to allow for a view to the open space area. Building design shall consider views from the adjacent open space areas. In general, architectural treatment and materials for those frontages visible from adjacent open space areas shall be similar in treatment and materials to the building frontages visible from public streets.

15. The on-site landscaping for Parcels 35 and 36 shall provide a minimum twenty foot (20') landscape setback from the edge of the Caltrans right-of-way adjacent to Highway 65. Buildings shall not "back" onto the highway frontage.
16. Provide visually separated service entries for the delivery of merchandise.
17. Parcel 97 is designated as a non-developable slope easement. This area is to be protected and maintained as an oak woodland ecosystem in accordance with the tree policies contained in Section 6.2 of this Specific Plan and all applicable ordinances and policies of the City of Roseville. The slope area shall remain in private ownership and shall be maintained by the owners of Parcel 36 in the debris-free and non-fire hazard condition. The following additional activities may be considered as approved by the City:
  - a. Oak tree regeneration.
  - b. Passive recreation such as picnic areas or nature observation areas.
  - c. Pedestrian pathways including the pathway proposed adjacent to Antelope Creek.
  - d. A single access road between Parcel 36 and the parcels east of Antelope Creek.
18. The development of Parcel 33 will be subject to the additional following conditions:
  - a. The parking area design shall minimize the need for grading.
  - b. Buildings shall be low-profile and limited to two stories in height in any vertical plane measured from the downhill side of the building. The height shall not exceed thirty-five feet (35') above the natural grade on the downhill side of the building.
  - c. Buildings shall be visually integrated with the natural terrain in design, style, materials, and color.
  - d. Designs shall be utilized which minimize grading. Any slope banks shall be designed to blend with existing terrain and revegetated with native materials.
  - e. All oak trees are to be protected pursuant to the policies set forth in Section 6.2 of this Specific Plan and all applicable ordinances and policies of the City of Roseville.
  - f. All proposed uses and designs will be subject to review by the Project Review and Planning Commission.
19. Due to the powerline easements and access limitations, the development of Parcel 37 is limited to small commercial or service uses such as auto service, restaurants, banks, or small office or retail uses relating to the commercial core area.
20. The development of Parcel 33 will be subject to the additional following conditions:
  - a. The parking area design shall minimize the need for grading.
  - b. Any slopebanks shall be designed to blend with existing terrain and revegetated with native materials.

## BUSINESS-PROFESSIONAL LAND USES

### 3.9 Business-Professional Land Uses

The Highway 65 Bypass provides approximately 3 miles of highly visible freeway frontage as it traverses the Plan Area. Variations in topography on Parcels 42a and 43 provide direct freeway visibility and access which is particularly desirable to large, stand-alone users.

In addition to large stand alone uses, Parcels 42a and 43 provide an opportunity for an office park environment that would include multi-tenant buildings. Parks of this nature are well suited for research and development facilities and also provide an attractive business environment for unrelated businesses. Typically, projects will not only mix users in a single building, but will also mix business uses on a single site. Common recreation/ open space, landscaping, dining and meeting areas are amenity features which are desirable within all business-professional developments.

Typical uses permitted within the Business-Professional land use include:

- administrative and general office
- corporate or regional headquarters
- research facilities
- medical offices
- professional services such as attorneys, accountants and insurance

The business-professional land use also permits a limited amount of service commercial and retail activities provided for the convenience of the employees within the area. The goal is to provide a mix of basic services in close proximity to employees to reduce daily convenience trips. Typical service uses may include restaurants, banks, day care, personal services, such as hair care and shoe repair, specialty retail, and similar uses. Such uses may not account for more than ten percent (10%) of the total gross floor area allowed for the individual parcels ultimately defined within Parcels 42a, 43, , and 45. Convenience services within the business-professional land use should be distributed within the area to be easily accessible by employees. These uses should generally be located within the business-professional building complexes and not as freestanding structures.

The smaller business-professional site, Parcel 45, provide locations for uses oriented to the commercial core area. This site has the advantage of an attractive creekside woodland setting along Antelope Creek. Smaller scale, "garden office" type uses which incorporate the natural environment are appropriate on this parcel. Although the sites will have good visibility from Highway 65 and Taylor Road, access to the parcel is limited. Access will be provided from Harding Boulevard across Parcel 36 via Street E. The alignment of Street E will be dependent upon the development of Parcel 36 and the final delineation of wetlands to be preserved within the parcel. The easement for Street E will be located at the time of development of Parcel 36 and may affect the timing of development of Parcel 45.

The interface between business-professional and multi-family residential uses (Parcels 21a and 43, and 21b and 42b) will typically be defined by berms and landscaping with "barrier" plant materials. An alternative interface would be landscaping and a fence. Pedestrian cross access shall be provided in accordance with Policy 11 in this section. The use of landscaping and berms rather than fencing is encouraged where feasible.

The following guidelines apply to the development of business-professional uses. Additional policies contained in the General Design Guidelines section of this element also apply:

1. Buildings shall be of an architectural design and character to provide the quality image desired by corporate and professional users. Business-professional buildings are to have unified design utilizing consistent building material, architectural style, textures, detail, landscaping, and signage.
2. Corporate buildings adjacent to Roseville Parkway or Highway 65 may be of larger scale, a minimum of two stories in height, and may have an individual image. The building image is to be compatible with adjacent structures. The ultimate height of such buildings shall be determined by the City on a case by case basis through the design review process.
3. Provide visually separated service entries for the delivery of merchandise.
4. Provide secured bike storage, lockers, and showers for employee utilization as an option to comply with the City Rideshare Ordinance.
5. Pedestrian plazas with landscaping, seating, drinking fountains, and points of interest such as water elements or art sculptures are strongly encouraged.
6. To ensure pedestrian access and clear connections to pedestrian pathways in the landscape corridors along the adjacent roadways and open space corridors, access connections should be provided at regular intervals. Pedestrian pathway access connections shall be provided along the perimeter of the project to adjacent pedestrian pathways at an average of not less than 600 linear feet, and in no instance greater than 1000 linear feet. The pedestrian pathway connections shall be separated from vehicular circulation and shall guide the pedestrians to the building accesses of each building. Such walkways are to be designed similar to Figure 3-18, Commercial/Open Space Overlook illustration. A minimum of one connection point per street or open space frontage shall be provided. The location of pedestrian access connections shall be coordinated with the location of transit stops to facilitate the use of public transit. The connections from the interior sidewalks to the pedestrian pathways shall be the responsibility of the developer.
7. Pedestrian access should be provided between individual business-professional projects and between business-professional and other uses to promote pedestrian access when practical.
8. Buildings directly fronting roads shall provide direct pedestrian connections to the landscape corridor pathway along the adjacent roadway.
9. The design of business-professional uses adjacent to designated light rail routes shall consider the possible future extension of light rail, including right-of-way and station needs. On-site transit stops, including those for buses, shall be considered during the design review stage.
10. A minimum forty foot (40') landscape easement shall be provided in Parcels 42 and 43 adjacent to Highway 65. The portion of the easement across Parcel 42 will accommodate an ten foot (10') wide pedestrian/bikeway connecting the bike trail in the Parcel 83 lower watershed preserve with the bike trail in the Parcel 40 landscape corridor along the shared boundary with Parcel 42. The landscape corridor located along the common property line between Parcels 42 and 40 shall consist of a ten foot (10') wide landscaped setback interior to Parcel 42, a ten foot (10') wide bike trail with two foot (2') shoulders immediately interior to Parcel 40 plus an additional eleven foot (11') wide landscaped setback along the bike trail's eastern edge, as shown in Figure 3-25c. In the alternative, the City may require a meandering bike trail alignment which crosses over the common boundary of Parcels 40 and 42, in which case a twenty foot (20') bikeway/landscape

corridor will be required immediately interior to Parcel 42. A twenty five foot (25') landscape easement shall be provided along the Parcel 40 Gibson Drive frontage, as shown in Figure 3-25d, in order to accommodate a ten foot (10') wide bike trail linking the bike trail in the landscape corridor along the common boundary of Parcels 40 and 42 with the bike trail from the regional mall site located in the fifty foot (50') wide landscape corridor along the Roseville Parkway frontage of Parcels 40 and 35. The bicycle pathway is to be installed as individual projects are built. The two foot (2') shoulder area adjacent to the bike trail shall not include any shrubs or thorny groundcover. Projects adjacent to the pathway shall provide connections to facilitate pedestrian access at not more than an average of 600 feet apart. A minimum of one connection point per building adjacent to the corridor shall be made. All building designs shall consider Highway 65 as a primary frontage.

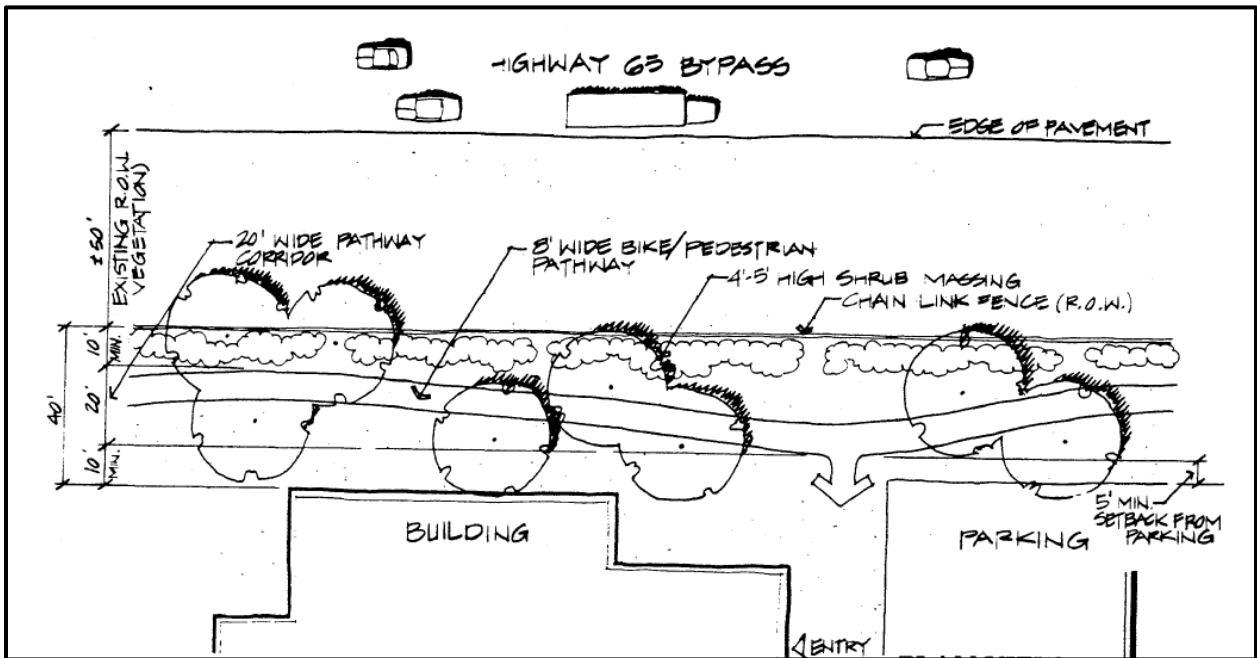


Figure 3-25a: PLAN VIEW - Bikeway Easement Adjacent to Highway 65 Bypass

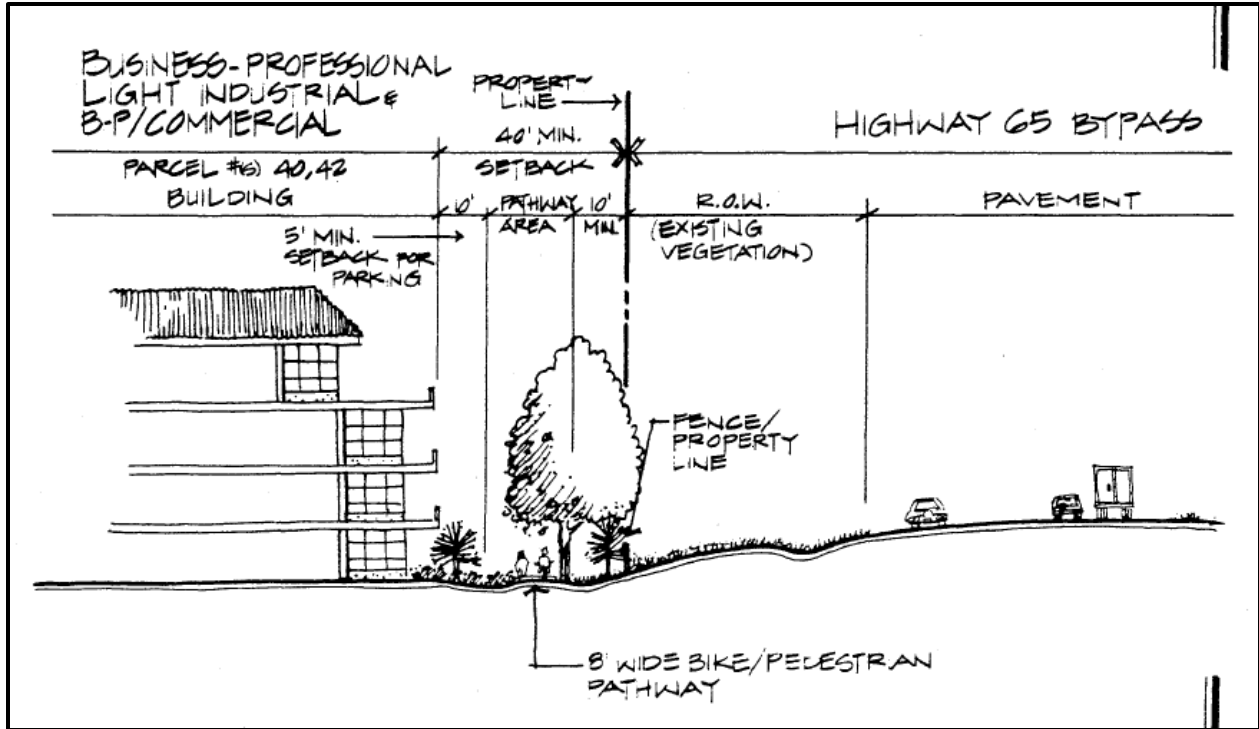


Figure 3-25b: SECTION VIEW – Bikeway Easement Adjacent to Highway 65 Bypass

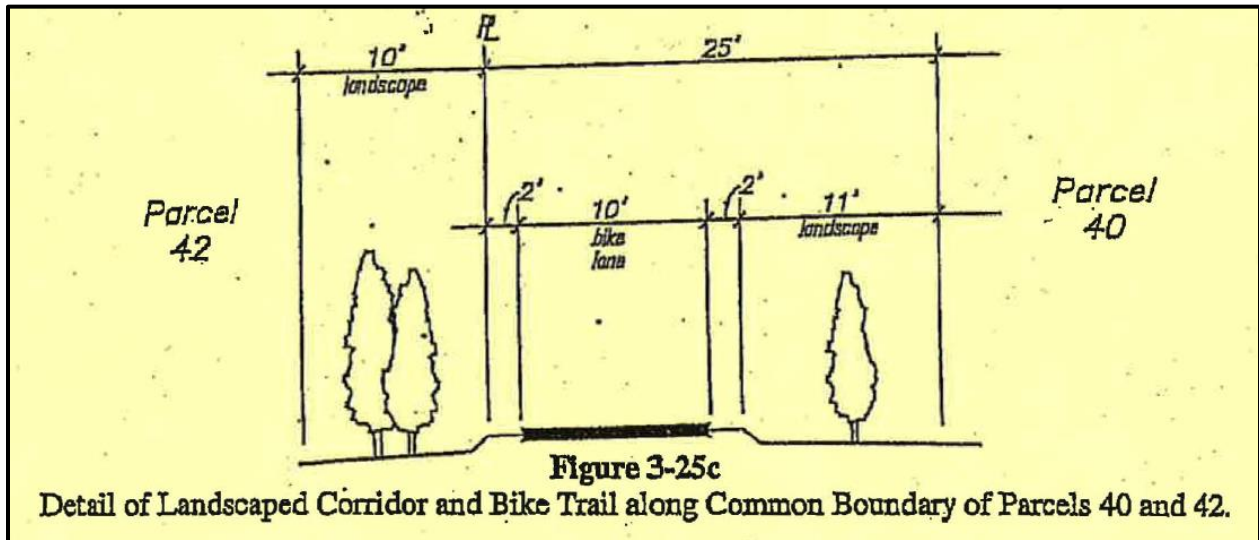


Figure 3-25c: Detail of Landscape Corridor and Bike Trail along Common Boundary of Parcels 40 and 42

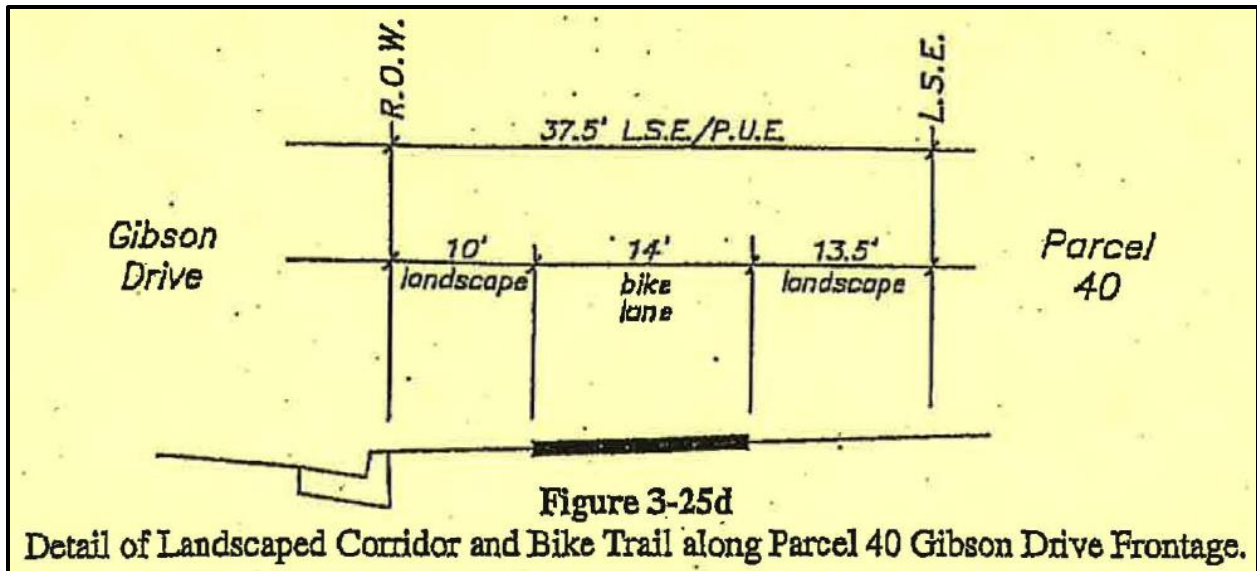


Figure 3-25d: Detail of Landscaped Corridor and Bike Trail along Parcel 40 Gibson Drive Frontage

11. Business-professional development should utilize open space, including watersheds and parks, as a visual amenity where feasible. A minimum twenty foot (20') setback shall be provided from the edge of the open space area. Such setbacks shall be landscaped and may include berms and swales to create a boundary and control drainage. Landscaping within the setback area shall be designed such that trees, when mature, may provide intermittent shade over the adjacent bikeway. Fencing adjacent to open space is discouraged. When necessary, such fencing should be an open type which allows a view to the open space area. Pedestrian connections to the open space areas and pathway systems shall be made by the developer to facilitate pedestrian access. In general, architectural treatment and materials for the building frontage visible from the open space areas shall be the same as used on the building frontage visible from public streets. All delivery and storage areas shall be screened.
12. Buffering between business-professional uses and multi-family residential uses shall be provided through setbacks, soundwalls or fencing, landscaping, or a combination thereof. The use of setbacks and landscaping with berms, rather than fences and soundwalls, is encouraged. Buildings shall be set back a sufficient distance and be designed to minimize impacts on adjacent residential units to the extent practical. The building setback will vary dependent upon building height and bulk, type of user, land topography. In general, a minimum building setback of twenty feet (20') for two story structures shall be provided. Buildings over two stories may be required to have increased setbacks. Trash enclosures, noise generating equipment, and other nuisances shall be located away from the adjacent residential use.
13. **DELETED 1/96.**
14. Parcel 45 presents unique conditions due to the proximity of the railroad tracks and limited access. This parcel is accessible only from a public street (Street E) connecting from Harding Boulevard across Parcel 36. Access to Parcel 45 is to be provided across Parcel 46 and the planning for both parcels must be coordinated to ensure access. Consequently, additional policies are required.

- a. The alignment of Street E will be dependent upon development of Parcel 36 and the final delineation of wetlands on the parcel. Street E alignment may impact the timing of development of Parcels 45. Parcel 46 shall provide access to Parcel 45.
  - b.
  - c. The building design should provide noise level reduction measures to meet interior noise level standards as required by the noise analysis.
  - d. Emergency vehicle access shall be provided and maintained by the developer between Parcels 45 and 46 to Springview Road to the north through Parcel 22 and under the highway 65 Bypass overcrossing. Such access shall be provided prior to or concurrent with the development of either Parcel 45 or 46.
  - e. Buildings shall be oriented towards and relate to the adjacent park (Parcel 57) and watershed area (Parcel 85). Pedestrian linkages from the business-professional uses to the park, watershed, and pedestrian pathways shall be provided.
15. Restaurants, banks, day care, personal services such as beautician, shoe repair, and florist, specialty retail, and other uses provided for the convenience of the employees may be included in business-professional uses. Such uses shall not account for more than ten percent (10%) of the total gross floor area of any given parcel ultimately defined within Parcels 42, 42b, 43, and 45. These uses should generally be located within the business-professional building complexes and not as freestanding structures. All service uses should be easily accessible by employees and pedestrians.

Additional Parcel 18c Policies:

- 16. For Parcel 18c, all site lighting shall utilize cut-off lenses and the maximum height of pole lights shall be 20 feet.
- 17. On Parcel 18c, parking, driveways, and landscaping are permitted in the minimum building setback of 100 feet from the west property line for two and three story buildings.
- 18. Site grading for Parcel 18c shall be sensitive to the nearby residential subdivisions. Cut and fill slopes shall be designed to maximize the effect of landscape screening, and retaining walls may be utilized where necessary to enhance the relationship between the properties.
- 19. Building materials for Parcel 18c should be designed to give consideration to the nearby homes and the potential for reflective light and glare.
- 20. Evergreen screen trees shall be planted at 20 feet on center within the landscape planter at the west edge of Parcel 18c to screen views to the site from adjacent residential subdivisions, unless an alternative spacing is approved by the city based upon the recommendations of an arborist or landscape architect. In addition, shrubbery shall be planted to minimize views to parking areas.

### 3.10 Business-Professional/Commercial Land Use

The business-professional/commercial land use is intended to provide a flexible mix of uses that are not usually found in a conventional office or commercial setting. For example, small professional offices may be mixed with specialty retail, restaurants, or leisure activities to serve the Plan Area residents, as well as the employees and visitors in the area. The uses permitted within the business-professional/commercial land use include:

- small administrative and general offices

- medical and dental offices
- professional services such as attorneys, accountants, and insurance agents
- restaurants
- hotels
- financial institutions
- bakeries
- hairdresser and related services
- travel agencies
- retail such as a book or stationery store, specialty clothing, shoes, and jewelry

The business-professional/ commercial parcels may include a gas station designed as an integral part of the entire site.

The design of business-professional/ commercial uses is intended to have an office or mixed use character rather than that reflective of a conventional community commercial use. In order to maintain an office/mixed use character, the retail commercial uses shall not exceed sixty percent (60%) of the total gross floor area in any ultimately defined business-professional/commercial parcel, with the exception of Parcels 38, 39A, and 39B, where the 60% retail commercial restriction shall not apply.

Parcels 39 A and 39B may accommodate any of the uses permitted for business-professional/ commercial. Due to the proximity of the residential neighborhood, the more intense uses should be located to the northern portion of Parcel 39A near Roseville Parkway. Site design and building treatment shall comply with Policy 3 of this section.

Parcel 41 is a relatively small site bounded by the lower watershed wetland area and the intersection of Roseville Parkway and Pleasant Grove Boulevard. Suitable access can be achieved by limiting the turning movements to right turn only ingress and egress. The size, configuration, and access limitations make this site most appropriate for a number of smaller uses such as a service station, restaurant, or small office building.

Parcel 40 could accommodate larger users, such as support retail, a hotel or conference center, and in addition to the uses listed, a performing arts center and other cultural facilities. A Conceptual Site Plan (Figure 3-25e) illustrates how a portion of Parcel 40 could develop within an Office/Retail Complex. Site development could consist of several office and retail buildings, ranging in height from one to ten stories, supported by a combination of at-grade and structured parking. A conceptual site plan (Figure 3-25d) illustrates how a portion of Parcel 40 (Parcel 40a) could be developed with a hotel conference center. The center could consist of several buildings clustered together, ranging in height from one to ten stories, that include conference facilities, hotels, and restaurant services. Parking may be provided by a combination of at grade and structured parking. Street "A" located off of Gibson Drive will connect the hotel conference center with the rest of the Parcel 40. While the plans conceptually illustrate how Parcel 40 could develop, actual site design and location, size, and orientation of buildings will be subject to review and approval by the City when a formal development application is submitted.

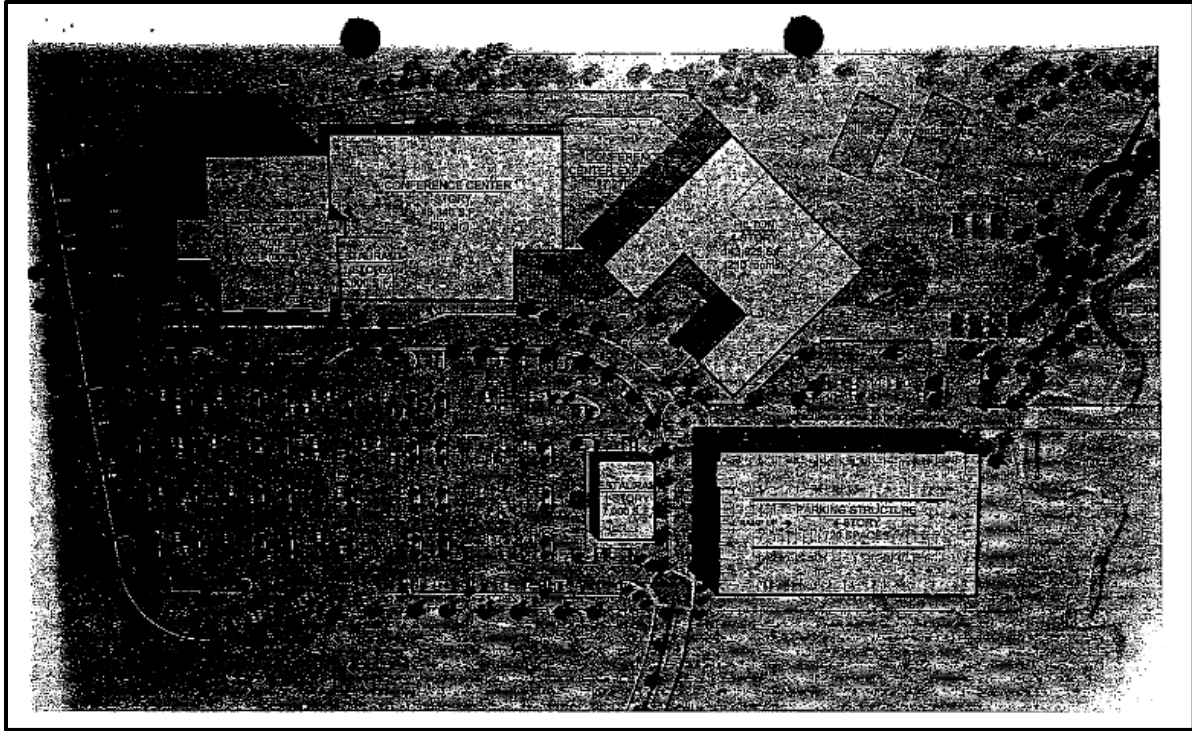


Figure 3-25d: Conceptual Site Plan - Hotel Conference Center (Parcel 40A)

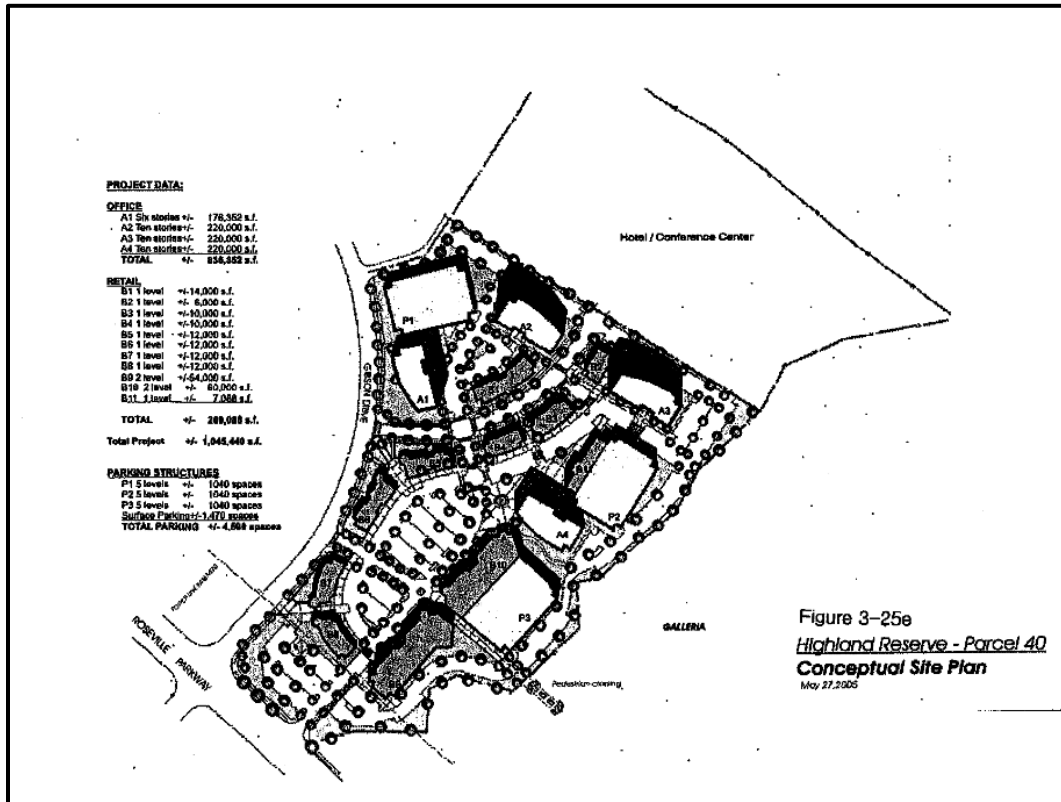


Figure 3-25e  
Highland Reserve - Parcel 40  
Conceptual Site Plan  
May 27, 2005

Figure 3-25e: Highland Reserve - Parcel 40 Conceptual Site Plan (May 27, 2005)

Development in the business-professional/ commercial land use will be subject to the applicable policies established for the business-professional uses and the General Design Guidelines. In addition, the following policies shall apply:

1. Business-professional/ commercial uses shall be subject to the same guidelines and policies established for business-professional uses. Some guidelines in the Community Commercial section of this Specific Plan may be applied to the commercial portion of a business-professional/ commercial project, when such guidelines do not conflict with the objectives of this section. The intent is to create a project that has the character of a mixed use office project rather than a commercial center. Of particular concern are the interfaces between uses and the provision of pedestrian access. The interface between business-professional/ commercial uses and open space, including watersheds (Parcels 82 and 84), park preserve (Parcel 93), business-professional (Parcel 42), and the Highway 65 Bypass, shall be as specified for business-professional uses (Section 3.8).
2. In order to maintain an office/mixed use character, a maximum of sixty percent (60%) of the gross floor area of any ultimately defined business-professional/ commercial parcel may be used for retail commercial uses, with the exception of Parcels 39, 39A, and 39B, where the 60% retail commercial restriction shall not apply.
3. The interface between Parcels 39A and 39B and the adjacent residential (Parcel 13) shall be carefully considered. Pedestrian access between the uses shall be provided through the park/preserve (Parcel 93) as illustrated in Figure 3-10b. Additional access points shall be considered during the design review of both projects. A six foot (6') masonry wall, with a minimum fifteen foot (15') planter strip incorporating screen trees and barrier shrubs, shall be located on Parcels 39A and 39B adjacent to Parcel 13. Buildings shall be designed to minimize the impacts on adjacent residential units to the extent practical. Building heights shall not exceed two stories adjacent to Parcel 13. Buildings shall be set back a minimum of fifty feet (50') for two-story buildings and a minimum of thirty-five feet (35') for single story. Trash enclosures, noise generating equipment and other nuisances shall be located away from Parcel 13. Higher intensity uses should be positioned towards Roseville Parkway on the northern portion of Parcel 39 A.
4. Development on Parcel 41 shall reflect the relatively small size and limited access to the site, and the potential for a grade separated interchange at Pleasant Grove Boulevard and Roseville Parkway. This site shall be limited to right turn only ingress and egress.
5. In addition to the standard uses permitted for the business-professional/ commercial land use, the following requirements shall apply to Parcel 40:
  - Parcel 40 may be developed with large users including retail, hotels, conference center, or cultural facilities such as a theater arts center or concert hall, designed to be oriented towards the commercial core area.
  - Parcel 40 has been subdivided into four (4) parcels (40A, 40B, 40C, & 40D). Parcel 40A is intended for development of a hotel/conference center facility. The remaining parcels comprising Parcel 40 (40A, 40B, & 40C) may be developed solely with commercial or office uses, or any combination thereof, provided that the cumulative total of commercial uses on Parcels 40B, 40C, & 40D shall not exceed 334,540 square feet and uses on Parcel 40A shall not affect the mix, intensity or developable square footage of

land uses on Parcels 40B, 40C, & 400, as further defined in the Development Agreement provisions governing development of Parcel 40.

- Buildings on Parcel 40, or any subdivided portion thereof, may have a maximum floor area ratio of 0.80, as more fully set forth in the Development Agreement affecting Parcel 40.
  - Office and hotel buildings on Parcel 40, or any subdivided portion thereof shall not exceed ten (10) stories in height.
  - Parcel 40 may provide pedestrian linkages to the regional commercial site to the south (Parcel 35), over open space Parcel 82, as more fully set forth in the Easement Agreement affecting these properties.
  - Parcel 40 shall provide clear pedestrian linkages from the commercial core area to adjacent business-professional uses (Parcel 42) and community park (Parcel 56)
6. Gas stations providing gasoline sales and limited auto service and repair may be located in any business-professional commercial designation, subject to a use permit. Consideration should be given to orienting the service islands away from the street. The stations are to be designed as an integral part of the business-professional/ commercial site in terms of architectural treatment, materials, colors, building location and orientation, and parking lot circulation.

Pursuant to a use permit approval, the designated land use for Parcel 38 will be subject to the business-professional/ commercial design guidelines of this Specific Plan. Development of the site will be subject to the policies applicable to all other business-professional/ commercial uses in the Plan Area.

## LIGHT INDUSTRIAL LAND USE

### 3.12 Light Industrial

A total of 67.76 acres (Parcels 49a and Parcels 48b and 49b) on the northwest end of the Plan Area have been designated for light industrial use. The parcels are bordered by the Highway 65 Bypass (or for Parcel 48b, Parcel 48a) to the north, Washington Boulevard to the west, Parcel 84 to the east and the fifty-foot (50') wide open space/bike trail corridors which separate the light industrial uses from the single-family uses to the south. The Highway 65 corridor has been established as a light industrial center for South Placer county, and particularly the cities of Roseville and Rocklin. The light industrial land use planned in the North Central Specific Plan Area provides ease of access for truck and employee traffic, and maintains continuity with the existing industrial land use pattern along the Highway 65 corridor. The industrial area is separated and adequately buffered from residential uses by a fifty-foot (50') wide pedestrian/bike corridor. Soundwalls may also be required if at the time of development of the light industrial property a noise analysis indicates the need. Pedestrian connections will be provided between the light industrial and residential uses through adjacent park, lower watershed and/or landscape corridors.

The City of Roseville General Plan states that the intent of the Light Industrial category is to allow intensive industries with relatively high numbers of employees per acre. However, the experience in the South Placer region indicates that light industrial land use encompasses a broad range of industrial activity including high technology manufacturing, warehousing and distribution. It is anticipated that the light industrial land use in the Specific Plan Area will also attract a mix of industrial and business park/research and development activities, plus supporting retail uses.

The light industrial areas in the North Central Specific Plan area may attract both large primary users and smaller uses. The large users may include high technology manufactures or research and development operations. The smaller uses will provide services for the larger industrial land uses, other businesses and the general public along the Highway 65 corridor. Small light industrial employers may provide a significant portion of the manufacturing and warehousing employment base. The users will require small parcels or sites coordinated into a light industrial setting. The light industrial parks will be developed as planned developments that incorporate a common design, landscaping, and signage program to ensure a continuity of appearance and efficient circulation.

Light industrial areas shall be designed to ensure the proper development and use of land in a manner so as to achieve a high quality, nuisance free environment for manufacturing, assembly, research and development, business park, warehousing and distribution type land uses.

The purpose of the Planned Development (PD) zone on Parcel 49a is to enhance the ability of the NCRSP to land a major corporate research and development user, while at the same time providing the flexibility to attract corporate users with major office needs. The PD zone also will allow retail sales and services, including lodging and a range of restaurant uses, in close proximity to employment centers on Parcels 48a and 49a.

A maximum of twenty percent of the building square footage on Parcel 49a shall be permitted to have retail uses as specified in the PD ordinance for Parcel 49a.

The following uses designated as "P" or "CUP", are permitted or conditionally permitted in the Light Industrial designation. In addition, prohibited uses are designated as "-":

<b>AGRICULTURAL AND OPEN SPACE USE TYPES</b>	
Resource Protection and Restoration	P
Resource Related Recreation	P
<b>CIVIC USE TYPES</b>	
Community Assembly	P
Community Services	P
Essential Services	P
Social Services (1)	CUP
Intensive Public Facilities	CUP
Public Parking Services	P
Schools, Private Elementary and Secondary	P
<b>RESIDENTIAL USE TYPES</b>	
Caretaker/Employee Housing	P
<b>COMMERCIAL USE TYPES</b>	
Adult Business Establishments	CUP
<b>Animal Sales and Services</b>	
Kennels	P
Veterinary Clinic	P
Veterinary Hospital	P
<b>Automotive Equipment</b>	
Automotive Rental	CUP
Automotive Repairs	P
<b>COMMERCIAL USE TYPES</b>	
Automotive Sales (2)	CUP
Car Wash and Detailing	P
Commercial Parking	P
Equipment Rental, Repair and Sales	P
Gasoline Sales	P
Banks and Financial Services	P
Bars and Drinking Places (2)	CUP
Broadcasting and Recording Studios	P
Building Materials Stores	P
Business Support Services	P
<b>Commercial Recreation</b>	
Indoor Entertainment (2)	P
Indoor Sports and Recreation (2)	P
Outdoor Entertainments (2)	CUP
Outdoor Sports Recreation (2)	CUP
Large Amusement Complexes (2)	CUP
<b>Eating and Drinking Establishments</b>	
Convenience (2)	P
Fast Food with Drive Through	P

Full Service (2)	P
Food and Beverage Retail Sales (2)	P
Lodging Services	P
Maintenance and Repair	P
Nursery, Retail	P
Offices, Professional	P
Personal Services (2)	P
Retail Sales and Services (2)	P
<b>Specialized Education and Training</b>	
Colleges and Universities	P
Vocational Schools	P
Specialty Schools	P
Storage, Personal Storage Facility	P
<b>INDUSTRIAL USE TYPES</b>	
Day Care Center, Secondary (employees only)	CUP
Equipment and Materials Storage Yards	CUP
General Industrial	CUP
Hazardous Materials Handling	CUP
Laundries, Commercial	P
Light Manufacturing	P
Printing & Publishing	P
<b>Recycling, Scrap and Dismantling</b>	P
Enclosed	P
Unenclosed	CUP
Research Services	P
Specialized Industrial	P
Wholesale Distribution	
Light	P
Heavy	CUP
<b>TRANSPORTATION AND COMMUNICATION USE TYPE</b>	
<b>Antennas and Communications Facilities (3)</b>	
Developed Lot	P
Undeveloped Lot	CUP
Heliport	P
Intermodal Facilities	P

Note:

(1) Additional requirements are contained in Chapter 19.38, 19.39, and 19.40

(2) Subject to limitation that retail use shall not exceed twenty percent of the building square footage on a cumulatively basis on Parcel 49A.

(3) Additional requirements are contained in Chapter 19.34. (Ord. 3046§2 (part), 1996: Ord. 3014 (part), 1996.)

Commercial support uses are permitted where appropriate and there is no conflict with planned industrial development. Such uses are intended to serve the employees in the light industrial area and thereby provide a higher quality working environment. Such uses will reduce the need for travel out of the area for goods and services needed by employees in the course of a normal work day. Typical commercial support uses may include day care, eating establishments, specialty retail, retail sales and services, food and beverage sales, personal services, such as hair care, and recreation, such as health clubs. Convenience services within the light industrial uses should be distributed to be easily accessible by pedestrians.

The following uses are permitted in the Light Industrial designation:

- Uses primarily engaged in research and development (R & D) activities, including but not limited to research and development laboratories and facilities and compatible light manufacturing.
- Uses primarily engaged in manufacture, assembly, packaging, testing and/ or repair of components, devices, equipment, systems and parts.
- Service operations, including delivery, radio and television repair, security service, print shops.
- Offices shall be allowed, but should not exceed thirty percent (30%) of the total gross floor area in any ultimately defined light industrial parcel.
- Warehousing and distribution facilities are permitted provided that such activities are conducted wholly within a completely enclosed building.
- Mail order businesses
- Mini-storage

The following additional uses may be permitted subject to the approval of a special use permit by the City.

- cabinet and upholstery shops
- dry cleaning plants, rug cleaning plants and laundries
- caretaker dwellings
- recycling centers
- veterinary hospitals

Commercial support uses are permitted where appropriate and there is no conflict with planned industrial development. Such uses are intended to serve the employees in the light industrial area and thereby provide a higher quality working environment. Such uses will reduce the need for travel out of the area for goods and services needed by employees in the course of a normal work day. Typical commercial support uses may include day care, eating establishments, specialty retail, personal services, such as hair care, and recreation, such as health clubs. Convenience services within the light industrial uses should be distributed to be easily accessible by pedestrians. The commercial support uses shall not exceed ten percent (10%) of the total gross floor area in any ultimately defined light industrial parcel. These uses should generally be located within light industrial building complexes, and not as free standing structures.

All uses within the light industrial designation will be subject to a use permit to ensure that impacts from noise, light, vibration, odor, and other factors do not exceed City standards and will not cause adverse impacts on nearby uses.

The following policies shall apply to all light industrial land use. Additional policies contained in the General Design Guidelines section of this element also apply:

1. All allowed uses and accessory activities shall be conducted wholly within a completely enclosed building with the exception of off-street parking spaces, off-street loading facilities, open storage areas (as specified in -Policy 2), and employee recreational facilities. Activities shall minimize noise, fumes, smoke, dust, or other environmental pollutants.
2. Incidental open storage of materials, goods, parts and equipment, including company owned or operated trucks and other motor vehicles, is permitted provided that all such activities shall be screened by a solid fence or masonry wall no less than six feet (6') in height and by landscaping and earth berms. No stored materials, goods, parts or equipment should be visible from any adjacent public streets or highways, wetland preserve areas, or adjacent residential properties.
3. No noise may be generated that exceeds 60 dba at the edge of the light industrial parcels. Outside phone and paging systems are prohibited.
4. The use of toxic or hazardous materials requiring the filing of a business plan, for emergency response pursuant to Section 25503.5 of the California Health and Safety Code or materials identified in Section 5194, Title 8 of the California Code of Regulations, shall be critically analyzed by the City when considering any use and shall be subject to the approval of a special use permit.
5. All users shall submit a list of hazardous and toxic materials with a qualified discussion of potential chronic and acute long term health hazards and toxicological effects, including those on children, from acute short-term or chronic long-term exposures. In addition, a plan shall be submitted specifying procedures for mitigating the emissions of toxic substances and ground water monitoring, and for identifying methods of hazardous waste disposal.
6. Loading facilities shall be designed as an integral part of the building which they serve and shall be located in the most inconspicuous manner possible. No loading facility, including incidental parking and maneuvering areas, shall extend into any required minimum setback areas.
7. All roof mounted mechanical equipment shall be screened so as to not be visible from any public street or highway, or area accessible to the general public. This is a particular concern because a portion of the light industrial area lies below the elevation of the adjacent Highway 65 Bypass. Special treatment of the roof is encouraged and may be required when buildings are below the level of adjacent streets, highway, or public areas.
8. A secondary street system shall be provided within the light industrial areas (Parcels 48 and 49) to provide access, to individual parcels. The street cross section shall include twenty-five foot (25') landscape corridors as specified for non-residential collectors in the Circulation Element. Common access driveways shall be utilized to the maximum extent possible in order to reduce circulation conflicts and maintain the continuity of frontage landscaping.
9. Light industrial uses adjacent to residential use will be buffered by a fifty foot (50') wide pedestrian/bike corridor.
10. Light industrial uses fronting on Washington Boulevard and Pleasant Grove Boulevard shall have a fifty foot (50') landscape buffer setback from the back of the ultimate curb. Projects shall not back onto these roadway frontages unless adequate screening is provided through berms, landscaping, or fencing. The use of berms and landscaping, rather than fencing, is strongly encouraged.

11. Pedestrian pathway access, connections shall occur along the perimeter of a project to adjacent pedestrian pathways (either adjacent to a roadway or watershed) at an average interval of every six hundred linear feet (600') along the perimeter of the project, with the distance between individual access points not to exceed one thousand linear feet (1000') along the perimeter of a project. A minimum of one connection per street or open space frontage shall be provided within each ultimately defined light industrial parcel. The pedestrian pathway connection shall be separated from vehicle driveways and shall function to guide the pedestrian to the main building accesses of each building. Such walkways are to be designed similar to Figure 3-19, Pedestrian Walkway in Commercial Parking Lot. Pedestrian access shall also be considered at all vehicular driveway locations. The location of pedestrian access connections shall be coordinated with the location of transit stops to facilitate transit use. Connections from the project to the adjacent pathway shall be made by the developer.
12. Buildings directly fronting roads shall provide direct pedestrian connections to the landscape corridor pathway along the adjacent roadway.
13. Pedestrian access should be provided between individual light industrial projects at regular intervals to promote pedestrian cross access.
14. Light industrial uses adjacent to lower watershed areas (Parcel 84), should incorporate, to the extent feasible, the visual amenity provided by the adjacent open space areas. A minimum twenty foot (20') landscape setback shall be provided from the edge of the watershed. Such setback shall be landscaped and shall include berms and swales to create a boundary and to control drainage. Trees shall be planted to provide shading for the adjacent pathway systems. Fencing between light industrial and wetland areas is discouraged. When determined necessary, such fencing should be an open type which allows a continuous view to the open space area. Regular pedestrian connections to the wetland area and pathway system shall be made by the developer in accordance with pedestrian connection Policy 11, with exact locations to be considered during the design review stage for the adjacent projects. Figure 3-26, Lower Watershed-Light Industrial Interface, illustrates the connection between uses. In general, architectural treatment and materials for those frontages visible from adjacent open space areas shall be the same as those utilized on the main frontages of the buildings. All delivery and storage areas shall be screened.
15. The on-site landscaping on Parcels 48A and 49A adjacent to Highway 65 shall provide a minimum forty foot (40') landscape easement from the edge of state right-of-way.
16. Sidewalks, separate from vehicular driveways, shall be provided from the pedestrian walkway in the landscape corridor along the street to each building by the most feasible direct route.
17. Pedestrian plazas with landscaping, seating, drinking fountains and points of interest, such as water elements or art sculptures, are strongly encouraged.
18. The provision of secured bike storage, lockers and showers for employees may be provided to facilitate walking and cycling to work as an option to complying with the City Rideshare Ordinance. Outdoor seating areas are encouraged.
19. The design of light industrial uses adjacent to potential light rail corridors shall consider the possible future extension of light rail, including right-of-way and station needs. On-site transit stops, including those for buses, should be considered during the design review stage.

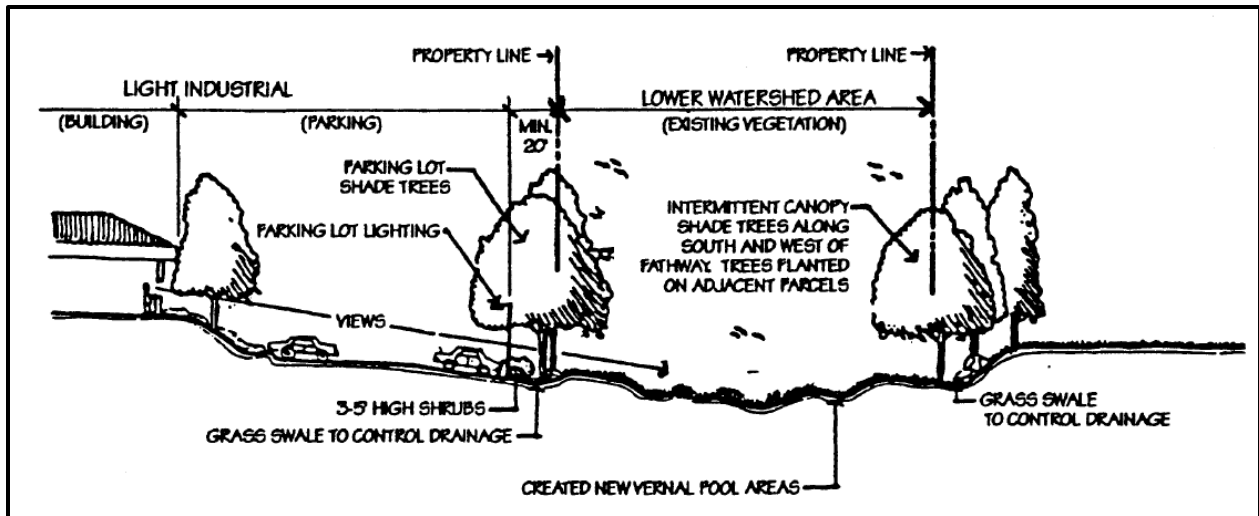


Figure 3-26: Lower Watershed - Light Industrial Interface

20. Land use within the light industrial area- may include convenience commercial support facilities such as day care, eating establishments, specialty retail, personal services such as hair care, and recreation such as health clubs primarily intended to serve the workers in the area. Such uses shall not exceed ten percent (10%) of the total gross floor area in any parcel ultimately defined within Parcels 48 and 49. These uses should generally be located within the light industrial building complexes, and not as free-standing structures. All structures should be easily accessible by employees and pedestrians.

### 3.12a Light Industrial Reserve

Although the Light Industrial Reserve sites (Parcels 48B and 49B) are potentially available for light industrial/commercial related use, these parcels are subject to powerline easements which significantly constrain their utility. For this reason, they will be held in reserve unless and until an appropriate light industrial/commercial related private use is identified and approved by the City through the design review process. Should a light industrial/commercial related land use (including but not limited to parking, storage, nursery or recreation) ultimately be approved for one or both of these parcels, such use will be required to be consistent with the industrial or commercial design guidelines, respectively, of this Specific Plan. The Light Industrial Reserve designation also serves to provide adjacent and nearby property owners with notice that these parcels are intended for private, light industrial/commercial related use in the future.

These reserve parcels will be subject to an open space easement to be dedicated to the City as part of the applicable Large Lot Final Map. The open space easements will be maintained by the North Central Roseville Lighting and Landscaping District ("NCR LLD") with the costs of such maintenance to be assessed against the adjacent Community Commercial or Light Industrial properties (Parcels 48A and 49A). At such time as an alternative private use is approved by the City, that portion of the open space easement that is approved for development will be abandoned by the City and maintenance by the NCR LLD will be limited to that portion of the site which remains unimproved open space. Because the open space easement will be dedicated to the City and will serve a public purpose, and because these reserve parcels cannot be privately developed unless and until all or a portion of the open space easement is abandoned, Parcels 48B and 49B are designated "public parcels" and will not be subject to the levy of

special taxes by the North Central Roseville Community Facilities District ("NCR CFD"). If private development of Parcels 48B and 49B is subsequently allowed by the City, annexation to the NCR CFD may be required for the purpose of assigning special taxes in the future.

### 3.13 General Industrial Land Use

A site of 10 acres (Parcel 69), located east of Harding Boulevard, has been designated for industrial use as illustrated in Figure 3-27, City of Roseville Property. The site is currently owned by the City and encompasses an existing electric substation. Approximately 3 acres of the property is undeveloped. This site will be zoned general industrial and uses are as permitted pursuant to the City of Roseville Zoning Ordinance. The light industrial design guidelines set forth in this Specific Plan shall apply to the design of any project on the property.

### 3.14 Open Space

Open Space within the Plan Area includes parks, the open field portion of school sites, park/ wetland preserve areas, the 100-year watershed along all intermittent and perennial water courses, and landscaped corridors along major arterial streets. Open space serves the following purposes:

1. Assists in providing a buffer between land uses.
2. Provides an opportunity for off-street pedestrian and bikeway circulation.
3. Provides a visually unifying landscape element.
4. Provides view corridors to points of orientation throughout the Plan Area; both for local, short range views to landmarks in the area, and long range views to the Sierra Nevada and Coast Range foothills.
5. Reduces the sense of intensity of land use in the higher density residential and non-residential areas of the Plan.

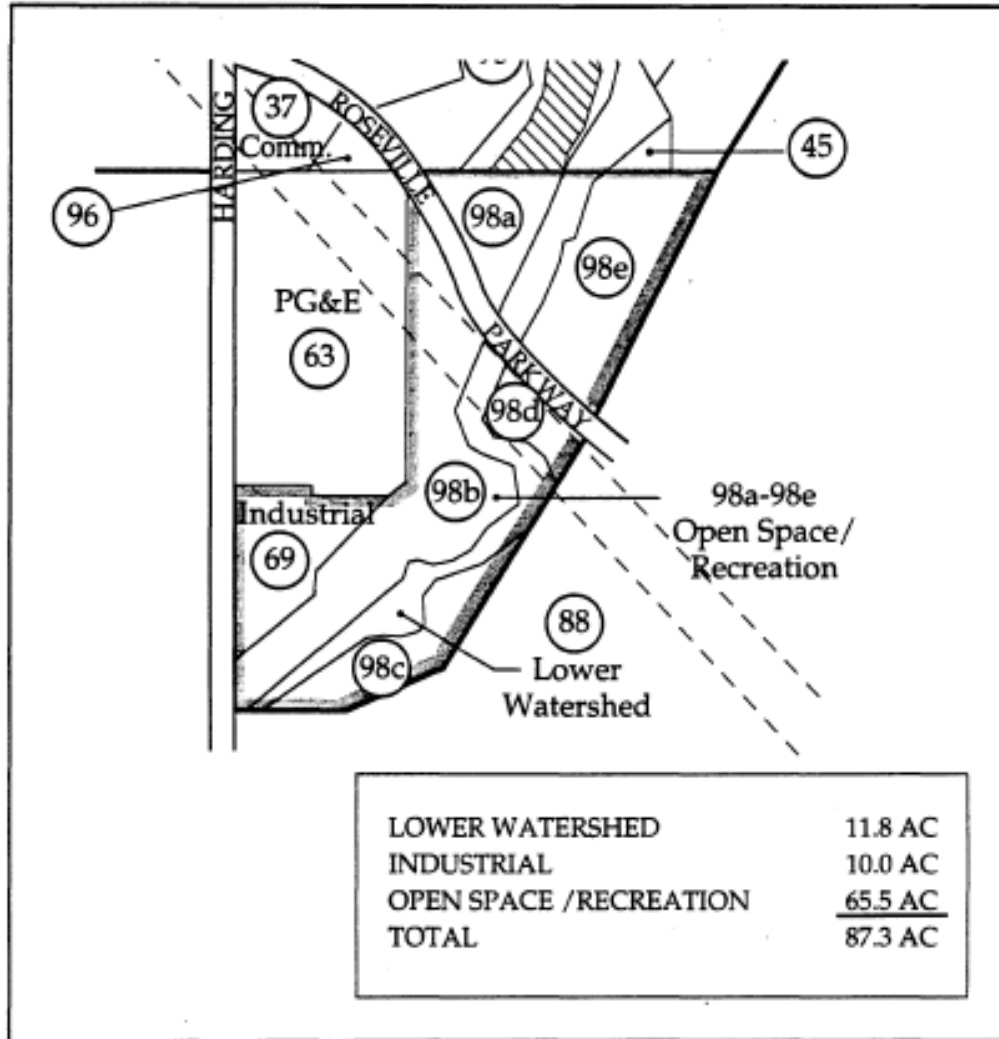


Figure 3-27: City of Roseville Property

6. Preserves special area of riparian or other botanical habitat

7. Provides space for on-site management of storm water drainage. Approximately 340 acres within the Plan Area will be left in primary open space or limited development areas. Approximately twenty percent (19.7%) of the total land area within the North Central Plan (excluding the Urban Reserve) is designated in open space uses, and provides for approximately forty acres of open space land per 1,000 people projected within the Plan Area. Primary open space areas are summarized by type in Table 3-2. The open space tabulation does not include those area designated as urban reserve.

**Table 3-2  
Summary of Primary Open Space**

<u>Open Space Use</u>	<u>Acres</u>
Parks	47.89
Wetland Mitigation	13.30
Park/Preserve	48.53
Schools (K-9)*	8.00
School (7-8)*	9.21
Lower Watershed Preserve	116.83
Golf Course Addition	.88
Slope Easement	18.10
Open Space/Recreation	65.50
<b>Total Primary Open Space Area</b>	<b>328.24</b>

\* Approximately one-half (1/2) of each school site is assumed to be open space.

The following policies are applicable to open space uses throughout the Plan Area:

1. The formal, on-site landscaping for land uses adjacent to natural open space areas shall provide a transition to the natural habitat. Berms and swales shall be utilized where required to control surface drainage and avoid impacts on adjacent wetland and woodland environments. Specific landscaping standards are provided in the General Design Guidelines portion of this element, in Section 6 the Resources Management Element and in the Landscape Design Guidelines. Projects adjacent to open space shall be designed to protect the integrity and function of the open space area. Open Space areas shall be incorporated into all Plan Area projects to the extent feasible.
2. The interface between other land use and open space uses shall be as specific by the applicable use Specific Guidelines of this element.
3. Parks, park preserves, and watershed areas shall be integrated with the comprehensive city-wide trail system. Bike trails in the lower watersheds and park preserve areas will be installed by the project proponents. Connections to adjacent uses shall be as required in the Use Specific Guidelines section of this element. Shading for the trail systems shall be provided within the landscaping of adjacent projects to the extent feasible.
4. Areas within the lower watershed preserve, Parcels 80 through 88, shall be delineated on all project plans and shall be preserved as permanent open space through dedication to the City.
5. Street and other public infrastructure improvements shall be sited to minimize intrusion upon open areas, particularly stream course wetland and woodland areas.

Additional policies relating to open space areas can be found in Section 6, Resources Management Element.

### 3.15 Recreation Open Space

As of the adoption of this Specific Plan, the City owns 87.3 acres of property located west of Harding Boulevard and south of Roseville Parkway. (Figure 3-27) The subject property incorporates a substantial area of wooded slopes, lower watershed preserve, and an old landfill site. Approximately ten (10) acres have been designated for general industrial use and 11.8 acres as lower watershed. The balance of the property, approximately 65.5 acres, is designated as recreation open space. Future use of this portion of the property may include passive open space uses, a golf driving range, archery range, ball diamonds,

picnic areas, and other public recreation uses. In addition, a park and ride lot transit facility may be developed on the property. Prior to any development of any portion of the recreation open space property, supplemental toxic studies will be required of the old landfill site on Parcel 98C.

### 3.16 Other Public Land Uses

The Specific Plan Land Use Map (Figure 2-4) illustrates the location of a fire station; a golf course corporation yard, and an electric utility substation. These facilities, and the policies that guide their development, are described in Section 7 of this Specific Plan. In general, public uses are to be treated, designed, and landscaped compatibly with and similar to adjacent non-public uses.

### 3.17 Houses of Worship

Although not specified on the Specific Plan Land Use Map, houses of worship are permitted in the Plan Area consistent with the provisions of the City of Roseville Zoning Ordinance. The location of such facilities is encouraged adjacent to the neighborhood park/school combinations to enhance the neighborhood focus. Houses of worship shall be treated, designed, and landscaped compatible with adjacent uses.

### 3.18 Highland Reserve North Specific Plan

A total of 615.04 acres of property were designated as Urban Reserve in the North Central Specific Plan Area when originally adopted in 1990. The intent of this designation was to reserve areas for future urban expansion. In 1997, the City approved the Highland Reserve North Specific Plan which encompassed all of the urban reserve area. This Plan is an amendment to the North Central Roseville Specific Plan and provides for a mixed use community to be developed in two phases. Please refer to the Highland Reserve North Specific Plan for a detailed description of planned land uses.

### 3.19 Deleted Text

## 4. Housing Element

It is the intent of the North Central Roseville Specific Plan to provide a mix of housing prices and types that are appropriate to the terrain, and that will meet a portion of the housing demand generated in the City.

### 4.1 Primary Housing Goal

The primary housing goal for the North Central Roseville Specific Plan is to provide a mix of housing types for affordable housing opportunities as defined in the General Plan Housing Element. The Housing Element of the Roseville General Plan, adopted in late 1988, specifies a goal of providing ten percent (10%) of the new City housing stock affordable to low-income households. The North Central Roseville Specific Plan incorporates a program designed to achieve this goal among the 4,711 dwelling units in the Plan Area. A total of 450 dwelling units, or nine percent (9%) of the units in the North Central Roseville Specific Plan, will be required to meet the affordable housing goal.

Housing policy relating to affordability is related to a certain extent to residential densities. In general, it is presumed that higher residential densities, notably multi-family housing, equate to housing prices which may be affordable to households in the lower end of the income range. The lower prices can be attributed to small dwellings and lower land costs per unit associated with higher densities. Consequently, the mix of low, medium, and high density housing is considered to be a factor in assuming housing affordability for the full range of household income groups.

### 4.2 Jobs/Housing Balance

Employment and land-use are integral considerations in planning for development of the entire South Placer region. All planning for housing, transportation, public services, and facilities is predicated on the principle that workers should have the opportunity to find housing that is affordable and suitable within a reasonable commute distance from their place of employment. A reasonable commute distance for the South Placer region has been defined as not more than eight (8) miles for a minimum of eighty percent (80%) of the labor force (South Placer Policy Plan, 1982).

The balance of jobs and housing does not respect municipal boundaries. Workers tend to choose their places of residence on the basis of many disparate factors. Consequently, an evaluation of jobs and housing must consider all housing and employment opportunities on the basis of commute distance rather than jurisdictional or Plan Area boundaries.

The North Central Plan Area is estimated to generate approximately 15,200 jobs in all commercial, business-professional, and industrial land uses. This estimate is based on assumed employee per acre generation factors of 20 employees per acre for commercial, 34 employees per acre for business-professional and 13 employees per acre for light industrial uses. The employee projections may be high relative to the recent experience for industrial land uses in the South Placer region.

There are 4,711 dwelling units proposed in the Plan Area. It is assumed that the regional average of 1.3 workers per dwelling will apply, resulting in a total Plan Area resident work force of 6,124 workers. Therefore, the ratio of employment opportunities to housing opportunities would be approximately 2.17:1. On a Plan Area basis this would indicate that a substantial number of workers will reside outside the North Central Plan Area, and that a significant imbalance in jobs/housing will occur. To a certain

extent, this is to be expected, in that the North Central Specific Plan Area includes a commercial core area that may serve as a satellite downtown center for the entire South Placer region.

A cumulative analysis of the jobs/housing balance for the commute shed centering on the Plan Area is a more accurate way of assessing the jobs/housing ratio. Such an analysis indicated that there is a substantial supply of housing, both existing and planned, that will offset localized imbalance. The housing stock proposed within eight (8) miles of the Plan Area along the Highway 65 corridor, includes 11,500 dwelling units planned at Stanford Ranch and 8,500 dwelling units in the Northwest Roseville Specific Plan, in addition to several other residential areas in the early phases of the planning process. A 1989 jobs/housing analysis showed that there will be a numerical balance of jobs and housing at full development of the current General Plans for jurisdictions in the South Placer Area. (Jobs/Housing Analysis North Central Roseville Specific Plan, Wade Associates, November 6, 1989). At full development 117,868 jobs and 88,127 dwelling units are projected with the eight (8) mile commute shed, which result in a 1.34 jobs to housing ratio. The local jurisdictions in the South Placer region, including the City of Roseville, are participants in a program to monitor the cumulative growth of jobs and the availability of housing over time to ensure that the balance of jobs and housing is sustained within the commute shed area. (Roseville Housing Monitoring Resolution, 1982).

Approximately 39.2 percent (South Placer Employee Survey, 1987) of all households within the commute shed earn \$50,000 or more, and based on an expenditure of thirty-five (35%) of their gross income for housing, they can spend \$17,500 per year on housing. The number of households in this income category exceeds the number of higher priced units planned within commute shed.

Households with higher incomes may choose to spend less than the maximum they can afford on housing costs. In such cases, households on the upper end of the income scale may occupy units which are also affordable to households generating less income.

In the middle-upper income range, where households can afford between \$12,250 and \$17,499 for housing, an adequate supply of affordable units will be available. For households in the mid-range which can afford approximately thirty percent (30%) of their annual income, or between \$10,500 and \$12,249 annually for housing costs, there is more demand for housing than units available. This corresponds to the entry level purchase market which is addressed by the low to middle income housing program set forth in the North Central Specific Plan.

For households with low and very low income, which can afford less than \$10,499 annually for housing, an adequate supply of units exists at the upper end of the range. A significant surplus of available units is estimated for households which can afford between \$7,400 and \$10,499 annually. The very low income housing demand includes households that earn less than \$18,000 annually, and who can afford to spend less than \$6,000, or about \$500 per month on rent. These account for approximately 6.2 percent of all households. Approximately nine percent (9%) of all new planned housing is projected to be rented at a rate in the range of \$500 per month.

Consequently, it would appear that a majority of households will potentially be able to find affordable units within the commute shed. However, the lower cost units still will not be affordable at the very low income range. The housing program set forth in the North Central Plan targets a total of ten percent (10%) of all new units in the Plan Area of which approximately 75 units or 7.5 percent of all units are intended to be affordable to households in the low and very low income ranges.

The overall concern of housing affordability for the majority of residents in the City can be best met by assuring that there is an adequate supply of affordable units relative to demand. The problems of traffic congestion, deteriorating air quality, and disproportionately high housing prices relative to average wages in many of the metropolitan regions in California is, in significant part, attributable to an insufficient affordable housing supply relative to demand.

The City of Roseville, in cooperation with other jurisdictions in South Placer County, has established both strong policies and implementation programs with regard to maintaining a balance of jobs and housing. Furthermore, it is the City's policy to provide assistance regarding equal housing opportunities through the City Housing Office and Housing Authority.

Such policies and implementation programs were put in place in 1983 to assure an adequate supply of housing relative to the employment growth anticipated as a result of the construction of the Highway 65 Bypass. The fundamental policy requires that there be an adequate supply of affordable housing to meet the needs of the labor force as it increases over time. Housing affordability for Plan Area residents is achieved primarily through the provision of an adequate supply of housing to keep home prices and rents competitive with other housing opportunities in the region.

The policy is implemented through a number of measures including:

- annual monitoring of job development and housing availability;
- requiring an evaluation of the availability of housing to accommodate the projected work force as a condition of approval for use permits for employment facilities at which more than 50 employees will work (Resolution #83-118); and
- providing adequate land area and infrastructure to accommodate future housing demand.

### 4.3 Affordable Housing Definitions

The terms used to define various income groups, as applied in this Specific Plan, are extracted from the Roseville General Plan Housing Element. For the purposes of this Specific Plan the income definitions are as follows:

<u>Income Category</u>	<u>Household Income as % of Regional Median Income</u>
Very Low Income	less than 50%
Low income	50% to 79.5%
Middle income	80% to 100%

In addition to providing housing to accommodate employment growth in the City, there is a fundamental problem of housing for low income households. Such housing needs are typically met in established neighborhoods where the older dwellings command a lower price than newer units. However, in relatively small communities the supply of older housing may not be sufficient to provide adequate opportunities for low income households.

The magnitude of this problem in Roseville is addressed in the City General Plan Housing Element. It is not known, however, how many households are currently living in substandard or overcrowded dwellings, or are paying more than they can afford to the detriment of meeting other basic needs. It is difficult to project, with any certainty, the magnitude of this problem over a period of years as the North

Central Specific Plan Area and other areas of the City develop. The General Plan Housing Element has specified that ten percent (10%) of new housing affordable to low income households is a reasonable goal based on analysis of the housing need and an estimate of the resources available.

#### 4.4 Housing Affordability Program

The North Central Specific Plan complies with the General Plan Housing Element by designating ten percent (10%) of the total Plan Area housing stock for participation in the affordable housing program defined in this section. A portion of the need will be met through purchase housing affordable to middle income residents, with the remainder provided through rental housing affordable to low and very low income residents.

##### 4.4.1 Housing Affordable to Middle (80% to 99.5% of Median) Income Households

In the North Central Specific Plan Area approximately seven percent (6.6%) of all single family housing will be purchase units affordable to middle income households as defined in the General Plan Housing Element. A number of dwelling units is designated on specific parcels shown on Table 4-1) to be affordable to middle income households at the time of sale. The developer of the designated parcels will commit to providing the affordable housing under the Development Agreement implementing this Specific Plan and no subsidy will be required from the City. The builders, as successors to the Development Agreement, will sell the units only to those buyers who qualify under the middle income category. The long term affordability of the designated units will be ensured by a subsequent affordable housing development agreement, recorded concurrently with tentative map approval, that requires the original purchaser and all successors in interest to sell only to middle income households as defined in the Roseville General Plan Housing Element.

**Table 4-1  
Allocation of Middle Income Purchase Units By Parcel**

<b>Parcel Number</b>	<b>Total Units</b>	<b>Affordable Units</b>	<b>% of Parcel Units Affordable</b>
A	79	4	5.1
B	73	6	8.2
C	94	4	4.3
D	63	2	3.2
E	73	6	8.2
F-1	38	3	7.9
F-2	35	3	8.6
G	48	3	6.1
H	29	3	10.3
I	75	6	8.0
J	27	2	7.4
K	68	5	7.4
L	61	6	9.8
M	45	3	6.7
N	78	3	3.8
R	71	6	8.5
9	38	2	5.0
11	21	21	100.0
13	90	9	10.0
17A	119	6	5.0
17B	106	5	5.0
18A	48	2	5.0
18B	53	3	5.0
19	165	8	5.0
26	45	45	100.0
29	11	1	9.1
44	240	4	1.6
<b>Total</b>	<b>1,893</b>	<b>171</b>	<b>9.0%</b>

#### 4.4.2 Housing Affordable to Very Low and Low (Less than 50% and 50% to 79.5% of Median) Income Households

In the North Central Specific Plan Area more than twenty-two percent (22.9%) all multifamily housing units will be affordable to low or very low income households as defined in the General Plan Housing Element. The housing is to be achieved by designating a percentage of the multi-family units on specific parcels (shown on Table 4-3) to be affordable to low and very low income households for a period of 20 or 30 years pursuant to a subsequent affordable housing development agreement with the City.

The City will compensate the apartment owner in an amount equal to two-thirds the net present value of the loss of revenue that would accrue over the period that the units are held in reserve for low income households. The agreement shall specify the number of units to be reserved, the number of years to be reserved, and the amount and type of compensation. The compensation may take any form available to the City including: cash payment, reduction in fees, increases in value attributable to a density bonus, or any form agreeable to the landowner and City. City staff shall specify the range of incomes to be served and identify the source of subsidy within sixty (60) days after the landowner applies for a use permit, with respect to any of the parcels identified on Table 4-3. The City and landowner shall enter into an affordable housing agreement prior to the issuance of a building permit for the subject parcel.

**Table 4-1a  
Allocation of Low Income Purchase Units by Parcel**

Parcel #	Total Units	Affordable Units	% of Parcel Units Affordable
18C	248	4	1.6%
44	240	10	4.2%
Total	488	14	2.9%

The City shall have nine (9) months from the time an apartment builder applies for a use permit to provide the funding for subsidies. If the City cannot provide the necessary funding, then the requirement to provide the low-income affordable units is terminated or shall be reduced to a level that can be funded by the City. If the City has agreed to provide funding and the project does not proceed due to failure by the developer, the City shall be entitled to reimbursement for any losses or penalties incurred pursuant to assembling the necessary funding or subsidies for the affordable units.

If adequate subsidies are not available to assist the developer in achieving the ten percent (10%) Affordable Housing Goal, the goal will be deferred to a future date agreed upon by the property owner and the City. Deferring the goal will give the City an opportunity to assemble the necessary financing. The property owner will be given the option of achieving the goal in a later phase of the development or giving the City the ability to provide subsidies after development has occurred, such as through rental assistance programs. However, if the needed subsidies cannot be assembled within the agreed upon deferment period, the Affordable Housing Goal can be reduced to a level that the available subsidies can support. If no subsidies can be assembled by the end of the deferment period, the Affordable Housing Goal can be terminated for that project. Table 4-2 illustrates a hypothetical low income subsidy calculation. The calculation is provided as an example only, as values will vary from project to project over time.

**Table 4-2**  
**Example of a Low Income Subsidy Calculation**  
**for Units Affordable to Households Earning**  
**60% of the Median Income**

Total Rent Income Shortfall	\$69,681	
Assumed Annual Increase in Market Rent	4%	
Assumed Annual Increase in CPI	4%	
Assumed Market Rent in First Rent	\$500	per unit/month
Assumed Affordable Low Income Rent in First Year	\$305	per unit/month
Discount Rate	10%	
Net Present Value of Rent Shortfall	\$26,298	per unit
Developer Share of the Subsidy (1/3)	\$8,757	per unit
City Share of Subsidy (2/3)	\$17,541	per unit

Table 4-3 lists units affordable to low and very low income households.

**Table 4-3**  
**Allocation of Low and Very Low Income Rental Units by Parcel**

Parcel Number	Total Units	Units Affordable	% of Parcel Units
20	602	120	20.0
20C	258	26	10.0
21B	149	30	20.0
22	340	34	10.0
27	174	26	15.0
28	72	11	15.0
42B	273	27	9.9
46	<u>224</u>	<u>23</u>	<u>10.0</u>
	2,092	451	21.5

Property owners will enter into supplemental affordable housing development agreements that set the initial rent for low income households, establish the criteria and basis for annual rent increases, and provide the City a method to monitor the rents paid by low-income households. The agreements shall also establish the basis and terms for refunding to the City a portion of the compensation if the units are not rented by qualified low income households.

Based on the subsidy need anticipated to meet the housing requirement for this income group, some form of deep subsidy will likely be required to help offset the difference between real cost of producing the housing and the amount that the households can reasonably afford. The source of such funding is typically government housing programs that directly subsidize the cost of land acquisition and construction. The landowner and City agree to join in any application prepared by the City for federal, state, local or private funding and agree to cooperate to obtain the approval of any such application.

#### 4.4.3 Tax Increment Funding

One source of funding for affordable housing programs is the significant amount of new property and sales tax revenues that will accrue to the City of Roseville General Fund as a result of growth. The City's Specific Plan Areas will provide a significant increment of increased property tax, sales tax, and utility user taxes generated by residential and non-residential development. The tax increment funding may be allocated to a variety of public service needs in the City's annual budgeting process as a matter of public policy. The allocation could include the support of affordable housing.

#### 4.4.4 Density Bonus Program

The City may, in accordance with the adopted Density Bonus Ordinance, assign additional residential units to projects in the North Central Specific Plan Area, in excess of those assigned in the 1985 General Plan Amendment. The additional dwelling units would be assigned for the purpose of achieving the affordable housing goal by providing a "density bonus" and, thereby, reducing the average development cost of the individual units. Such density bonus units would be assigned by an affordable housing development agreement to individual projects on a case-by-case basis, and may constitute a portion of the compensation for the provision of units affordable to low income households.

#### 4.5 Specific Plan Housing Policy

The following housing policies shall be implemented in the North Central Roseville Specific Plan:

1. The Roseville Housing Office will notify each applicant for a residential project in the North Central Plan Area of opportunities to participate in affordable housing programs at the time of application.
2. The City and developers shall implement the housing affordability programs as outlined in Sections 4.4.1 and 4.4.2 of this Element and in the Plan Area development agreements.
3. The developers of the parcels identified in Table 4-1 shall commit to provide the number of dwelling units specified at a price affordable for purchase by middle income households at the time of sale. A supplemental affordable housing development agreement that requires the original purchaser and all successors in interest to sell only to middle income households as defined by the Roseville General Plan Housing Element, will be recorded concurrent with tentative map approval.
4. The developers of parcels identified in Table 4-3 shall commit to provide the number of dwelling units specified at a price affordable to low and very low income households, in accordance with the dwelling unit price (rent) set forth in the Housing Element of the Roseville General Plan. A supplemental affordable housing development agreement shall be recorded at the time a use permit is issued, specifying the number of years to be reserved and the type and amount of compensation.
5. The City shall implement a "surplus" density bonus program as outlined in Section 4.4.5 of this Element per the adopted Density Bonus Ordinance.

## 5. Circulation Element

The comprehensive circulation system in the North Central Specific Plan Area is designed to provide a range of transportation options for the safe and efficient movement of people. The circulation system includes public transit (including buses and future light rail), public streets, pedestrian paths, bikeways, and parking areas. Most notably, the Specific Plan incorporates an extensive bicycle and pedestrian system which provides direct access to all land uses. Convenient pedestrian connections are provided, to allow area residents the opportunity to walk throughout the Plan Area, thereby reducing vehicular traffic.

The North Central Roseville Specific Plan Area is served by a network of streets organized in accordance with function and size. Streets will be built to accommodate the intensity of land uses they serve, consistent with the level of development as specified on the Land Use Map. Roseville Parkway, Harding Boulevard/Stanford Ranch Road, Pleasant Grove Boulevard, and Highway 65 carry regional traffic through the Plan Area and provide direct access to the major land uses in the Specific Plan. Secondary collector streets provide access to the light industrial and business-professional sites. Residential collector streets connect to local residential streets which provide access to individual homes, a small commercial use within one of the neighborhoods, neighborhood parks, and schools.

All of the streets in the North Central Specific Plan provide for pedestrian pathways set back from the curb to provide the option of walking within the Plan Area. This pathway system is directly linked to the various land uses and the open space pathway system in accordance with policies in this element and in the Community Form and Design Element. The arterial and collector streets also provide Class I bicycle lanes set back from the curb on both sides of the street.

The circulation system is designed to achieve the following objectives:

1. Construct a safe and efficient circulation system.
2. Provide convenient and safe pedestrian routes to all land uses.
3. Furnish an aesthetic environment for public circulation.
4. Maintain Level of Service (LOS) "C" for roadway linkages and intersections at arterial and collector streets.
5. Minimize impact on regional air quality.
6. Facilitate alternatives to vehicle transportation.
7. Accommodate future public transit services, including light rail.
8. Facilitate regional traffic for uses which attract trips from throughout the South Placer /Sacramento area.

### Existing Street System

As of adoption of the Specific Plan, portions of the major circulation system have already been implemented. The Highway 65 Bypass, completed in 1987, is a key element in the overall circulation system. The bypass bisects the Plan Area and provides direct access to I-80. Stanford Ranch Road, connecting Sunset Boulevard in Rocklin to the Highway 65 Bypass, is scheduled to be completed in mid-1990.

Washington Boulevard, the former route of Highway 65 along the west boundary of the Plan Area, provides a through link between the Plan Area and downtown Roseville. The Harding Boulevard

extension to the Atlantic Street/I-50 interchange to be completed in 1991, will also provide a connection to the downtown area via Atlantic Street. The Harding Boulevard/Stanford Ranch interchange with the Highway 65 Bypass is scheduled to be completed by September 1990.

## 5.1 Streets

Although the land use pattern, the pedestrian pathway system and other methods to accommodate alternative transportation are expected to reduce auto use, automobiles will remain a predominant transportation mode for Roseville residents and employees in the foreseeable future. Slightly more than fifty percent (50%) of the traffic projected on Roseville Parkway and Pleasant Grove Boulevard will not originate or terminate in the Plan Area. As a result, there would be significant traffic anticipated within the Plan Area irrespective of the proposed Specific Plan land use.

The alignment of the planned major arterial and collector streets is illustrated on Figure 5-1, North Central Roseville Circulation Master Plan. Secondary, or collector streets are illustrated where access to public facilities, notably schools and parks, is required. Local neighborhood streets will be designed through the subdivision and design review process for individual projects and, therefore, are not illustrated on the Circulation Master Plan. The pattern of local streets will be designed to facilitate access within neighborhoods to the planned neighborhood community and commercial centers.

### 5.1.1 Major Arterial Streets

The primary function of the major arterial streets is to move traffic to from and through the Plan Area to other sections of the City. Employees from adjacent residential areas, particularly Rocklin, will travel to and through the Plan Area to the employment centers in North Roseville. Residents in the eastern areas of Roseville will need to commute through the North Central Plan Area to the employment areas within the Plan and along Industrial Boulevard and Foothill Boulevard. It can also be assumed that customers coming from the City of Roseville and beyond will travel to the regional shopping center from all directions.

Washington Boulevard and the Highway 65 Bypass will serve as major entryways into Roseville and the Plan Area from the north and from Interstate 80. The Plan Area will be crossed by a number of major arterials that link the east and west sides of the City, and north-south routes linking with Rocklin and established areas in Roseville. These links, illustrated on Figure 5-2, Central Roseville Circulation Network Diagram, include existing Washington Boulevard, Pleasant Grove Boulevard, Stanford Ranch Road, and Harding Boulevard connecting to the north and south. State Route 65 (Highway 65 Bypass) and Roseville Parkway will link the City to the east and west.

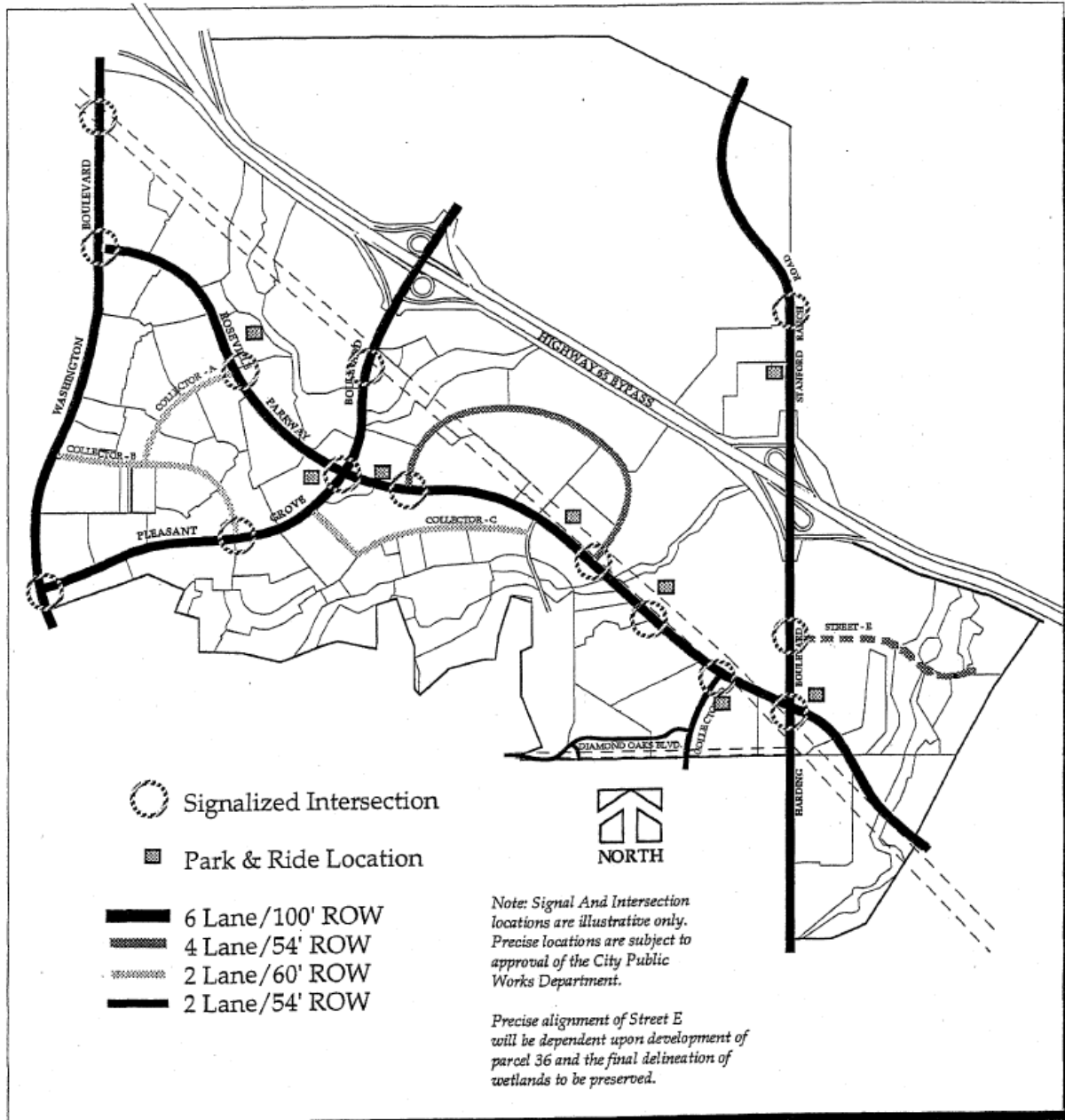


Figure 5-1: North Central Roseville Circulation Master Plan

Roseville Parkway is planned as a major intra-city 6-lane expressway connecting the Northeast and Southeast Specific Plan Areas with the North Central Specific Plan Area and the North Industrial Area to the west. West of Pleasant Grove Boulevard, the initial construction will be 4-lanes with two future expansion lanes within the thirty-eight foot (38') landscaped median. This configuration will ultimately accommodate six (6) traffic lanes and a fourteen foot (14') wide median as shown in Figure 5-3, Initial 4-Lane Configuration With Median Expansion to 6-Lanes. Roseville Parkway will be a limited access roadway with minimal cross traffic and turning movements.

Pleasant Grove Boulevard will serve as a major arterial linking Highway 65 and the North Central Specific Plan Area with the Northwest Specific Plan. Pleasant Grove Boulevard will intersect Foothills Boulevard

and other north-south roads further to the west, including Cook-Riolo Road and, ultimately, Fiddymont Road.

The Harding Boulevard extension will be a major north-south arterial linking the Highway 65 Bypass and the Plan Area with Atlantic Street, downtown Roseville, and the Douglas Boulevard/Interstate 80 area. Harding Boulevard/Stanford Ranch Road will also serve as a through route for residents of west Rocklin to access downtown Roseville, the commercial development along Douglas Boulevard/Interstate 80, and the regional mall site.

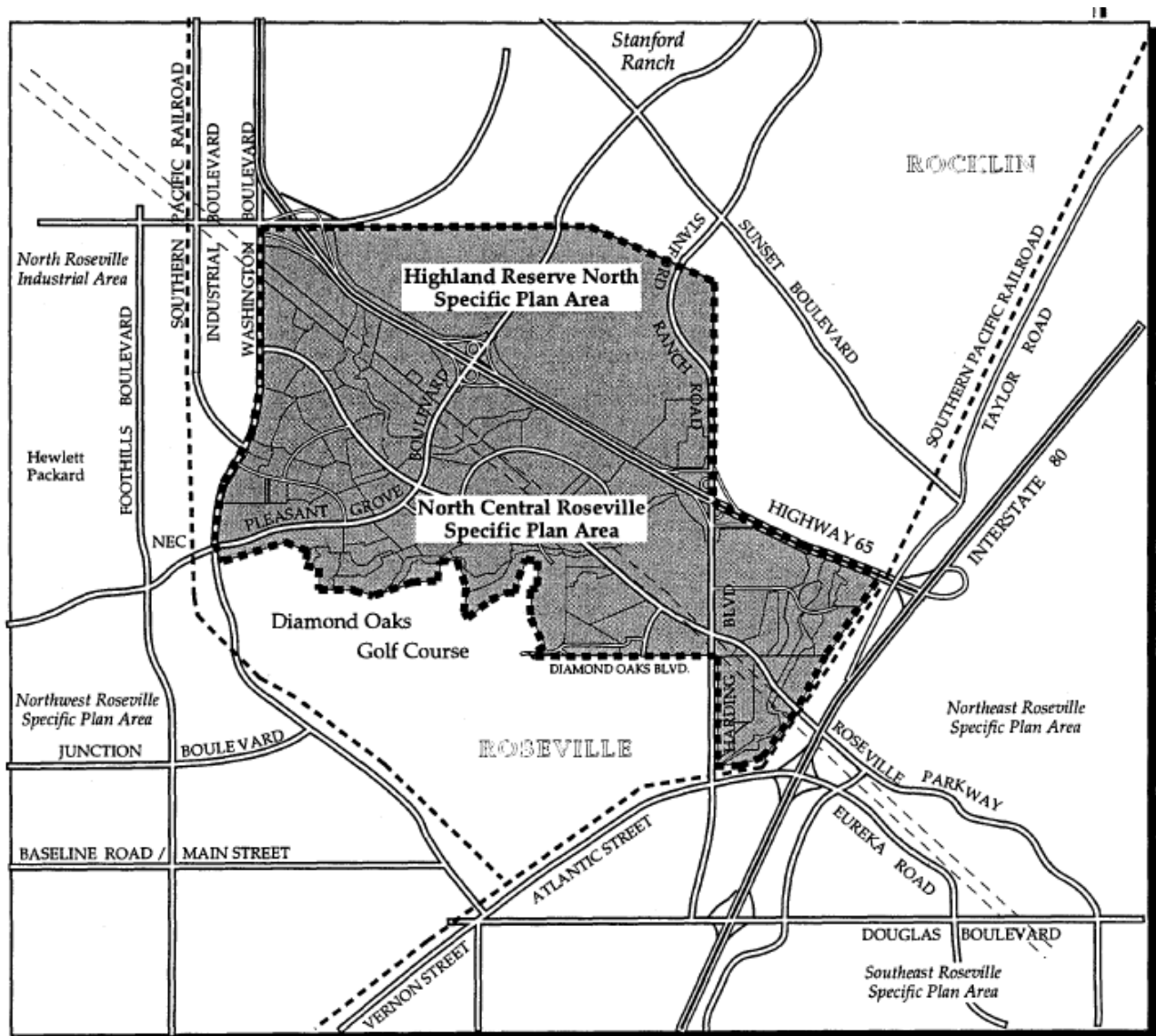


Figure 5-2: Central Roseville Circulation Network Diagram

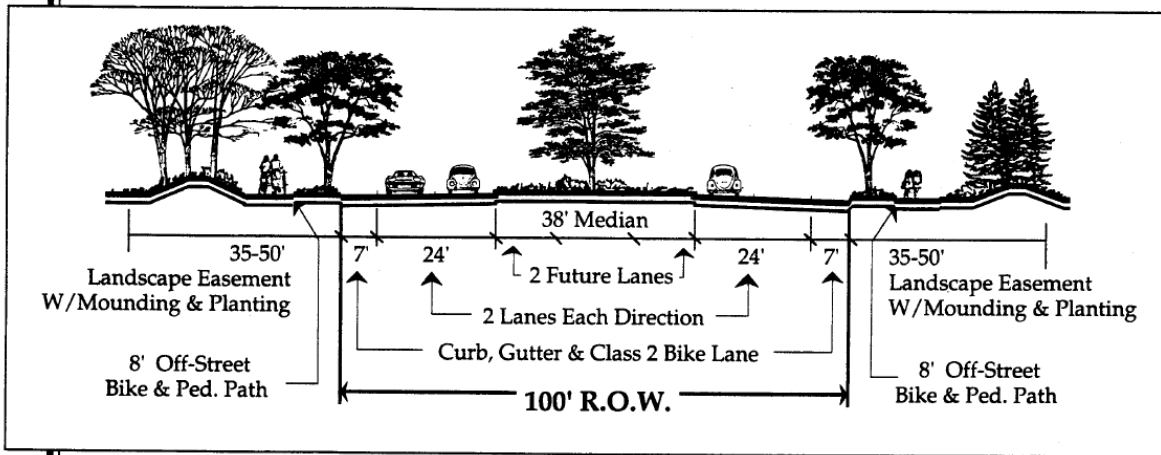


Figure 5-3: Initial 4-Lane Configuration with Median Expansion to 6-Lanes

Arterial streets are designed with a one hundred foot (100') right-of-way that incorporates six (6) travel lanes, on-street bike lanes, a landscaped median, and standing curb and gutter, as illustrated in Figure 5-4, Typical Six Lane Arterial. On-street parking will be prohibited. Arterial streets in the Plan Area will include landscaped corridors with an eight foot (8') wide meandering pedestrian walkway separated from the street by a landscape strip. The landscaped corridor on arterial roadways is measured from the back of ultimate curb, and is located in an easement over adjacent private lands. The exception is for the landscape corridors adjacent to single family residences, in which the corridor will be dedicated in fee to the City. In order to facilitate visibility and a sense of openness, the landscape corridors at intersections will be flared. Figure 5-5, Typical Corner Setback, provides an illustration of the typical additional setback required at intersections.

Noise buffering techniques, such as landscaping, berms and acoustically designed masonry walls, will be utilized to attenuate noise impacts on single family residences adjacent to arterial streets. This interface occurs only along portions of Pleasant Grove Boulevard, Washington Boulevard, and the Roseville Parkway. Similar treatment shall be utilized at the intermediate school frontage along Washington Boulevard. The right-of-way, number of lanes, and width of the required landscape corridor for each major arterial and collector street is summarized in Table 5-1.

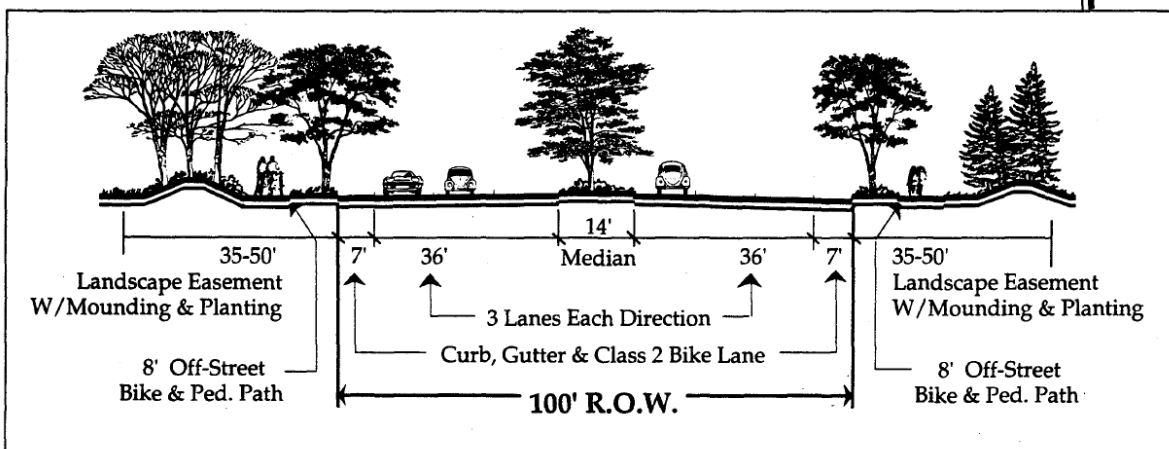


Figure 5-4: Typical 6-Lane Arterial

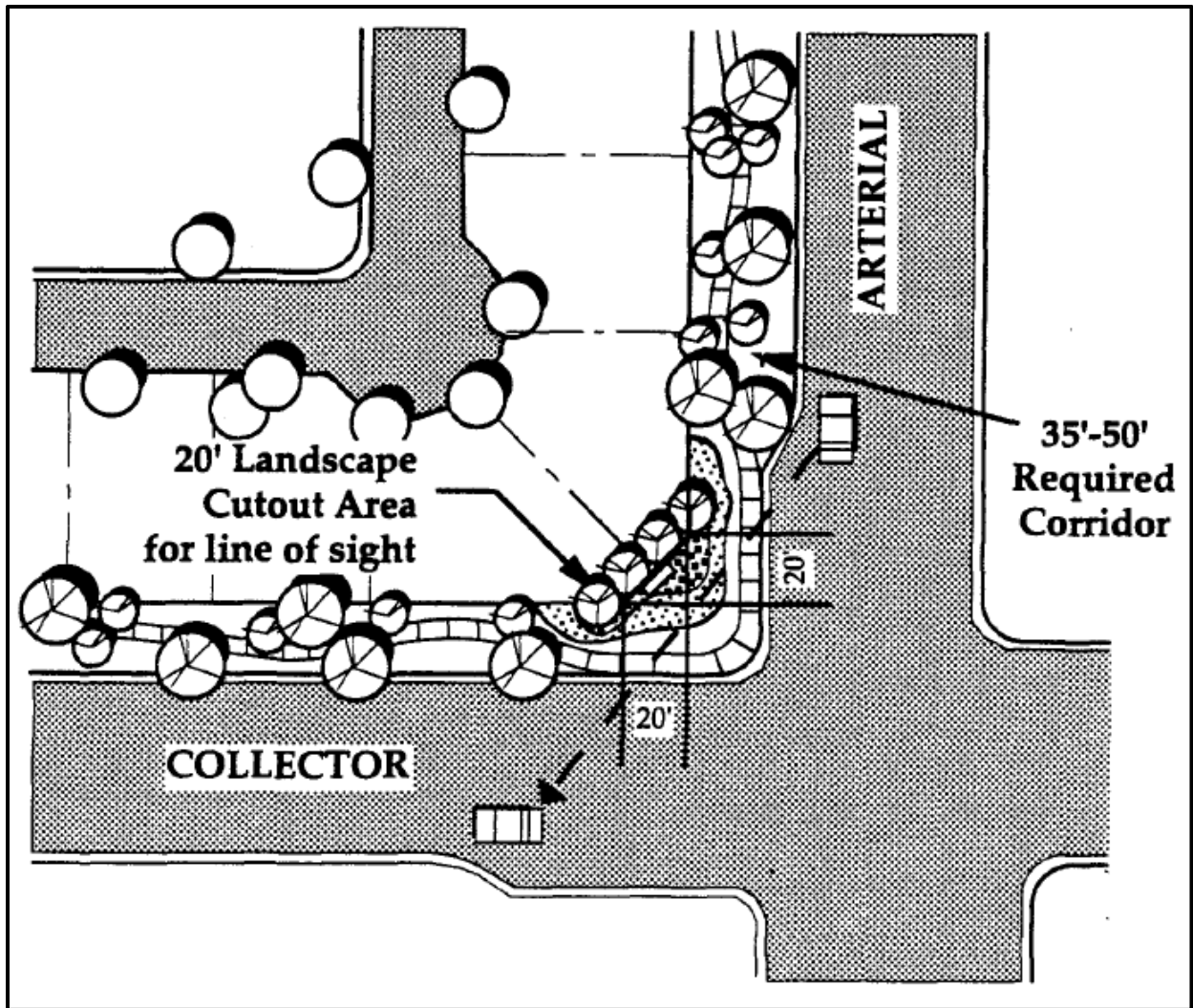


Figure 5-5: Typical Corner Setback

**Table 5-1  
Summary of Vehicle lanes on Major Arterial and Collector Streets**

Road Link	Ultimate Lane Capacity	Landscape Corridors (Measured from back of Curb)		
		ROW	Single Family Use	Multi-Family & Other Uses
<b>Arterials</b>				
Washington Blvd.	4	100	50'	50'
Pleasant Grove Blvd.	6	100	50'	50'
Harding Blvd./Stanford Ranch Road	6	100	50'	50'
Roseville Parkway	6	100	50'	50'
<b>Collectors</b>				
Eastpark Drive, Street E, and Other Non-Residential Collectors	4	54	N/A	30'
Diamond Oaks Blvd.	2	54	25'	25'
Residential Collectors	2	60	25'	25'

Note: The landscape corridor along Diamond Oaks Road adjacent to single family uses will only apply to those locations where units do not directly front the roadway.

The lane requirements for major arterial and collector roads are specified in the *Transportation Impact Study for the North Central Roseville Specific Plan EIR*, Fehr & Peers Associates, January 1989.

### 5.1.2 Collector Streets

Collector streets in the Plan Areas are designed to link local streets and nonresidential land uses to the arterial street system. Residential collectors are designed with a sixty foot (60') right-of-way that incorporates two lanes of auto traffic, on-street parking, curb and gutter, and a pedestrian sidewalk separated from the back of curb by a five foot (5') planter strip, illustrated in Figure 5-6, Typical 2-Lane Residential Collector Street. Residential units will not be permitted to front directly on a residential collector street with the exception of Diamond Oaks Road. A twenty-five foot (25') wide landscape strip, measured from the back of curb, and a masonry wall will be provided along residential collectors. In order to avoid a continuous wall for extended distances along a collector street a variety of site planning treatments are recommended. Alternate site planning configurations are illustrated in Figure 5-9, Alternative Site Plans Adjacent to Residential Collector Streets. The specific site plan configuration will be subject to Planning Commission review as part of the Tentative Map process. A typical intersection between a residential collector street and the arterial street is illustrated in Figure 5-7, Location of Residential Collector and Entry Islands and in Figure 5-8, Typical Entry Island Detail. Driveway locations for access to adjacent streets for Parcel 44 shall be generally as shown in Figure 5-9a.

Diamond Oaks Road is indicated to have a twenty-five foot (25') landscape corridor from back of curb for both non-residential and residential uses. Single family units may front directly on this roadway and will be subject to the standard front yard setbacks as measured from back of walk. The landscape corridor will only apply where units do not front on this roadway.

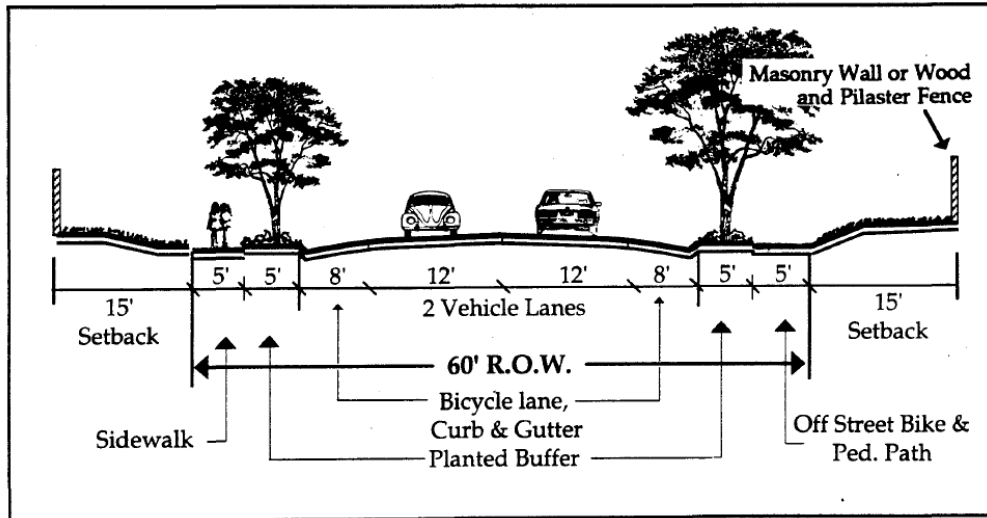


Figure 5-6: Typical 2-Lane Residential Collector Street Where Fences Are Required

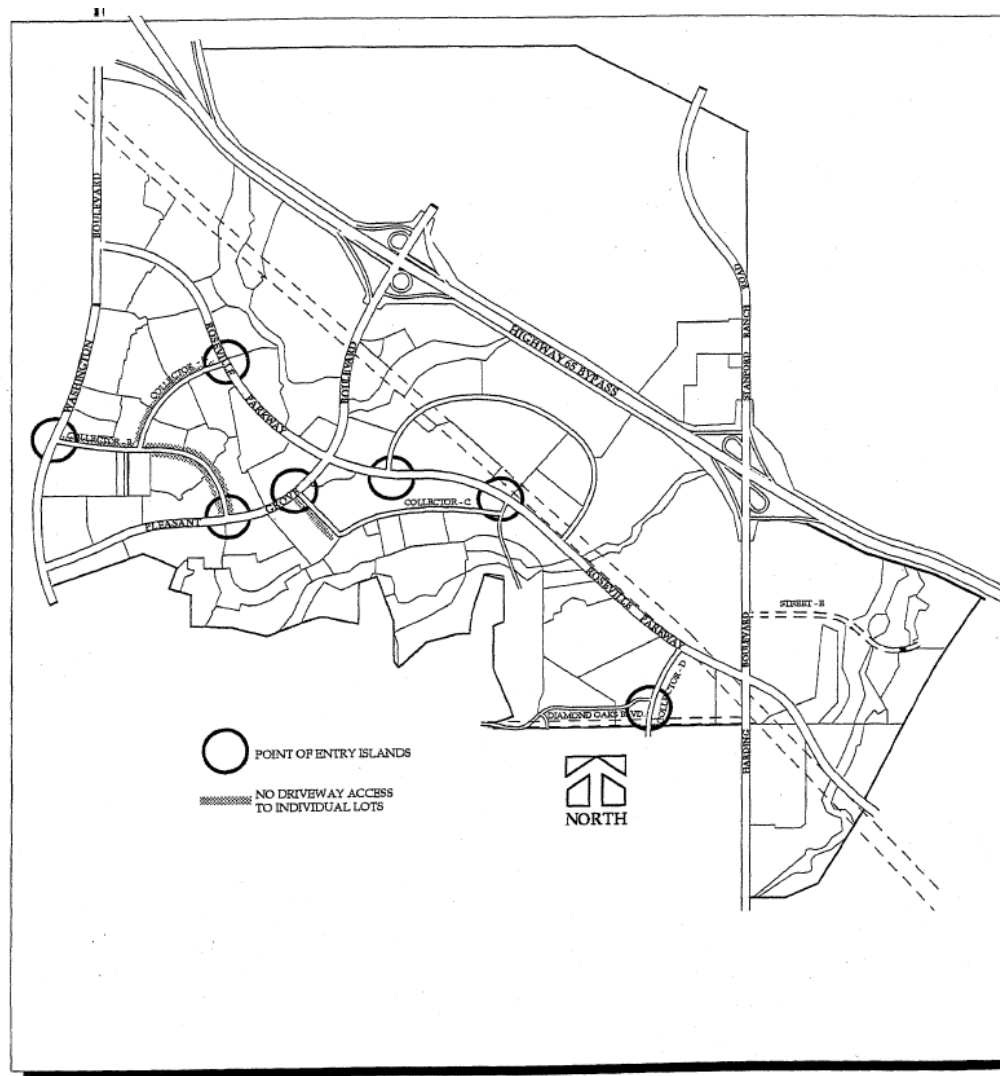


Figure 5-7: Location of Residential Collector and Entry Islands

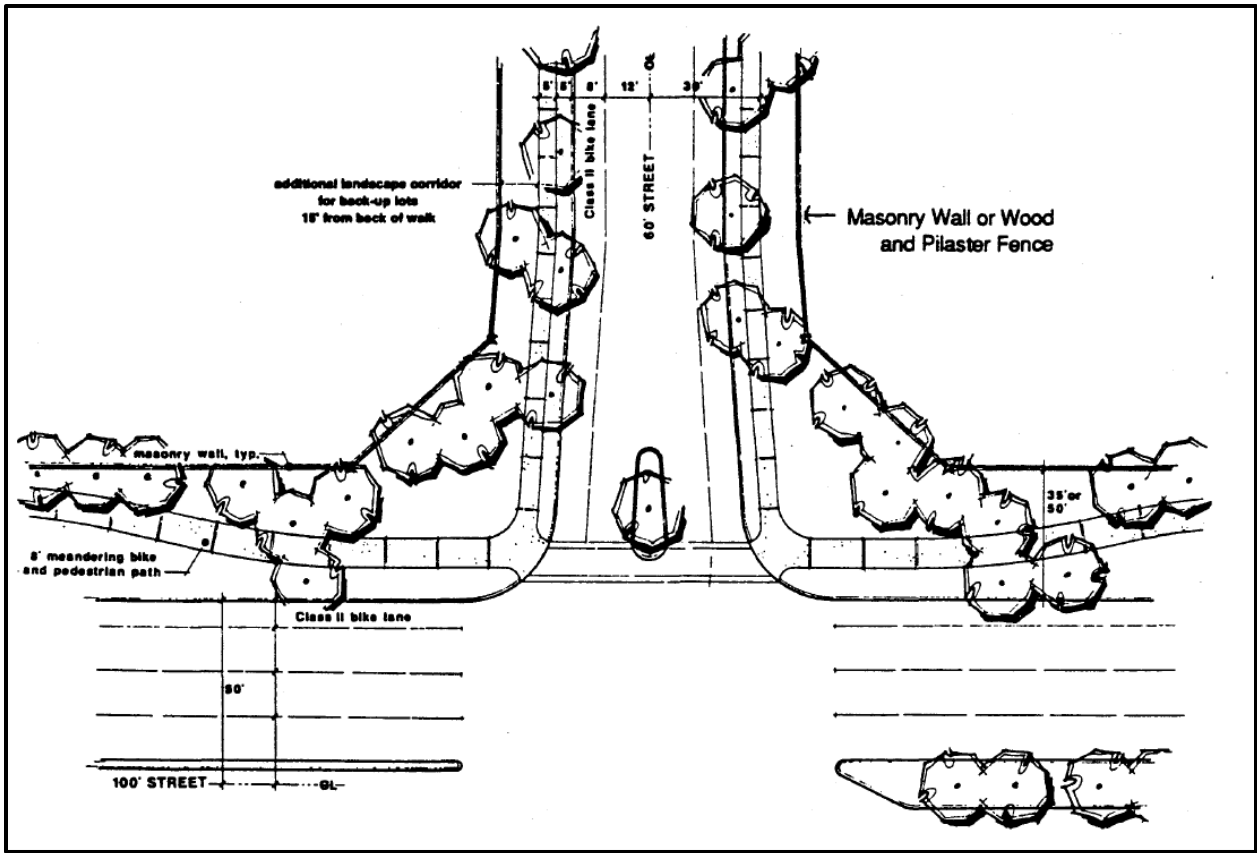


Figure 5-8: Typical Entry Island Detail

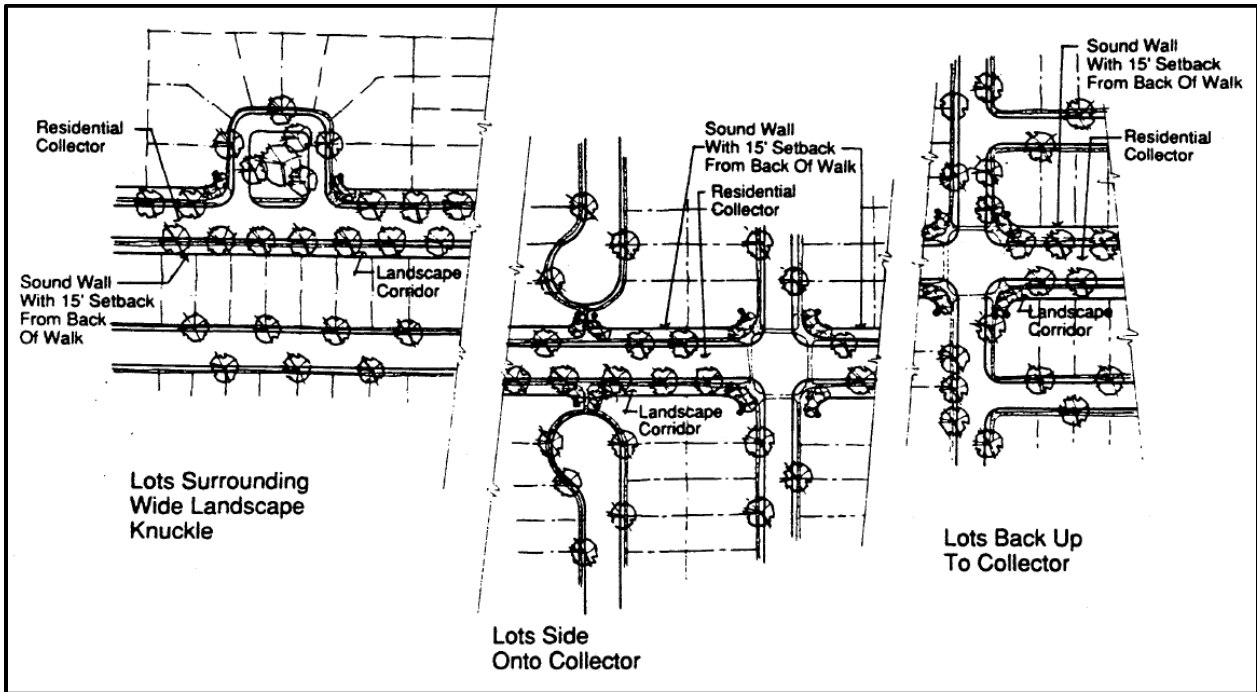


Figure 5-9: Alternative Site Plans Adjacent to Residential Collector Streets

Figure 5-1: Alternative Site Plans Adjacent to Residential Collector Streets

Non-residential collector streets in the Plan Area are designed with a fifty-four foot (54') right-of-way that includes two lanes of auto traffic, on-street parking, bike lanes and curb and gutter as illustrated in Figure 5-10, Typical Non-Residential Collector. Outside the right-of-way, an eight foot (8') wide pedestrian pathway is separated from the curb by a minimum five foot (5') planter strip will be provided. This walkway may meander. Landscape corridor widths along non-residential collectors are measured from the ultimate back of curb. Although initially stripped for two (2) lanes, the non-residential collectors may be restriped to ultimately accommodate four (4) lanes.

Public access from Harding Boulevard to parcels on the east side of the Plan Area shall be provided by Street E across Parcel 36. The alignment of Street E will be dependent upon development of Parcel 36 and the final delineation of wetlands on the parcel. The easement for Street E shall be dedicated, as generally shown on Figure 5-1, North Central Roseville Circulation Master Plan, and as reasonably determined by the Director of Public Works and as indicated in development agreement. The landscape setback standards for Street E shall be as specified in appropriate development agreements and may be modified for any portion of Street E traversing Parcel 97 or adjacent to a wetland preserve.

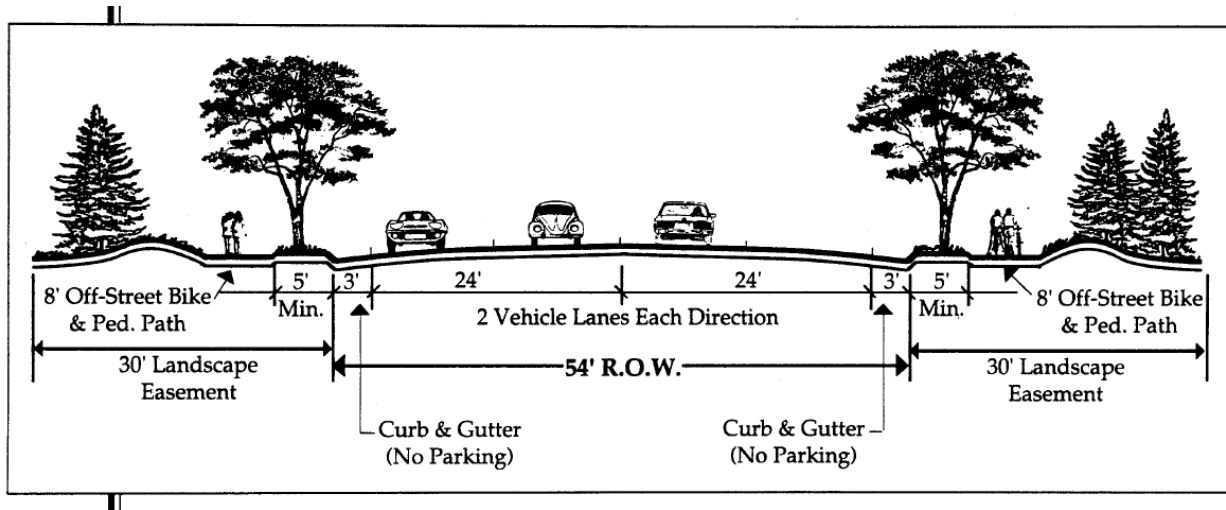


Figure 5-10: Typical Non-Residential Collector 4-Configuration

### 5.1.3 Local Residential Streets

The local residential streets provide access to homes and residential projects. The streets will have a fifty-four foot (54') right-of-way that provides two (2) travel lanes and a narrow parking lane on both sides. This roadway configuration is illustrated in Figure 5-11, Typical Local Residential Street. The five foot (5') sidewalk on both sides of local streets will be separate from the curb by a five foot (5') wide planter strip. The sidewalk and planter strips are an important feature of the North Central Plan Area because they enhance the pedestrian environment. The planter strips provide a sense of distance and safety from the traffic lanes, and provide space for street trees. The trees may grow to form a shade canopy over the street and sidewalk. The landscaping and trees in the planter strip are to be installed by the developer. Trees in the strip will be maintained by the Lighting and Landscaping Maintenance District as specified in the Development Agreement. The groundcover and shrubs are to be maintained by the adjacent property owner. The sidewalks are connected to pedestrian pathways in the landscaped

corridors along the arterial and collector streets, and pathways in the open space areas. The overall goal is to create a pedestrian-friendly community which encourages people to walk.

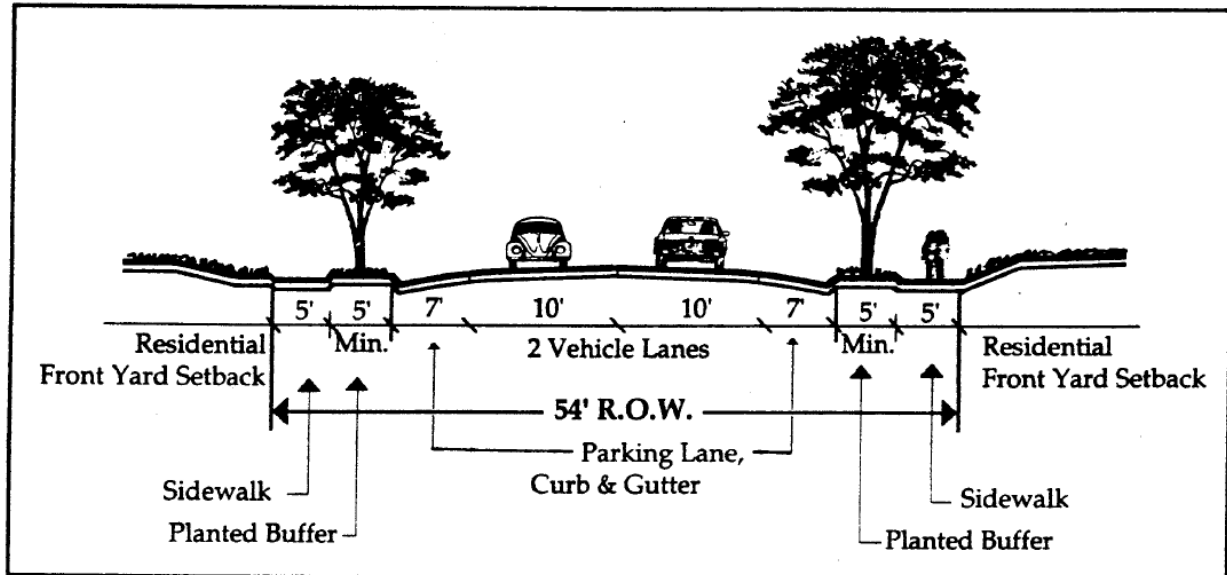


Figure 5-11: Typical Local Residential Street

The streets in the residential neighborhoods are purposely not designated in the Specific Plan in order to provide design flexibility in tentative subdivision maps. The local residential streets will connect to the collector streets and, generally will be limited in their connections directly to a major arterial street. Where local streets intersect arterial roadways, an entry island similar to that illustrated in Figure 5-8, Typical Entry Island Detail, for collector roadways shall be provided. Although the points of connection to arterial roads are to be limited for safety reasons, the interior road system is intended to facilitate access within the neighborhood. Streets may be generally curvilinear, but are to provide a sense of direction and orientation for the person unfamiliar with the street pattern. The street pattern should allow for reasonably direct routes to commercial centers and to the neighborhood schools and parks. To facilitate this goal, the pattern of local residential streets shall be designed to connect to collector streets at intervals of not more than every eight hundred linear feet (800'), where feasible.

## 5.2 Intersections

The capacity of intersections to carry traffic quickly and safely is a paramount matter of public policy in the City of Roseville. All intersections must be capable of operating at a Level of Service (LOS) of "C", or better at all times. This standard is achieved by a combination of the following measures:

- Land use planning proportioned and distributed to reduce vehicular trips.
- Signalization controls and appropriate design of intersections to optimize the flow of traffic.
- Use of Transportation System Management (TSM) programs, including support of alternative commute modes.
- Construction of extraordinary improvements such as grade separated intersections or single point diamonds when necessary.

All intersections within the North Central Plan Area benefit significantly from the implementation of TSM measures and the prohibition of peak hour turning movements at intersections beyond the Plan Area.

### 5.2.1 Grade Separated Interchanges

The traffic analysis prepared for the North Central Roseville Specific Plan

(Fehr & Peers Associates, June, 1989) identified two (2) key intersections in the North Central Specific Plan, Roseville Parkway at Harding Boulevard and Roseville Parkway at Pleasant Grove Boulevard, that would potentially require a grade separated improvement to operate a LOS "C" or better. This would occur under a scenario that assumes full buildout of all four Specific Plan Areas within Roseville, two-thirds development of all other areas in South Placer, and the equivalent of two regional mall sites in the North Central Specific Plan Area. These intersections are improved to LOS "C" or better by full or partial grade separation. The current and planned LOS capacities are summarized in Table 5-2. Other intersections within Roseville outside the North Central Plan have also been identified in city-wide traffic studies as exceeding the LOS "C" standard under buildout conditions. These intersections as well as those in the North Central Plan, are mitigated with improvements funded by the city-wide traffic mitigation fee approved by the Roseville City Council in December, 1988.

**Table 5-2  
Key Intersections Within the  
North Central Roseville Specific Plan Area  
Potentially Requiring Grade Separated Interchanges**

Intersection	Unmitigated V / C Ratio	Unmitigated LOS	Grade Separated Mitigated V / C Ratio	Mitigated LOS
Roseville Parkway & Harding	1.00	E/F	0.73	C
Roseville Parkway & Pleasant Grove	0.86	D	0.60	A/B

Source: Fehr & Peers Associates, June, 1989

Grade separated intersections allow the traffic on one arterial to pass. Through an intersection unimpeded by a traffic signal. Traffic on the secondary arterial, and those wishing to make a turn at the intersection, are channeled through a broad, signalized intersection on a level either above or below the predominant through traffic. Such intersections typically require a bridge structure and ramps to carry turning traffic into the intersection. The width of the ramps is no greater than the normal acceleration and deceleration lanes associated with a standard intersection. However, they require more width than a standard, at-grade intersection to accommodate the cut or fill slopes required for the change in elevation of the ramp. The interchange can be accommodated in the right-of-way for the planned, at grade intersections because the right-of-way and the landscaped corridors are widened to accommodate the additional turn lanes. Figure 5-12, Single Point Diamond Interchange, illustrates a typical grade separated Interchange as may be ultimately required at the Roseville Parkway/Harding Boulevard interchange. Any grade separation at the Roseville Parkway/Pleasant Grove Boulevard intersection would likely be limited to a left turn flyover from Roseville Parkway west onto Pleasant Grove Boulevard as illustrated in Figure 5-13, Left Turn Flyover. Alternately, a triple-left turn would also

accommodate traffic at Level of Service "C". The residential neighborhood on the southeast corner of the Roseville Parkway/Pleasant Grove Boulevard intersection will have a special landscape setback design to provide a buffer from the intersection. The buffer will include a small neighborhood park and landscaped strip on the residential side of the sound wall and a landscape corridor along the arterials. Homes will face the park across a single-loaded residential street, as shown in Figure 5-13a, Roseville Parkway/Pleasant Grove Boulevard Intersection Design Plan.

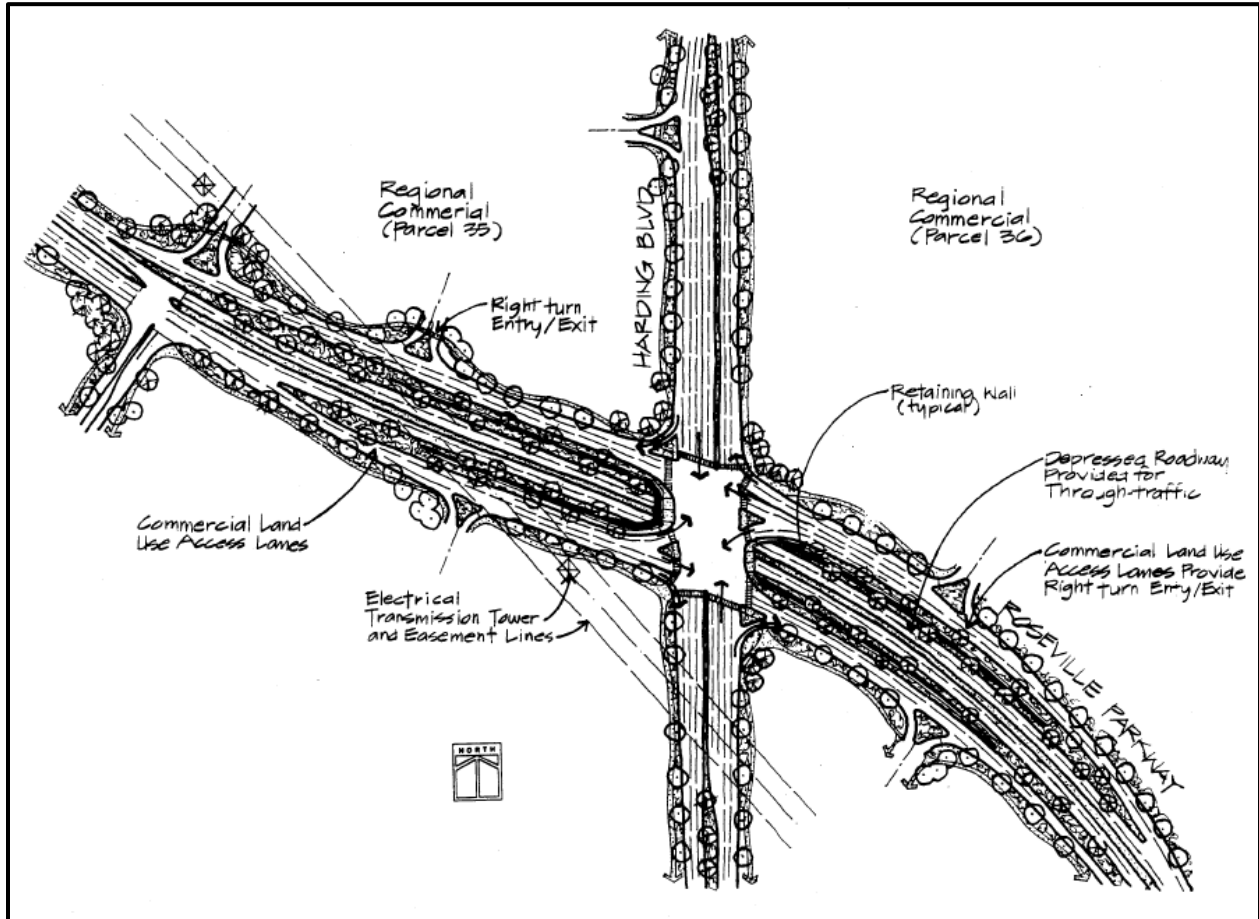


Figure 5-12: Single Point Diamond Interchange

Additional width for the grade separated facilities will be set aside on the large lot tentative map for the Plan Area. This additional width may partially encroach into adjacent landscape corridors. However, a minimum thirty foot (30') wide landscaped corridor, measured from the edge of the ultimate right-of-way of the completed interchange, will be provided at both locations. The Roseville Parkway and Harding Boulevard grade separated interchange design should be low profile to minimize visual impact to adjacent properties. Both intersection designs shall consider the safe and convenient access of pedestrians and cyclists. The bike/pedestrian system will be channeled through the intersection level for both those wishing to make a turn and those traveling through the intersection. Access to parcels adjacent to the interchanges would be limited to right turn only ingress and egress movements. The grade separated interchange will need to be designed to accommodate the future extension of light rail into the Plan Area.

The traffic monitoring program, discussed in Section 5.7, of this Element, is intended to monitor traffic, housing and employment such that the potential need for a grade separated interchange would be recognized in sufficient time to allow the appropriate measures to be taken before a serious deterioration of traffic service occurs. Major circulation improvements throughout Roseville, such as a planned interchange, are identified in, and funded through, the City Capital Improvement Program.

### 5.2.2 Signalization

The major arterial streets within and adjacent to the Plan Area will require signalization of intersections in accordance with the plan depicted in Figure 5-1, North Central Roseville Circulation Master Plan. Installation of these signals will be in accordance with the sequencing plan described in Section 7 of this Plan.

### 5.3 Bikeways/Pedestrian Pathways

Bicycling and walking are alternatives to driving that people may use regularly for short trips if they can proceed in relative safety. Moreover, children and bicycle enthusiasts will make extensive use of bicycle trails for both recreation and transportation.

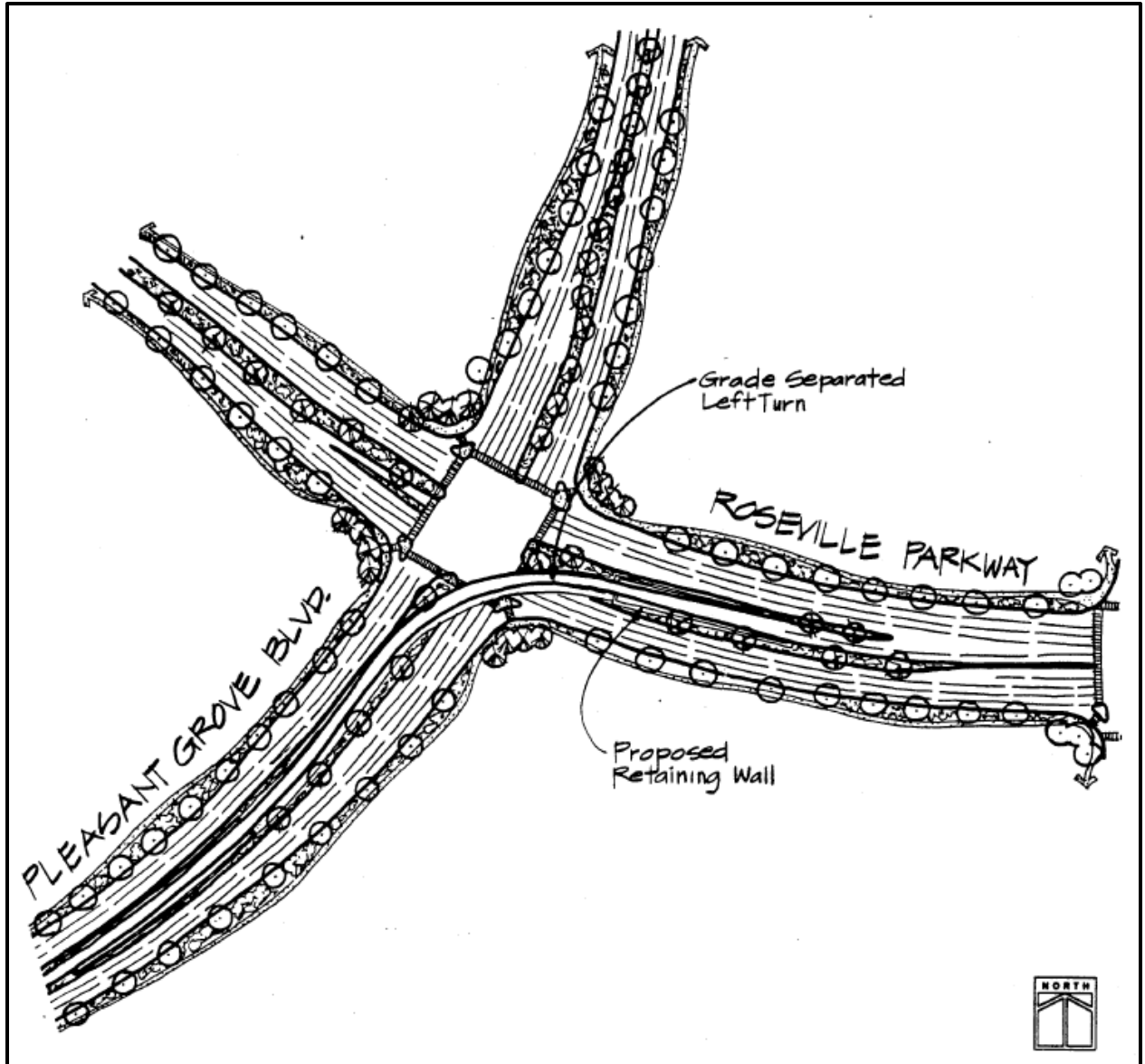


Figure 5-13: Left Turn Flyover

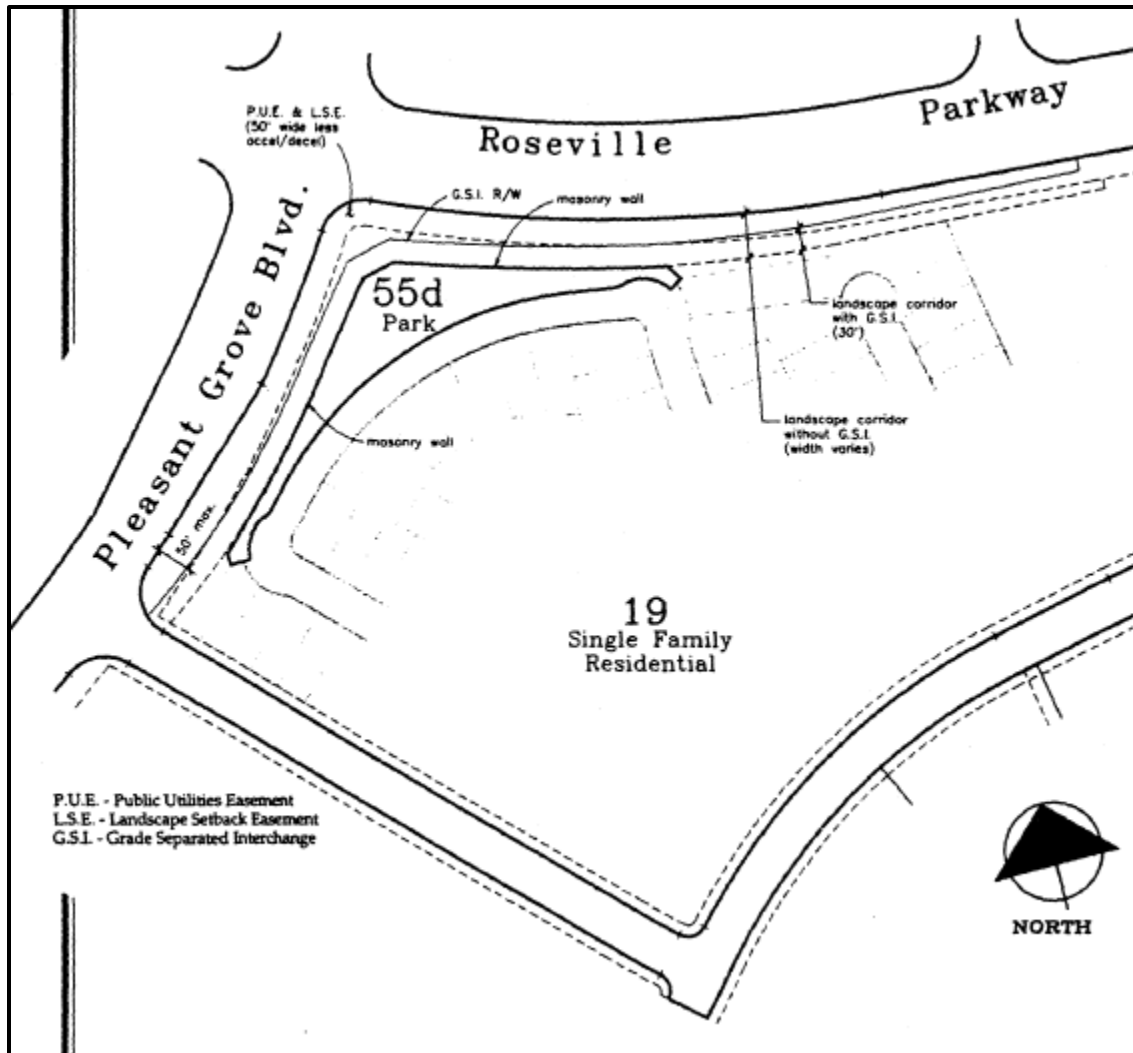


Figure 5-13a: Left Turn Flyover

The use of bicycles and walking is strongly encouraged in the North Central Plan Area through the placement of land use to reduce the distance people must travel, by ensuring convenient access to services and shopping, and by providing an extensive network of walking and cycling paths.

The Plan Area provides a system of bikeways and pedestrian pathways for recreation, commuting and other non-recreational purposes. The system consists of three key elements: the primary (backbone), secondary, and tertiary systems. These elements are organized in a connected hierarchy of pathways designed to take a resident from their front door to the inter-city network.

### 5.3.1 The Primary Pathway Network

The primary pathway network consists of two Class I bike path systems. Class I bike paths will be located within the landscape corridors adjacent to all arterial and collector streets. These paths will be interconnected with a second Class I bike path system within the lower watershed preserves, park and park preserve areas, Antelope Creek floodway, and special easements within neighborhoods and non-residential areas. These components form a network that allows a cyclist or pedestrian to travel through the Plan Area along a separate trail system with only minimal street/pathway intersections. A cyclist or

pedestrian need travel no more than a few blocks before connecting with a designated bike path that will connect to the primary pathway system.

The primary pathway system will connect to the city-wide bicycle network on Washington Boulevard, Pleasant Grove Boulevard, Harding Boulevard, Roseville Parkway and Antelope Creek. The primary bicycle I pedestrian system is illustrated in Figure 5-14, Bikeway Master Plan.

The Class I bike paths along the arterial and collector streets consist of slightly undulating, eight foot (8') wide concrete paths, totally separated from the streets within the landscaped corridors. The pathways are designed to accommodate both pedestrians and cyclists. Bikeways are intended to provide a safe and convenient route for commuting cyclists at a reasonable speed. Consequently, the alignment of the routes should not vary so greatly that the safe and convenient movement of cycle traffic is inhibited.

Pathways within the watershed preserve corridors will be placed so as to not interfere with the natural hydrology of the wetland areas. Specific design of the pathway, in the lower watershed reserve areas, may be subject to the provisions of the Corps of Engineers 404 permit. The pathways will be constructed with funding provided by the Community Facilities District. The drainageways cannot be landscaped due to the sensitive character of the native wetland species. Therefore, the bikeway will be located near the south or west edge of the drainageway, when possible, so that the landscaping for adjacent uses can provide shading along the path. Landscape requirements for individual land uses are discussed in the appropriate sub-sections of the Community Form and Design Element, and are specified in more detail in the Landscape Design Guidelines. Connections across the corridor to adjacent parcels will be designed into the system. Individual parcels abutting the corridor shall provide for at least one point of access to the pedestrian path system within the corridor.

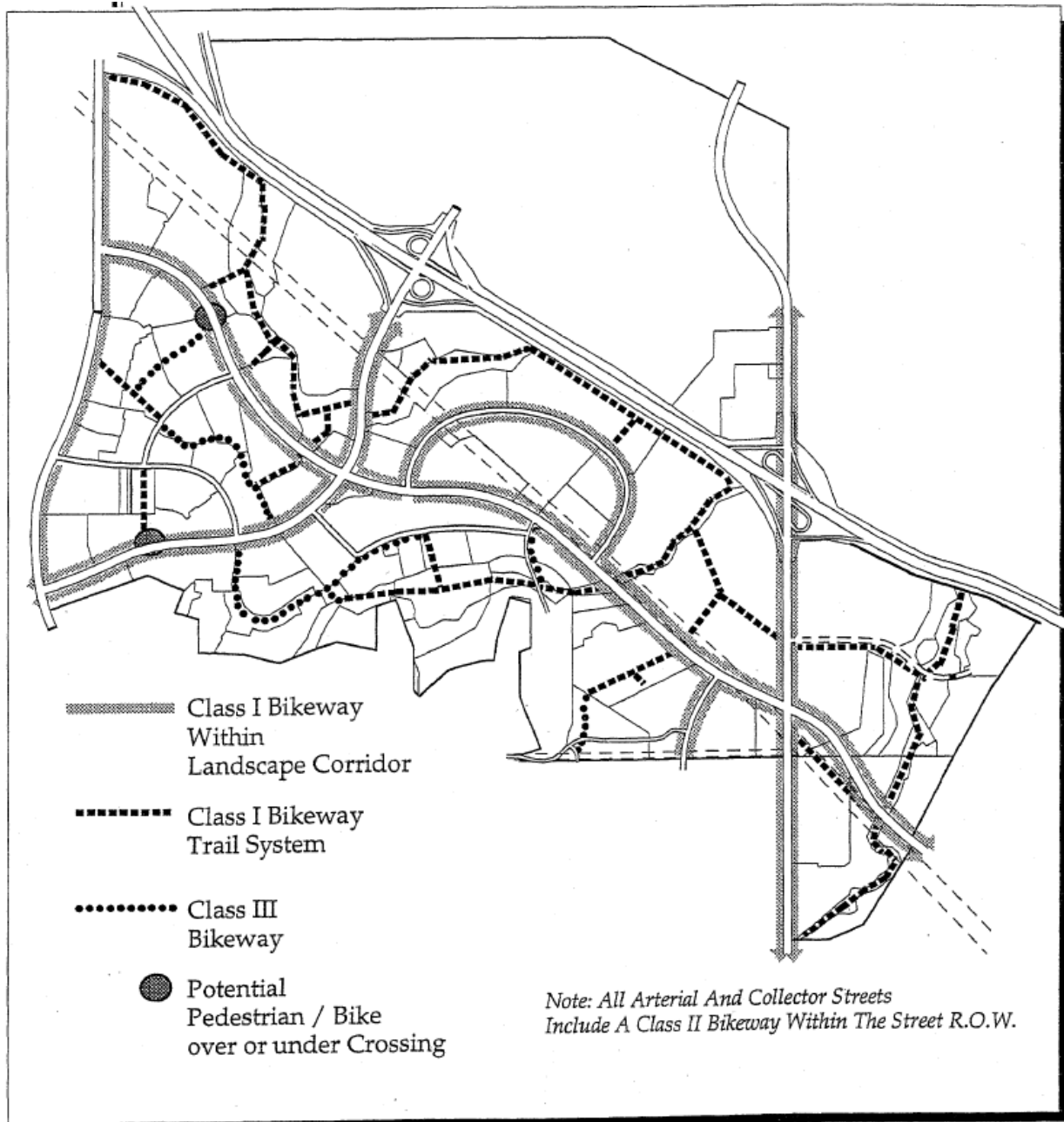


Figure 5-14: Bikeway Master Plan

Where bikeways are located outside the dedicated watershed, an easement for the bikeway will be granted. Easements shall be granted in Parcel 43, adjacent to lower watershed (Parcel 83); in Parcel 40 north of lower watershed (Parcel 82); southeast of Parcel 11 in lower watershed (Parcel 81A), and in the southern portion of Parcels W and X, adjacent to lower watershed (Parcel 80B). In no case will any buildings, parking facilities or other non-landscaped element be located closer than ten feet (10') to the edge of the bike trail. The general location of easements is shown on Figure 5-14, Bikeway Master Plan.

In order to make connections between Parcels 82 and 83, and between Parcel 84 and Washington Boulevard, a forty-foot (40') wide landscape easement will be provided along the Highway 65 frontage

of Parcels 40 and 42 and a fifty-foot (50') wide bike trail corridor will be provided adjacent to the southern boundary of the light industrial reserve sites (Parcels 48B and 49B). These landscape and bike trail corridors will incorporate eight (8') wide trail connections. The chain link fence at the edge of the State right-of-way and landscape corridor will separate the bikeway from the Highway 65 right-of-way. The bikeway will be set back at minimum of ten feet (10') from the edge of State right-of-way. The relationship of these easement to the adjacent land uses is specified in the Community Form and Design Element.

A pedestrian and bicycle path will be located in dedicated public access easements along the Antelope Creek drainage corridor. The path will connect to the approved city-wide floodway trail system and may also connect north into Rocklin. Roseville Parkway will be elevated over Antelope Creek and will carry the bike path across I-80 to the Northeast Plan Area. Cyclists will be able to travel from the Parkway to the bike path along Antelope Creek via Harding Boulevard and the Class I system across Parcel 36.

The access from the bike and pedestrian paths to adjacent uses is ensured through the policies of this Specific Plan which require safe, clear and direct connections between land uses and the adjacent pathways. Cycling for shopping and home-to-work commuting is supported by provisions of the City's Transportation System Management (TSM) Ordinance which promotes adequate bicycle parking facilities in commercial and employment generating projects. In addition, the TSM Ordinance specifies the provision of bike storage and showers at employment locations as a means of complying with trip reduction requirements.

### 5.3.2 The Secondary or Collector Path System

In addition to the Class I network, a Class II bike lane system will extend throughout the Plan Area within the right-of-way of collector streets and arterials. The bike lanes will be seven feet (7') wide, located adjacent to the travel lanes with signage and a stripe on the pavement demarcating the edge of the lane. Class II bike lanes will be provided on both sides of all arterials and collector streets.

### 5.3.3 Tertiary Bikeway System

All local streets within the residential neighborhoods will serve as Class III, on-street bikeways. This system will connect to the Class I and Class II systems through direct local street connections with collector streets and arterials, and through the pedestrian access connection points specified in the Community Form and Design Element. The land use policies specify a land use pattern that facilitates access from the neighborhoods to the secondary and primary bikeway system. The neighborhoods are also designed to restrict through vehicular traffic thereby making the street safer for cyclists.

### 5.3.4 Commercial Core Pedestrian Corridors

Pedestrian circulation is of particular importance in the commercial core area. This area is planned to accommodate a regional commercial site as well as a substantial retail and service commercial, restaurants, theaters, hotels, offices, and possibly, multi-family residential uses. The core area is conceived as a pedestrian oriented mall that provides safe and convenient walkways throughout, and from one side of Harding Boulevard to the other. Pedestrian corridors, will extend from buildings through the parking areas to connect across Harding Boulevard. The corridors or "promenades", will be landscaped walkways of sufficient width to allow groups of people to walk, and to provide seating areas. Landscaping will provide a shade tree canopy and will buffer the pedestrians from the adjacent parking.

The promenade will cross Harding Boulevard as a walkway at street level protected by a traffic signal. The promenade will include rest islands at the center medians, special lighting and a special paving texture or markings. Similar promenades, although smaller in scale, shall be provided on Roseville Parkway between Parcels 35 and 39, and across the lower watershed area between Parcels 35 and 40. In addition, a promenade will extend down the bluff on the east side of Parcel 36 to connect with the business-professional uses, the multi-family residential uses, and the bikeway along Antelope Creek. Specific standards relating to the promenades and other pedestrian paths are included in the Community Form and Design Element.

#### 5.3.5 Pedestrian Crossings

To facilitate the safe, convenient and comfortable crossing of pedestrians and cyclist across arterial roadways, all lighted intersections along arterials shall incorporate enhanced pedestrian crossings. The crossings shall incorporate increased distance between the crosswalk area and vehicle limit line, and enlarged median areas sufficient to allow pedestrians and cyclists to comfortably stop and wait between light cycles.

Three locations are identified on Figure 5-14, Bikeway Master Plan for the potential future construction of pedestrian overcrossings. Adequate right-of-way is included within the landscape corridors to permit construction should the City desire.

### 5.4 Public Transportation

#### 5.4.1 Bus Service

Bus service is the only form of public transit that could be extended immediately within the North Central Specific Plan Area. The City of Roseville is currently served by three public transit systems. Greyhound is under contract to provide commuter bus service between Roseville and Sacramento. Roseville Urban Shuttle (RUSH) provides regularly scheduled fixed route service, and Roseville Area Dial-A-Ride (RADAR) provides on-call service throughout the City. The RUSH and RADAR systems will be expanded to the Plan Area as demand for these services occurs and funds are available as determined by the City. Bus turnouts will be provided as determined by the City Public Works Department to accommodate future needs. Likely locations include intersections and mid block turnouts adjacent to major projects or public facilities. All residents and employees within the Plan Area will be in close proximity to potential bus routes. This level of service will facilitate ease of access and encourage high levels of ridership.

#### 5.4.2 Light Rail Service

The Sacramento Area Council of Governments (SACOG) and the Regional Transit District (RTD) are investigating the potential extension of light and/ or heavy rail to outlying communities in the Sacramento region. The City of Roseville is participating in these studies to try to obtain an extension to Roseville. A light rail connection between Sacramento and the commercial core in the North Central Specific Plan Area would provide a logical linkage within the region. Preliminary discussions with RTD staff have indicated that alignments can be accommodated within the landscape corridors of Roseville Parkway and Harding Boulevard, as shown on Figure 5-15, Potential Roseville Light Rail Alignments. Final decision on the alignments may be made sometime in 1991. Until a final decision is made, any land use development along potential routes must be designed to accommodate the possibility of light rail

including right-of-way and station locations. The extension of light rail into the North Central Plan Area is expected to be of significant benefit to adjacent land uses.

The fifty foot (50') wide landscape corridors along Harding Boulevard and Roseville Parkway are wide enough to accommodate a double track light rail line. Where the bikeway in the landscaped corridor abuts the light rail tracks the bikeway will double as a maintenance route and a guard fence will be provided for pedestrian and cyclist safety. The configuration of the arterial street corridor with the light rail line is shown in Figure 5-16. Right-of-way will be preserved in accordance with the provisions of the Specific Plan Development Agreements. Any grade separated interchanges and bridges along Roseville Parkway and Harding Boulevard shall be designed to accommodate the potential future extension of light rail.

### 5.5 Park and Ride Lots

Park and ride lots will be located throughout the Plan Area to provide convenient places for commuter car pooling. Additional park and ride activity will likely occur through the sharing of rides among residents in the Plan Area who pick one another up at or near their homes, and thus, do not require a central park and ride facility.

The park and ride lots will typically include approximately 25 to 50 spaces incorporated in the parking for a commercial, business-professional/commercial, or park use. Park and Ride spaces shall be provided as follows:

Parcels 35, 36 and 56	50 spaces
Parcel 34	35 spaces
Parcels 31, 32, 38 and 49b	25 spaces

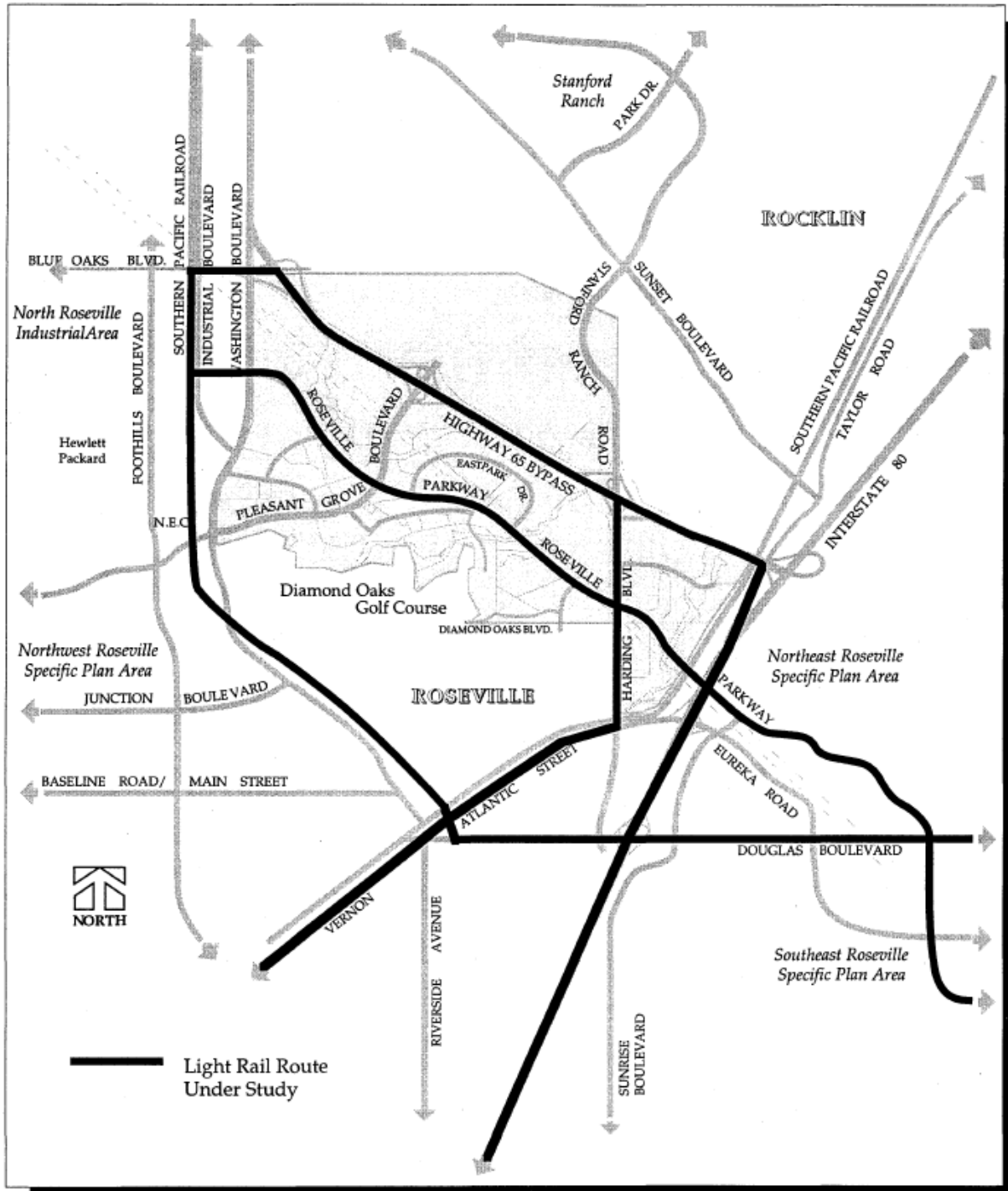


Figure 5-15: Potential Roseville Light Rail Alignments

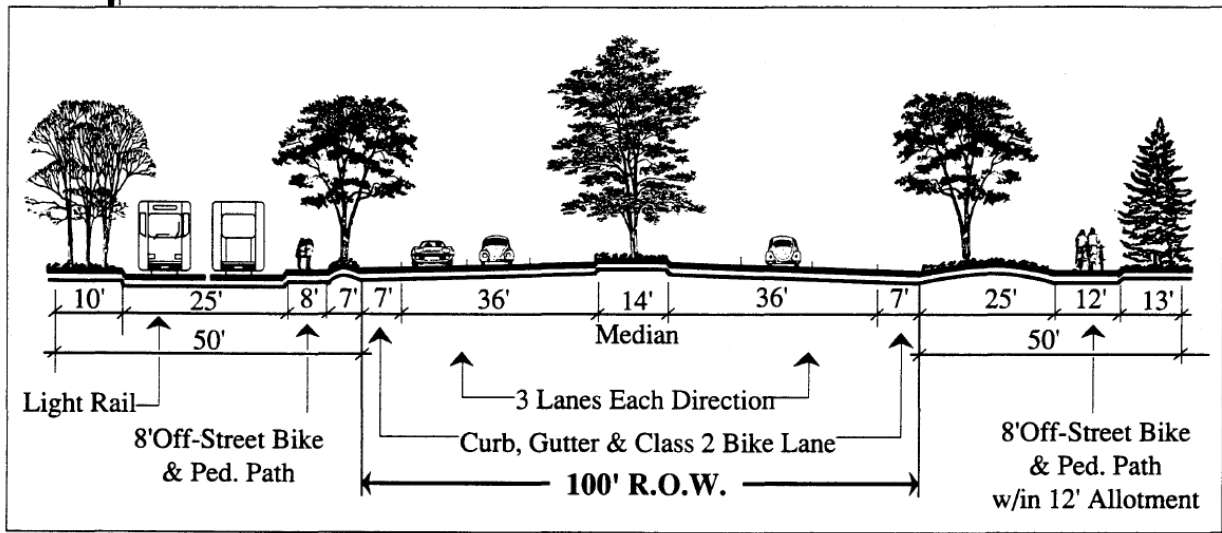


Figure 5-16: Typical 100' R.O.W. (If Light Rail Occurs)

Where park and ride lots are within a business parking area, they should be located so as to not interfere with business operations. The park and ride spaces may be included as part of the normal parking requirement for the planned business or commercial use if peak use of the park and ride does not coincide with peak use of the business or commercial use and if approved by the City. In all cases, the park and ride spaces are to be clearly marked through pavement markings and directional signage. Spaces are reserved exclusively for park and ride Monday through Friday from 6:30 a.m. to 6:30 p.m.

### 5.6 Transportation System Management

Traffic impacts on Roseville streets can be reduced through TSM measures which encourage employees to rideshare and to travel at times outside the normal peak travel periods. Based on research conducted by Caltrans, the Institute of Transportation Engineers and Fehr & Peers Associates, the traffic reductions which can reasonably be realized by these programs are summarized in Table 5-3, Estimate of TSM Effectiveness.

**Table 5-3  
Estimate of TSM Effectiveness**

		Peak Hour Trip Reduction	
Type of Employment	Ridesharing	Flex Time	Total
Business-Professional	7%	4%	11%
Industrial (General or Light)	7%	9%	16%

Traffic estimates based on trip generation rates for business-professional and industrial land uses in the North Central Specific Plan Area were reduced in accordance with these estimates of effectiveness. Projects in the Plan Area, therefore, will need to provide trip reduction levels consistent with the estimates in order to be consistent with the Plan Area EIR.

The City of Roseville adopted an ordinance implementing TSM in February, 1983. The ordinance is directed at reducing the number and length of home-to-work commuting trips through a variety of methods. The predominant method is private ridesharing programs in which employers are required to participate in the Sacramento Rideshare Program or a locally sponsored, comparable program. All projects in the North Central Roseville Specific Area are required to comply with the provisions of the City's Rideshare Ordinance and any future amendments thereto.

## 5.7 Traffic Monitoring Program

The North Central Roseville Specific Plan contemplates the development of a major transportation system over a period of years. Most of the basic circulation infrastructure will be constructed under a single funding program; however, expansion of the system for additional lanes or improved intersections may occur through the City's Capital Improvement Program as the traffic levels dictate. Changes in traffic demand brought about by new types of vehicles, changes in lifestyle, more effective TSM measures, improved public transportation and the cost of private transportation, may all have an effect on traffic patterns over time. Furthermore, some of the improvements that may be required in the future are not definite and would be prohibitively expensive to build until the need is actually demonstrated. Consequently, the City Public Works Department will maintain a program of monitoring traffic conditions over time and implement the Capital Improvement Program as needed.

Traffic monitoring will be used to detect changes in traffic patterns that are approaching Level of Service "C" for intersections throughout the City. Level of Service "C" will be as defined in the 1985 edition of the Highway Traffic Manual. The Roseville Public Works Department will monitor key locations through a combination of site traffic counts and techniques of modeling the effects of cumulative growth attributable to approved development projects. From this, a list of traffic projects to be funded in a five-year capital improvements program will be maintained. The program will be updated on an annual basis and new projects added to the five-year program as LOS "C" is approached in specific locations. The monitoring program will be conducted on a city-wide basis and the funding of specific improvements will come from a combination of the traffic mitigation and development fees, conditions in various Development Agreements, assessment districts, and other sources.

## 5.8 Specific Plan Circulation Policies

### 5.8.1 Street System Design and Layout

1. Streets shall conform to natural topography and vegetation to the extent possible.
2. The right-of-way requirements, land configurations, landscape corridors and pathways for all arterial and collector roadways shall be as specified on Table 5-1 and the associated exhibits and figures.
3. The landscape corridors shall be granted as landscape easements over private property, except that corridors adjacent to single family uses shall be dedicated in fee to the City. Such corridors shall flair at corners as illustrated on Figure 5-5, Typical Corner Setback. All landscape corridors are to be landscaped consistent with the provisions of the Community Form and Design Element and Landscape Design Guidelines.
4. Vehicular access along Roseville Parkway is limited and shall be restricted to points approved by the Public Works Director. Parking on all arterial streets shall be prohibited by posting.
5. To provide an acoustical and visual buffer, a six foot (6') high masonry wall shall be provided along arterial streets adjacent to single family residential neighborhoods; a six foot (6') high

wood and pilaster fence shall be provided between single family residential uses and residential Collectors A and B, and a six foot (6') high masonry wall shall be provided between single family residential uses and all other collector streets. Alternate site planning treatments as illustrated on Figure 5-9, Alternative Site Plans Adjacent to Residential Collector Streets, shall be considered along collector streets in order to avoid a continuous fence for extended distances. Pedestrian access breaks shall be provided per the policies of the Community Form and Design Element. Acceleration and deceleration lanes at intersections between arterial and/ or collector streets will be accommodated, but in no case shall the landscape corridor be reduced to less than thirty feet (30') from the back of curb.

6. Intersections of collector streets with major arterial streets shall be kept to a minimum. Collector streets should not intersect with a major arterial street closer than six hundred feet (600') from an intersection formed by two arterial streets or closer than three hundred feet (300') from another collector/arterial intersection. All residential collector intersections with an arterial shall provide an entry island as illustrated on Figure 5-3, Initial 4-Lane Configuration with Median Expansion to 6-Lanes.
7. The alignment of Street E will be dependent on development of Parcel 36 and the final delineations of wetlands to be preserved on the parcel. The dimension of landscaping strip and landscaping will be determined at time of alignment. A five foot (5') sidewalk, a landscape strip between the sidewalk and curb not to exceed five feet (5'), and landscaping behind the sidewalk shall be provided along Street E.
8. On-street parking may be permitted on all collector and local streets. Local street intersections with arterial streets shall generally be kept to a minimum. Where such intersections occur, an entry island shall be provided similar to that illustrated on Figure 5-8, Typical Entry Island Detail, for collector streets.
9. Local residential streets shall be designated through the subdivision review process. They shall typically intersect a collector street at intervals of not more than every 800 linear feet (800') to facilitate neighborhood access and to avoid concentrating traffic on residential streets to limited access points within a neighborhood.
10. Local streets shall provide a five foot (5') wide sidewalk on both sides of the street separated from the curb by a five foot (5') wide planting strip. Tree planting and landscaping within the strip is the responsibility of the developer. Trees in the planter strip shall be maintained by the City through the Lighting and Landscape Maintenance District. Groundcover and shrubs are to be maintained by the adjacent property owners.
11. Intersections between local residential streets and collector streets should not occur closer than one hundred fifty feet (150') from the intersection of a collector street and arterial street.
12. Local streets within residential neighborhoods shall be designed to discourage outside vehicular traffic from passing through the neighborhood.

### 5.8.2 Intersections

1. Any grade separated interchange at Roseville Parkway and Harding Boulevard shall be of low profile design.
2. The right-of-way required for grade separated interchanges at Roseville Parkway /Harding Boulevard and Roseville Parkway /Pleasant Grove Boulevard shall be reflected on the large lot subdivision map for the Specific Plan. A minimum thirty feet (30') wide landscape corridor shall

be provided from the edge of ultimate right-of-way. In addition, an approximately one acre neighborhood park (Parcel 55D) will provide an additional buffer between the grade separated interchange at Roseville Parkway/Pleasant Grove and the single family residential land uses to the southeast.

3. All grade separated interchange at Roseville Parkway and Harding Boulevard shall allow easy pedestrian/bicycle crossings and accommodate the potential extension of light rail into the Plan Area.
4. All projects adjacent to intersections where grade separation may be required shall consider such intersections in project design.

### 5.8.3 Bikeways and Pedestrian Paths

1. Bicycle and pedestrian circulation systems shall be designed to minimize conflicts with the vehicle circulation system. Separation of the cyclist/ pedestrian from the automobile shall be provided to the extent feasible along all Class I bike paths.
2. Connections to the pathway systems from adjacent land uses shall be as specified in the Community Form and Design Element.
3. Routes should be carefully chosen along natural grades to protect native vegetation and reduce grading. The routes should not exceed a gradient of seven percent (7%), where possible.
4. Class I bike paths doubling as pedestrian walks shall be a minimum of eight feet (8') wide and shall be constructed of cement or other material appropriate to the natural conditions as determined by the City of Roseville. All Class I bikeways within the roadside landscaped corridors and public open space areas shall be maintained by the Lighting and Landscape Maintenance District
5. Class I bikeways within the landscaped corridors shall be designed to allow safe and convenient bicycling by commuters. Bikeways shall not vary from a straight line by more than eight feet (8') in one hundred feet (100') of length. Landscaping and grade changes shall be employed as a means of separating pedestrian paths from streets.
6. Pathways adjacent to watershed preserves shall be placed so as not to interfere with the natural hydrology of the wetland areas. Pathways should be located near the south or west edge of drainageways when possible so that landscaping from adjacent uses may provide intermittent shading. Connections across the watershed corridors shall be designed into the system. Specific design of the system may be subject to the provisions of the Corps of Engineers 404 permit.
7. Where bikeways are located outside of lower watershed areas, a bikeway easement shall be granted. Buildings and parking facilities and other non-landscaped areas will not be permitted within ten feet (10') of the bikeway.
8. On-street, Class II bikeways of seven foot (7') width shall be provided on both sides of collector and arterial streets with proper signage and striping.
9. All pedestrian sidewalks shall be handicap accessible with curb cuts at all intersections.

### 5.8.4 Commercial Core Pedestrian Corridors

1. Pedestrian connections shall be provided in the commercial core area as described in policies contained in the Community Form and Design Element.

### 5.8.5 Pedestrian Crossings

1. All lighted intersections along arterial roadways shall incorporate enhanced pedestrian crossing. The crossings shall include increased distance between the crosswalk and vehicle limit line and enlarged median rest areas.
2. Three locations are located on Figures 3-6 through 3-8, Neighborhoods A, Band C, for the construction of potential future pedestrian overcrossings. All pedestrian overcrossings shall be handicap accessible and will be subject to design review approval by the City.

### 5.8.6 Public Transit

1. Alternatives to the automobile as the primary means of transportation shall be encouraged. Public transportation services, such as those provided by Placer Mini-bus, Roseville Urban Shuttle and by Roseville Area Dial-A-Ride (RADAR), shall be accommodated in the arterial and local collector street system. RUSH and RADAR systems will be expanded to the Plan Area as demand for these services occurs and as funds are available as determined by the City.
2. Bus turnouts and shelters shall be located consistent with City Improvement Standards as approved by the Public Works Director. Turnouts shall be provided at the time of roadway installation. Shelters and benches shall be provided by adjacent projects at the time of construction unless otherwise required by the City. The design of the shelters shall conform to the standard set forth in Section 3, Community Form and Design Element and the Landscape Design Guidelines. Major projects shall consider on-site bus stops in the project design.
3. Bus turnouts for school buses may be required as determined by the City on a project by project basis.
4. The City and project developers shall participate in the studies for the potential for future extension of the Sacramento light or heavy rail systems into the area, and shall participate in any future implementation programs pertaining to that opportunity as approved by the City.
5. The use permit or site review for any proposed land use on parcels adjacent to any potential light rail lines shall consider the effect of the proposed use on accommodating future light or heavy rail stations and rights-of-way. Such right-of-way shall be preserved in accordance with the provisions of the Specific Plan Development Agreement. Light rail may be accommodated within the landscape setbacks adjacent to arterials as illustrated on Figure 5-16. Projects adjacent to potential light rail corridors that are approved prior to a final decision on alignments, shall be designed, constructed and landscaped with the assumption that light rail will be constructed in the adjacent corridors.
6. Any grade-separated interchanges or bridges along Harding Boulevard or Roseville Parkway shall be designed to accommodate the potential future expansion of light rail.

### 5.8.7 Park and Ride Lots

1. Park and ride lots with fifty (50) spaces shall be located on Parcels 35, 36, and 56. Park and ride lots with twenty-five (25) spaces shall be located on Parcels 31, 32, 38, and 49b. Parcel 34 shall provide a park and ride lot with thirty-five (35) spaces. In all cases the park and ride spaces are to be clearly marked through pavement markings and directional signage. Spaces are reserved exclusively for park and ride Monday through Friday from 6:30 a.m. to 6:30 p.m.

2. A portion of the park and ride spaces may be included in the normal parking required for the planned business or commercial use if the peak use of the park and ride does not coincide with peak use of the business and commercial uses and if approved by the City.

#### 5.8.8 Transportation System Management

1. Employers shall be required to participate in the Transportation System Management Program pursuant to the Rideshare Ordinance of the City of Roseville and any updates or amendments thereto. Projects within the Plan Area will need to achieve trip reduction levels as indicated in Table 5-3 in order to be consistent with the Plan Area EIR.
2. Bike racks, storage facilities, lockers, and showers may be provided by all non-residential uses in accordance with the City Rideshare Ordinance.
3. Applications for a use permit for all non-residential uses shall include a transportation plan detailing trip reduction measures as required by the Rideshare Ordinance.
4. Employment and housing opportunities in the North Central Specific Plan Area will be monitored on an annual basis by the City of Roseville in the annual employee survey and in the annual housing monitoring program conducted by the City Planning Department.

#### 5.8.9 Traffic Monitoring Program

1. Traffic conditions will be monitored periodically by the City Public Works Department in the North Central Specific Plan Area at intersections and on the major arterial streets. As an intersection approaches Level of Service "C", as determined by site observation, traffic counts, or modeling of cumulative growth conditions, the improvements required to maintain LOS "C" shall be identified for funding in the city-wide five-year traffic system improvements program. Level of Service "C" is defined as a measure of delay at an intersection. For planning and evaluation purposes, the LOS "C" is assumed to be less than a vehicle/capacity ratio of .80.

## 6. Resources Management Element

It is a goal of the North Central Roseville Specific Plan to conserve natural resources and to reduce pollution associated with development in the Plan Area. Urban development will invariably modify the natural environment. However, careful community planning and care in placing and constructing urban land uses, combined with judicious use of open space, can result in a community that retains and incorporates the essential character and most valuable elements of the natural environment.

Human habitation also places demands on community and regional resources, such as potable water, clean air, and available energy. It is the purpose of this Element of the Specific Plan to address the conservation and management of both the natural resources and amenities within the Plan Area boundary, and the regional and community-wide resources that support urban development.

The North Central Roseville Specific Plan is designed to conserve natural resources and mitigate, to the extent possible, the impacts associated with development in the Plan Area. Resource management issues tend to encompass multiple objectives and an approach to one issue often affects other concerns. In this Specific Plan the land use, circulation system, open space, recreation, and other public facilities all support the complex objectives of maintaining natural amenities in an urban setting, and of minimizing negative effects on air quality, water quality, energy demand, and the natural and historic resources in the area. To a certain extent, resource management policies have been incorporated into the various other elements of the Plan and are inherent in the Plan Area land use pattern. This element provides the detailed implementation of resource management. The resources addressed in this Element include:

- Vernal Pools and Other Wetlands
- Oak Woodlands
- Soils
- Water Quality and Conservation
- Air Quality
- Historical and Cultural Sites

The following sections describe each of the resource categories, the concept for their use and protection, and the policies to implement these concepts.

### 6.1 Wetlands and Drainage Resource Management

There are five types of wetlands found within the North Central Roseville Specific Plan. Development of any area which directly impacts wetlands may be subject to a permit by the U.S. Army Corps of Engineers under Section 404 of the 1972 Clean Water Act, as well as the jurisdiction of other federal and state agencies. Depending on wetland size and species present, these agencies may have jurisdiction separate from, and concurrent to, that of the City. Requirements which will be imposed by the Corps of Engineers pursuant to the issuance of a 404 permit are intended to mitigate and compensate for the loss of wetlands from proposed development. In general, a no net loss policy is applied.

The five wetlands types found in the Plan Area are as follows:

## 1. Intermittent Drainage Above Headwaters

A system of intermittent channels drains the Plan Area generally from east to west. Channels are typically one to ten feet in width (bank to bank ordinary high water), devoid of vegetation and often flow over exposed substrate on the Mehrten formation. The channels carry run-off during winter storm events but are typically dry for the remainder of the year. Plant species diversity and wildlife habitat values are typically low. A total of 8.3 acres of intermittent drainage above headwaters occur in the Plan Area. The preserved headwater areas are included within the lower watershed preserve category on Figure 6-1, Preserve Delineation.

## 2. Perennial Stream

Antelope Creek is the only perennial stream in the Plan Area. A riparian oak forest (*Quercus* sp.), with emergent species and understory grasses, occurs along the creek. Riparian forest corridors typically provide habitat for a variety of wildlife species including passerine birds, waterfowl, raptors, mammals, amphibians, reptiles, and anadromous fish. A total of 3.2 acres of perennial stream occur in the Plan Area of which all is designated for preservation. The perennial stream area is represented under the lower watershed preserve category on Figure 6-1.

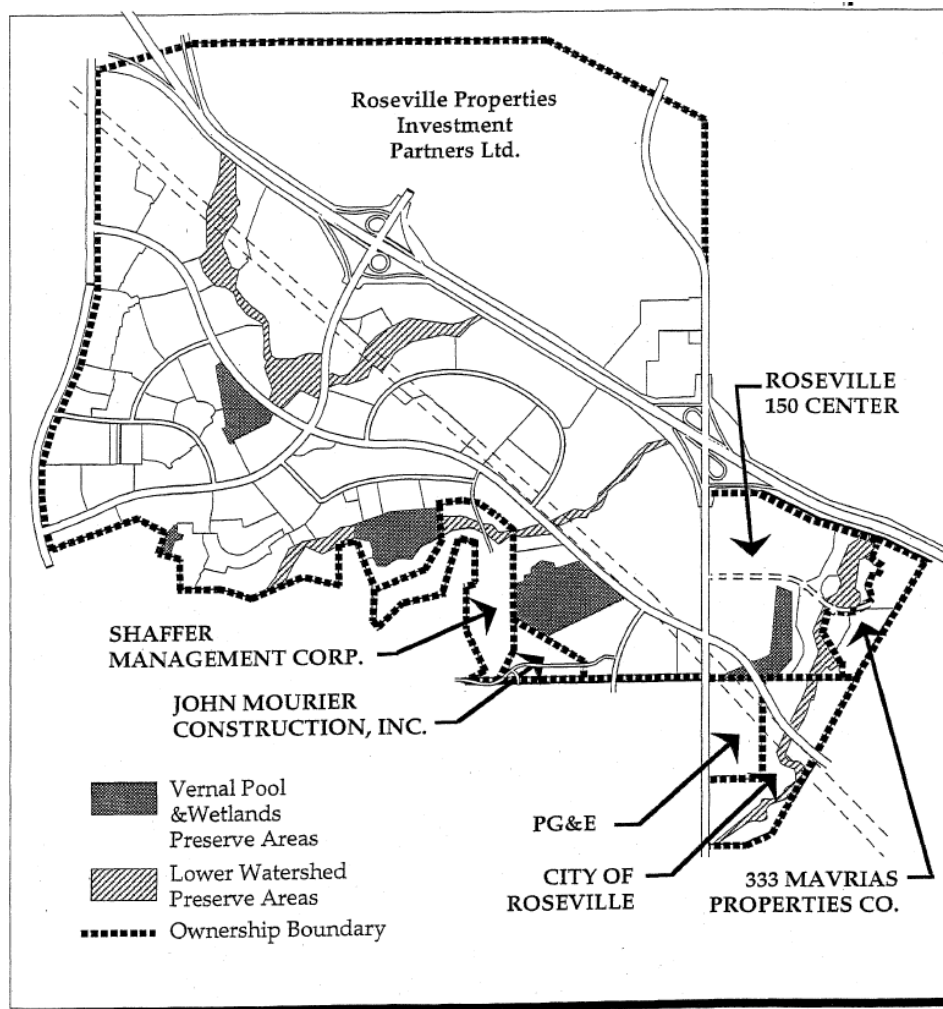


Figure 6-1: Preserve Delineation

### 3. Adjacent Seasonal Wetlands

Seasonally wet swales within drainage floodplains are classified as wetlands because they are poorly drained and remain saturated during wet winter months. Plant species diversity and wildlife habitat values are typically low. Dominant plant species include coyote thistle (*Eryngium vaseyi*), annual hairgrass (*Deschampsia danthonioides*), goldfields (*Lasthenia chrysostoma*), tarweed (*Holocarpha virgata*), Fitch's spikeweed (*Hemizonia fitchii*), and various mediterranean annuals. Soils are typically of the Inks series (SCS 1980) with a hardpan at approximately 12 to 24 inches. An organic layer occurs above a sandy loam or sandy clay loam. A total of 1.395 acres of adjacent seasonal wetlands occur in the Plan Area within the lower watershed preserve areas designated on Figure 6-1. All of this acreage is designated for preservation.

### 4. Northern Volcanic Mudflow Vernal Pools

Northern volcanic mudflow vernal pools occur in shallow concave depressions on Mehrten mudflow formations where the slope is generally less than two percent (2%). Soils in pools are typically characterized by a black algal mat above approximately 0.5 to 1.0 inches of moist, cobbly loam, underlain by massive hard andesite. The loam tends to have a very dark gray or dark reddish brown color.

Pool species diversity in the mudflow pools varies from a high of 37 species on the most easterly portion of the Plan Area to 15 species on better drained volcanic formations to the north and west. Waterfowl and shorebird use of pools was found to be minimal during a study conducted during the winter of 1988. (Sugnet, 1988)

A total of 13.9 acres of northern volcanic mudflow vernal pools are scattered throughout the Plan Area in upper watershed positions. The preserve areas are shown under the vernal pool and wetland preserve areas category of Figure 6-1.

### 5. Northern Hardpan Vernal Pool

Northern hardpan vernal pools occur primarily on Inks soils at lower watershed positions along Plan Area floodplains. Pools tend to occur in concave depressions in mounded topography. Soils in pool bottoms are typically sandy clays with iron mottles. Vernal pool species occur in pool bottoms, while species described previously under Adjacent Seasonal Wetland Swales, occur at upper pool elevations, transitioning into upland vegetation. A total of 4.8 acres of Northern Hardpan Vernal Pools occur in the Plan Area. The areas set aside for preservation are shown on Table 6-1.

#### 6.1.1 Wetlands Avoidance and Preservation

The land use pattern proposed in the North Central Roseville Specific Plan reflects an evaluation of practicable land use alternatives and proposes extensive avoidance of vernal pools and other wetlands. The plan includes the establishment of approximately 206 acres of preserves. The proposed preserves will protect known populations of the California endangered Bogg's Lake hedge hyssop and other species of high value. Table 6-1 presents "unavoidable" impacts to wetlands that will occur as a result of implementation of the Specific Plan, for each property owner within the Plan Area as of January, 1990.

Where wetlands are destroyed, compensation will be achieved "in-kind" and "on-site" wherever possible and practicable in accordance with the Corps of Engineers 404 Permit process.

**Table 6-1  
Projected Impacts to Wetlands Proposed for  
North Central Plan Area, October 1989**

<u>Property Owner</u>	<u>Total Wetland Acreage</u>	<u>Proposed Wetland Impacts (Acres)</u>	<u>Wetland Ac. Avoided (%)</u>
RMB Realty	19.721	6.9	70
Shaffer Mgmt.	1.247	0.8	36
Mourier Const.	0.321	0.321	0
Roseville 150 Center	7.177	4.9	32
333 Mavrias	0.544	0.47	14
PG&E	0.25	*	
City/Roseville	1.07	*	

\* No proposal at this time.

The State Department of Fish and Game recommends that a mitigation plan be developed that is based upon the concept of "no net loss" to wetland habitat values or acreage and no adverse impact upon any State listed sensitive plant or animal species. Consequently, the Specific Plan incorporates a comprehensive program of wetlands avoidance, creation of preserve areas, maintenance of the preserves, and enhancement of wetlands designed to achieve a cumulative "no net loss" of values.

Due to the scattered distribution of northern volcanic mudflow vernal pools and other seasonal wetlands, all wetlands in the North Central Roseville Specific Plan Area cannot realistically be avoided. In response to recommendations made by Laurence Stromberg and Sugnet and Associates, alternative land use plans were developed to avoid wetlands to the extent "practicable" according to guidelines established by the Environmental Protection Agency (EPA), U.S. Army Corps, U.S. Fish and Wildlife Service (USFWS), and the California Department of Fish and Game (CDFG). The proposed Specific Plan Land Use Map represents the least environmentally damaging practicable alternative given the land use allocation. (Sugnet, 1989) Significant plan revisions, as compared to earlier drafts, have been made to avoid wetland areas. Such revisions include the relocation of roadways and reconfiguration of parcels to create preserve areas.

The Specific Plan includes the designation of 14 wetland preserves which incorporate many of the highest quality wetlands in the vicinity. The preserve areas will be dedicated to the City to insure long-term preservation and maintenance. The location of the preserves are shown in Figure 6-1, Preserve Delineation, and are summarized below:

1. A 13.67 acre preserve (Parcel 91) is established adjacent to a neighborhood park and a neighborhood school to protect an area of northern hardpan pools.
2. A 33.04-acre preserve (Parcel 93) is established to protect a cluster of high quality mudflow pools in the southeast quadrant of the Plan Area. Wetlands in this preserve support approximately 32 vernal pool plant species.

3. Two wetland mitigation areas (Parcels 95 and 96) have been established in the southeast portion of the Plan, east of Harding Boulevard. The boundary of Parcels 95 and 96 may be altered to reduce wetlands disruption in accordance with Mitigation Measure 3.4-26 of the Draft EIR. In the event that such boundary alteration is not feasible or does not adequately reduce impacts, off-site mitigation, or a combination of off-site and on-site mitigation, may be proposed by the landowner. Concurrent with the submittal of any request to the Army Corps of Engineers for off-site mitigation such submittal shall be forwarded to the City for Planning Commission and City Council review. If any significant modifications occur to the proposal during or as a result of the Corps of Engineers' review process, the proposal shall be resubmitted for City review. The City may approve or deny any request to relocate off-site any of the acreage shown for Parcels 95 and 96, and the review of such request shall be made in accordance with the requirements of CEQA.
4. An 18.03 acre lower watershed area (Parcel 87) has been established in the south central portion of the Plan Area and includes 6.9 acres of vernal pool preservation. The watershed area is located adjacent to a neighborhood park and a neighborhood school.
5. A 1.81 acre wetland preserve area (Parcel 90) is designated at the northwest corner of Diamond Oaks Golf Course. This preserve includes a small wetland/ riparian area that has been created primarily as a result of drainage from the Golf Course.
6. Throughout the Plan there are 107 acres of lower watershed preserves (Parcels 80-86, 88, and a portion of 87) created to protect the northern hardpan pools adjacent to drainage channels. Significant realignment and channelization of intermittent drainage channels is not permitted and ninety-four percent (94%) of intermittent channel acreage is protected within the lower watershed preserve areas.

#### 6.1.2 Wetlands Maintenance and Monitoring

Associated with designation of the wetland preserves are measures incorporated to minimize impacts on the areas from adjoining properties and uses. A monitoring system will be established pursuant to the 404 Permit. The landowner will be responsible for required 404 monitoring on Parcels 93, 95, and 96. These parcels will not be dedicated to the City until the monitoring provisions have been completed to the satisfaction of the Corps of Engineers. The landowner has the opportunity to potentially amend the 404 requirements relating to the parcels subject to City approval and in accordance with the terms of the Plan Area Development Agreements. The remaining wetland parcels will be monitored by the City. The long term maintenance of all wetland parcels will be the responsibility of the City. A Landscape Maintenance District will be established to fund City monitoring and maintenance responsibilities.

The maintenance of quality natural areas within the Specific Plan Area will require supplemental action by the City to ensure protection of the sites. Measures should include restricted use of these areas for passive recreation activities which are compatible with the natural communities. Motorcycles, hunting, dumping, or other activities which could be detrimental to the ecosystem shall be prohibited. It may be necessary to mow fire breaks for the safety of adjacent uses in and around the vernal pool and wetland areas.

Erosion control measures will be utilized to reduce water quality impacts and protect aquatic habitats. Specific measures for erosion control (Subsection 6.3.1, Soil Protection Policies) include restricting grading, development of sediment traps, and prompt replanting of disturbed areas.

A setback buffer will be established along both sides of Antelope Creek to protect fish and/ or wildlife values. The extent of buffer required shall be determined at the design review stage of adjacent projects.

### 6.1.3 Wetlands Enhancement and Compensation

In addition to preservation, a program is proposed to enhance preserved wetlands and establish new wetlands within the designated preserve areas. This effort focuses primarily on the enhancement and creation of vernal pools. The purpose of the enhancement and compensation program is to achieve a no-net loss policy in the North Central Plan Area. It is proposed that seed material, collected from pools which will be lost, be introduced into newly created man-made pools within the designated preserves. The enhancement/ compensation program will be the responsibility of the property owner. No wetland areas will be dedicated to the City until the enhancement and compensation required under the 404 Permit is completed.

Vernal pools may be constructed at appropriate sites within the designated preserves. The density of the created and/ or enhanced pools shall be constrained by natural drainage conditions to ensure that no existing pools are adversely affected. Two basic strategies will be used in pool creation. When feasible, water will be impounded in existing depressions by placement of check dams to form outlet barriers. Where check dams cannot be used effectively, depressions will be formed by excavation. The use of check dams is preferred because site disturbance is minimized. Where excavation is required, spoils will be spread to form gentle contours.

Created pools will be monitored by qualified biologists for five years or longer, as determined by the Corps of Engineers and the City, to assure that prescribed criteria for vegetation, wildlife, and hydrology are met. All monitoring and maintenance responsibilities are described under Section 6.1.7 of this element.

### 6.1.4 Wetland Preserve Policies

The following policies are intended to achieve a goal of no net loss in wetland values or acreage. Construction related policies shall be incorporated as conditions of project approval for any private or public project adjacent to a wetland preserve area.

1. The requirements of any permit issued by state and federal agencies will be fully implemented. No net loss of wetland will be accepted. All properties identified on Table 6-1 shall be subject to review by the Corps of Engineers and other responsible federal and state agencies.
2. The Wetland and Lower Watershed Preservation Areas (Parcels 80-88, 90, 91, 93, 95, and 96) designated on Figure 2-4, Land Use Map, shall be dedicated to the City. No dedications shall occur until all enhancement and compensation requirements of the 404 Permit have been completed by the landowner. In addition, Parcels 93, 95, and 96 will not be dedicated until the required 404 monitoring requirements have been completed to the satisfaction of the Corps of Engineers and the City of Roseville. The landowners of Parcels 93, 95 and 96 are granted the opportunity to potentially amend the 404 requirements relating to these parcels and retain them in private ownership subject to City approval and in accordance with the provisions of the effective Development Agreements.
3. The boundaries of Parcels 95 and 96 may be altered to reduce wetlands disruption in accordance with mitigation measure 3.4-26 of the Draft EIR. In the event that such boundary

alteration is not feasible or does not adequately reduce impacts, off-site mitigation or a combination of off-site and on-site mitigation may be proposed. Concurrent with the submittal of any request for off-site mitigation to the Army Corps of Engineers, (COE) such submittal shall be forwarded to the City for Planning Commission and City Council review. If any significant modifications occur to the proposal during or as a result of the COE review process, the proposal shall be resubmitted for City review. The City may approve or deny any request to locate any of the acreage shown for Parcels 95 and 96 off-site, and the review of such projected modifications shall be made in accordance with the requirements of CEQA.

4. All enhancement/ compensation programs shall be the responsibility of the property owners. Where excavation is utilized to create or enhance vernal pools, spoils shall be seeded with native species and spread to form gentle contours.
5. Implement both a short-term and long-term monitoring program to ensure the success of the required 404 and EIR mitigation measures. The landowner will be responsible for required 404 monitoring on Parcels-93, 95, and 96. The City will be responsible for monitoring on the remaining wetland parcels. Long-term maintenance shall be the responsibility of the City. A Lighting and Landscape Maintenance District will be established to fund all City monitoring and maintenance responsibilities.
6. The wetland preserve areas shall be limited to passive recreation activities compatible with the natural communities. Motorcycles, hunting, dumping or other activities, which could be detrimental to the ecosystems, shall be prohibited.
7. A chainlink fence, or acceptable alternative, shall be installed along the boundary of any wetland preserve prior to construction, including roadways, grading, the movement of material or machinery onto the site, approval of improvement plans, or the issuance of any permits for adjacent property. The fencing shall not be removed until the completion of construction activity. Written release from the Planning Department must be received prior to the removal of any fencing.
8. A minimum 12" X 12" sign shall be erected along every fifty feet (50') of fencing or portion thereof. The sign shall indicate that the area is a wetland preserve and that trespassing is prohibited.
9. A minimum \$10,000 bond or other security deemed appropriate by the Planning Commission and in a form approved by the City Attorney shall be posted to ensure the preservation of adjacent wetlands during construction. Each occurrence of violation of any condition regarding wetland preservation shall result in forfeiture of the security.
10. Projects adjacent to wetland areas shall be designed with consideration of the recommendations of a consulting plant ecologist. Drainage constraints and proposed detention basins are shown on Figure 7-13, Proposed Detention Basin Sites. In general, the following standards apply in the design of projects adjacent to wetland preserve areas:
  - Sprinkler systems must be designed so that no direct irrigation reaches any portion of the preserve. Grass-lined swales must be constructed at the margins of all turfed and irrigated areas that slope toward the preserves to intercept irrigation water and surface water run-off and prevent flow into the wetlands.
  - No concentrated run-off may be released from artificial swale networks into any natural swale that carries water to wetlands unless it has been treated or filtered to remove herbicides, fertilizers, and excess nutrients.

- No mowing should occur in the preserve or in any adjacent areas, except where management is necessary for fire protection.
  - A low or open fence, or other barrier that is visually appealing and compatible with the surrounding features should be constructed around park preserve and mitigation areas. The fence and pedestrian entry points should be designed to prevent access by dirt bikes and other motorized vehicles.
  - Each preserve area should be signed to state its purpose and to explain the unique values of the wetland resource.
  - Passive recreation may be allowed in the preserves. Hiking trails shall be designed to avoid alteration of pool watershed.
  - Photo blinds and observation platforms may be allowed in preserve areas.
11. Any proposed detention/ retention facilities located within or adjacent to wetland preserve areas shall be in compliance with the Corps of Engineers 404 Permit requirements and in accordance with the requirements of CEQA.
  12. Buffers will be created along the banks of Antelope Creek in accordance with DFG recommendations for the protection of watercourse wildlife values. In some locations, this may include areas outside of the designated 100 year floodplain and not slated for protection within designated lower watershed preserve areas. Such buffers will be identified through the design review process for projects adjacent to the creek.
  13. Consider off-site "mitigation banking" as an alternative to on-site mitigation measures to be used in the event that anticipated success rates as deferred by the 404 Permit and City for wetlands mitigation are not met. Any off-site mitigation banking shall be in compliance with the Corps of Engineers 404 Permit requirements and shall be approved by the City in accordance with CEQA.

## 6.2 Oak Woodlands

Oak woodlands within the North Central Roseville Specific Plan Area are found in the open grasslands at the southern perimeter of the Plan Area and adjacent to Antelope Creek. Oak trees include scattered Valley oak (*Quercus lobata*), interior live oak (*Quercus wislizenii*), and blue oak (*Quercus douglasii*). Blue oak and riparian species are much more dense along the Antelope Creek drainage and adjacent minor tributary swales. The trees within the slope area west of Antelope Creek are protected in a non-development easement.

This Specific Plan recognizes the importance of native oak trees in providing habitat for numerous small animals and birds, and as a significant visual asset to the community. The Plan establishes detailed policies for the conservation and regulation of future development in areas that include or are adjacent to the oak woodlands.

### 6.2.1 Oak Woodlands Policies

The City of Roseville Tree Ordinance applies to the North Central Roseville Specific Plan. In addition the following standards shall apply. Where inconsistencies exist between the two, the more restrictive will take precedence as determined by the City.

1. It is the intent of the North Central Specific Plan to ensure that all project design, grading, construction and landscaping shall give the highest priority to the preservation of native oak trees. The location and preservation of native trees shall be a primary factor in site design.

2. The cutting or removal of oak trees prior to approval of any development plan is prohibited. The sole exceptions shall be for City-approved roadways, sewer and utility extensions, detention facilities, removal suggested by an arborist for public health and safety, or for other reasons as shall be determined by the City in accordance with the adopted tree ordinance. A certified arborist's report which maps the location and condition of all trees on the site is required for all projects with oak trees. The arborist's report is to be submitted concurrently with the development plan submittal to the City. All trees will be mapped. Those proposed to be removed shall be identified and their removal shall be approved by action of the Project Review Commission, Planning Commission or the City Council, or as otherwise required by City ordinance or regulation.
3. The arborist's report and mapping shall serve as the basis for preparation of a plan to protect trees. The plan shall contain all recommendations from the required arborist's report as well as the policies expressed in this element. All initial measures recommended by the arborist's report and approved by the City, such as deep root feeding or pruning, shall be completed prior to movement of heavy equipment, or commencement of construction activity on the property. The survey and analysis shall occur at the earliest stage of project design. Once project design is complete, a follow-up analysis of the potential project impacts shall be prepared, with mitigation measures recommended and incorporated.
4. Submittal of a bond or other security, in an amount approved by the reviewing City body and form approved by the City Attorney, shall be required for all development projects to ensure compliance with preservation conditions and standards. The amount of bond or security shall relate to the value and number of trees on the site. Violation of the conditions of the approved permit or the policies of this section shall result in forfeiture of the bond or security.
5. Chain link fencing, or an acceptable alternative, shall be installed one foot (1 ') outside the driplines of trees identified to be preserved prior to project construction, grading, the movement of materials or machinery onto the site, the approval of improvement plans or the issuance of any permits, to avoid damage to the trees and their root systems. Fencing shall not be removed until the completion of construction activity. Written release from the Planning Department must be received prior to the removal of any fencing. During the period of road construction all trees within the road right-of way or abutting the road right-of-way which are threatened by construction or related activities and identified by the reviewing City body to be preserved, shall be fenced in accordance with the above requirement. Prior to the commencement of any road construction, the developer must receive written release from the Planning Department that all trees identified to be preserved are properly protected.
6. Paving within the driplines of trees identified to be protected shall be avoided. When determined to be absolutely unavoidable by the reviewing City body, porous materials requiring no, or minimal, compaction shall be used along with aeration systems where appropriate.
7. Signs, ropes, cables, and other items shall not be attached to trees identified to be preserved except as required by a certified arborist.
8. No vehicles, construction equipment, mobile offices, supplies, materials, or facilities are allowed to be parked, stockpiled, or located within the driplines of trees identified to be preserved.
9. No artificial irrigation within the driplines of indigenous oak trees is permitted unless recommended by a licensed arborist and approved by the City. Landscape and grading plans shall be designed to prevent irrigation or non-natural run-off from flowing within the dripline of

native oaks. If necessary, swales and drainage systems will be required outside the dripline of native oaks to prevent such run-off. Irrigation of planted oak trees in new landscape areas may be acceptable as determined by a licensed Landscape Architect or certified arborist. Irrigation of relocated native oaks shall be limited in accordance with the recommendations of a certified arborist. Such trees shall generally not be located in turf areas.

10. If vegetation is approved for planting within the driplines of indigenous oak trees, it must consist of species that require no artificial irrigation and are tolerant of the natural semi-arid environs of the trees, as recommended by a licensed arborist. Landscaping beneath indigenous oak trees will normally include non-plant materials such as boulders, cobbles, bark, and similar materials.
11. Native trees six inches (6") in diameter or greater at forty-eight inches (48") above grade, which are approved for removal, shall be replaced on an inch-for-inch basis with like kind. Replacement shall be by use of a tree size determined appropriate by the reviewing City body in consideration of the soil conditions, the availability of water and the ability of trees to survive transplanting. Replacement locations may include City parks, along the bikeway/pedestrian paths adjacent to open space corridors, along the Antelope Creek corridor and adjacent slope easement, in Diamond Oaks Golf Course, and in the design of specific projects. Relocation of existing trees may be considered on a case-by-case basis.
12. Soil disruption within the dripline of native trees shall be avoided. In those cases where it is determined by the reviewing City body that disruption is absolutely unavoidable, the following guidelines, along with the recommendations of a certified arborist shall apply:
  - Soil surface removal shall not occur within the driplines of trees identified to be preserved.
  - Earthen fill shall not be placed within the driplines of trees identified to be preserved, unless approved with mitigation recommended by a certified arborist. Such mitigation will likely include the implementation of an aeration system.
  - If cuts or fills are made near trees identified to be preserved beyond their dripline, adequate drainage, supplemental irrigation, and/ or aeration may be required to mitigate the adverse effects caused by elevation changes.
  - Generally no trenching is allowed within the driplines of trees identified to be preserved. An exception will occur in limited instances where the off-site sewer line requires trenching along the south branch of Pleasant Grove Creek. Where absolutely necessary to install underground utilities within the dripline of trees, a single trench for all utilities shall be either bored or drilled under the primary root zone, but not within six feet (6') of a tree trunk. In such cases, light construction equipment shall be used. After trenching within the dripline, the tree may need to be pruned to remove canopy material proportional to the roots damaged or lost, as recommended by a certified arborist.
  - Where soil compaction is approved within the dripline of a tree identified to be preserved, measures as recommended by a certified arborist shall be taken to restore soil condition and integrity.
13. Once construction is completed, and the bond or other security released, no tree identified for preservation in approved plans may be removed or significantly altered without approval by the Roseville Planning Department.

14. Tree conservation and site development policies set forth herein shall be incorporated into Covenants, Conditions and Restrictions (CC&Rs) for all projects with native oaks within the Plan Area to ensure that subsequent property owners are aware of their obligation to preserve natural site features.
15. Development of wooded parcels designated for business-professional and commercial land use (Parcels 33, 36, and 45) shall be designed to preserve the oak trees through careful siting of buildings and parking areas. All projects shall be designed so as to avoid:
  - compaction of the tree root zone,
  - discharge of roof run-off to the root zone,
  - placing parking or walkways across the root zone
  - heat damage or scorching of trees from highly reflective building materials or pavingSuch preservation measures may limit the allowable building coverage to less than the nominal square footage otherwise allowed in the Zoning Ordinance, or normally anticipated given parcel acreage.
16. Development of residential land use on Parcels U, V, W, Y, 10, 12, 13, 17, 22, 23 and 46 (Land Use Map, Figure 2-4) shall require special attention to lot patterns and sizes, building locations, grading, utility locations, landscaping, and irrigation due to the woodland cover. The nominal dwelling unit allocation on each parcel may be reduced as a result of the application of oak woodland policies at the time a tentative map or use permit application is approved. A building envelope shall be designated and approved for each lot with tree cover on the tentative map indicating the boundary of any structures to be allowed on the lot.
17. Parcel 97 is designated as a non-developable slope easement in private ownership. This area is to be protected and maintained in its natural condition as an oak woodland ecosystem in accordance with section 3.12 of the Community Form and Design Element.
18. The construction area for roadways and other improvements within the Antelope Creek drainage and adjacent oak woodland shall be delineated and fenced off. The size of the construction area will be kept to the minimum necessary to efficiently accommodate construction. Construction equipment operation and materials storage will be restricted to designated construction areas, as determined by the City, in order to limit unnecessary habitat and wildlife disturbances in the woodland areas . In addition the following policies shall apply:
  - a. Minimize the number of trees to be removed;
  - b. Promptly revegetate cleared areas with native species;
  - c. Minimize the number of paths and trails. Such trails shall be located to avoid snags and trees favored by raptors;
  - d. Locate creek crossings to minimize intrusion into riparian and woodland areas;
  - e. Require input from biologists when designing any improvements intended to occur or be developed within or adjacent to riparian or woodland habitat areas.
19. A spring raptor survey will be conducted by a qualified biologist prior to any tree removal. If an active raptor nest is located in any tree slated for removal, that removal will be cancelled or postponed until the young have "fledged" and the nest is determined, by the biologist, to be inactive.

### 6.3 Soil Protection and Grading

Erosion potential within the Plan Area is slight due to the Mehrten formations. Exposure of the soils, especially the alluvial soils, could result in erosion if not carefully managed during the construction

period. Consequently, the North Central Roseville Specific Plan establishes policies for the maintenance of soil stability in the area.

### 6.3.1 Soil Protection Policies

1. All development plans submitted for City review and approval shall provide an erosion and sediment control plan. Specific erosion control measures shall be adopted for all development plans to protect area waterways from erosion and debris during construction. Such measures shall include, but not necessarily be limited to, seeding of graded areas, watering during grading activities to reduce wind erosion, and the use of hay bales and filter cloth to prevent siltation of stream courses.
2. Individual projects will be required to develop and implement a dust control plan in accordance with the guidelines established by the Placer County Air Pollution Control District. Grading and similar activities shall not be conducted during windy conditions.
3. Site grading for structures and streets shall preserve natural land forms to the maximum extent possible. Construction techniques including, but not limited to, stepped footings and retaining walls are encouraged as a means of preserving native topography. Split level dwellings and other flexible building designs which work with the natural topography shall be utilized.
4. Slopes shall be rounded and taper to blend with existing topography, contours on adjacent sites, and roadways. Steep slopes and large retaining walls should be avoided when feasible.
5. Grading shall minimize disruption to existing natural features such as trees and other vegetation, natural ground forms, water courses, and views.
6. Soil exposed during grading which will be left exposed and are not under active construction during the rainy season, assumed to occur between October 15 and March 15, shall be promptly replanted with native compatible, drought resistant vegetation.
7. Prior to the development of any neighborhood identified in Section 3, the Community Form and Design Element, a master conceptual grading plan shall be approved which identifies the overall grading concept, the location and preservation of natural features, the resulting drainage pattern, and the balance of soil materials within the neighborhood.
8. Drainage problems resulting from poor soil permeability, shall be reduced through development of gravel subdrains and the creation of swales and channels to convey run-off.
9. Structural limitations posed by the high shrink-swell potential and limited load-bearing strength of Cometa Fiddyment Complex and Cometa Ramona Sandy Loam soils shall be compensated for through the following measures or a combination thereof:
  - development of reinforced foundations,
  - use of concrete piers,
  - removal of soft, compressible surface soils to expose subgrade,
  - followed by importation of base material,
  - development of subdrains.
10. Limitations on landscaping created by shallow soils, limited water-bearing capability, and/or impermeable underlying materials shall be reduced through the following measures or a combination thereof:
  - overexcavation or drilling of areas to be landscaped followed by the importation of topsoil,
  - use of drought-tolerant or shallow-rooted landscaping,
  - use of efficient irrigation systems,

- development of uses which allow for common landscaped areas with guaranteed maintenance.

## 6.4 Water Quality

Development within the North Central Plan Area will increase the impervious surfaces where soil permeability is naturally very slow. The rate of storm run-off will increase as a result of the increase in impervious surface area, however, the storm drainage will be conveyed to the natural stream channels by a master storm drainage system. The following policies are intended to minimize the impacts of development on water quality.

### 6.4.1 Water Quality Policies

1. All development plans submitted for City review and approval shall provide an erosion and sediment control plan as specified in Section 6.3.1 of this element.
2. The design of paved parking areas should be designed to provide the minimum amount of paving area necessary to meet required parking standards. Permeable paving materials should be considered where feasible.
3. Best Management Practices (BMP) shall be incorporated into the design of drainage systems for individual projects within the Plan Area.
4. Sediment traps, evaporation basins, flow reduction devices, and other methods to reduce the volume of pollutants in parking area and street surface run-off shall be installed in the storm drain system in accordance with City standards. Best Management Policies (BMP) shall be utilized where feasible, as determined by the City.
5. The City of Roseville should initiate programs to familiarize residents with environmental concerns which directly affect the standard of living in Roseville. Such programs should address such issues as contamination of the wetlands through improper use and disposal of household products.
6. The City of Roseville should sponsor a program which routinely offers special locations for the pick up of hazardous wastes such as automotive oil, old pesticide and herbicide containers, paints and thinners, or other wastes which require special consideration for disposal.
7. Construction of stream crossings or other improvements in Antelope Creek or lower watershed areas shall be kept to the absolute minimum necessary to provide reasonable access to developed areas west and east of Antelope Creek or lower watershed. Department of Fish and Game Stream Alteration Agreements will be obtained prior to commencement of any such construction of any such proposed stream crossings in the North Central Roseville Specific Plan Area. All conditions of compliance set forth in these agreements will be strictly enforced as part of the mitigation monitoring program. In addition the following policies shall apply:
  - a. All grading, site preparation, and construction work in or near Antelope Creek shall be confined to the period of June 1 through September 30 in order to minimize erosion and impacts on the October-November spawning run and April-May out migration of chinook salmon.
  - b. Construction access roads across creek or watershed areas shall be kept to the absolute minimum necessary to accommodate construction activity. No heavy construction equipment shall be allowed in a live stream.

- c. Cofferdams, where approved, shall be properly constructed (generally of geotextile material, sandbanks, and culverts) so as to provide an adequate "dewatered" area for construction, while minimizing impacts to the environment.
- d. Construction crossings of Antelope Creek (located only in the dewatered area) and lower watershed areas shall employ proper construction methods including an initial layer (approximately 18 inches) of clean gravel so as to allow for the clean removal of the crossing at the conclusion of construction.
- e. Prior to the start of construction in Antelope Creek or lower watershed areas, or coffer dam construction, photographs of streambed and bank contours shall be taken. Following construction, streambed and bank contours will be restored, as near as possible, to pre-project conditions.
- f. Materials and water from excavation activities within the dewatered stream area shall be pumped into settling areas and shall not be discharged directly into live streams. Any water passing through a coffer dam shall be released back into the stream unless it has a high sedimentation load, in which case it shall be pumped to settling areas.
- g. Settling ponds shall be cleaned when they have reached fifty percent (50%) of their sediment storage capacity. All sediment shall be disposed of behind a protective berm well above the average winter flows of the stream.
- h. Areas adjacent to the finished improvements in Antelope Creek or lower watershed areas which were disturbed during construction will be hydroseeded and revegetated to reduce erosion potential. All disturbed areas that are not actively being developed shall be planted, mulched or otherwise protected by an acceptable means for the duration of the winter. In no case shall disturbed areas be left exposed between October 15 and March 15.
- i. While grading prior to construction, topsoil shall be set aside for later use in revegetation and recontouring efforts along Antelope Creek or lower watershed areas.
- j. The City and the project engineer shall strategically lay out the construction programs and schedules so as to minimize the disturbed areas that must remain through the winter season. (October 15 through March 15).  
 Prewintering (on-site) meetings shall be scheduled in early September to ensure that all contractors and parties understand their responsibilities concerning preparing sites for the winter season. Any new potential erosion problems will be identified at that time.  
 As a follow-up to the prewintering meetings, a specific schedule of inspection and maintenance of construction sites shall be identified by the City to ensure erosion control measures are operative through the winter period. A copy of this schedule shall be forwarded to the Department of Fish and Game for the file on the project.
- k. Construction roads shall have water bars and culverts if they are required to remain through the winter season.
- l. Design all stream crossings to permit movement of wildlife beneath them.
- m. Design all stream crossings for 100-year event.
- n. Design stream crossings such that approaches are as close to a right angle as possible, and utilize bridges when feasible.

- o. Any stream work and construction activity within the identified wetland areas may require additional measures or restrictions as determined by Department of Fish and Game and/ or the City of Roseville.
- 8. Any public works projects in the floodway, lower watershed, riparian, or woodland areas shall include a preservation and restoration plan to be approved by the Planning Commission and City Council.
- 9. Rock energy dissipators or other methods shall be used at the outflow points of any culverts.
- 10. The Public Works Department shall approve all drainage facility designs prior to the approval of individual projects.
- 11. All areas within the 100-year floodway and lower watershed areas shall be dedicated to the City. The City will specifically prohibit the construction of habitable structures, any fill or any structures intended to dam the flow of water within the 100-year floodway.
- 12. A water quality monitoring program should be established by the City to assess the long term effectiveness of proposed pollution control measures.

## 6.5 Air Quality

Air quality in the Sacramento Valley is recognized as a significant environmental concern which influences the quality of life for all residents. Vehicle trips are a major factor in the deterioration of air quality, and new development can influence the degree to which air quality impacts the region. The location of new residential and employment areas, and the spatial layout of communities can play a role in the effectiveness of efforts to maintain air quality over a period of many years.

The North Central Roseville Specific Plan describes a land use pattern which provides housing to support nearby employment opportunities and public services. This land use pattern will help to minimize automobile traffic and exhaust emissions within the Plan Area associated with daily routines. Retail goods and business-professional services are provided for the convenience of residents and employees to limit the length and number of trips, thereby reducing the average daily vehicle miles traveled (VMT) throughout the region. In addition, the Plan provides for alternative transportation modes, including public transit, pedestrian and bikeways, and Transportation Systems Management (TSM) which will also serve to reduce automobile trips. Light rail may be extended to the Plan Area in the future.

The City of Roseville is a participant in the 1989/1990 update of the Regional Air Quality Plan. City participation includes a funding contribution to the Phase 1 Study; representation on the Technical Advisory Committee; and the contribution of in-kind staff services. Upon adoption of the updated Regional Air Quality Plan, the City will amend its current Air Quality Plan.

### 6.5.1 Air Quality Policies

The following Air Quality policies apply to the North Central Roseville Specific Plan:

1. The North Central Roseville Specific Plan shall be amended upon the completion and adoption of the revised City of Roseville Air Quality Plan, and shall be in conformance with the updated Sacramento Air Quality Plan as implemented by the City Plan.
2. The measures established by the City Rideshare Ordinance and any updates or revisions thereto shall be fully implemented and enforced in the Plan Area.
3. Park and ride lots shall be strategically located in the Plan Area as specified in the Circulation Element (Section 5) of this Plan to facilitate ease of use.

4. The use of public transportation systems, including the potential extension of light rail, shall be an integral consideration in the design review for all projects along the light rail routes. Transit services shall be extended into the Plan Area in accordance with the policies contained in the Circulation Element (Section 5) of this Plan.
5. Pedestrian access connections to the Plan Area pathway systems shall be made from adjacent projects to facilitate pedestrian/bicycle travel in accordance with the policies of the Community Form and Design Element (Section 3).
6. Short term air quality impacts associated with construction activities shall be reduced through implementation of the following measures:
  - Spray water on exposed earth surfaces during clearing, grading, earth moving, and other site preparation activities.
  - Require construction contracts to include watering at a minimum in the late morning and at the end of the day.
  - Use tarpaulins or other effective covers for haul trucks which travel on public streets.
  - Institute measures to reduce wind erosion when site preparation is completed.

## 6.6 Historical and Cultural Resources

With approximately eighty-five percent (85%) of the North Central Plan Area previously subjected to archaeological reconnaissance, coverage of all potentially sensitive locations is considered to be complete. Those sites not covered are dry, inhospitable, and are not likely to contain archaeological features. The cultural reconnaissance survey identified seven prehistoric food processing activity sites considered archaeologically valuable, and recommends that such sites be incorporated into the land use planning and landscape design of the North Central Roseville Specific Plan. Six of these sites are located within lower watershed preserve areas and will be left undisturbed.

The archeological survey also identified historic rock walls built by Chinese laborers on the Spring Valley Ranch. The rock walls were not identified in the archaeological study as a significant resource. Portions of the walls will, however, be preserved on Parcels 84 and 93 as reflected on Figure 6-2, Rock Walls in Preserve Areas. The exact location of archaeological sites in the North Central Plan Area are identified in studies on file with the Roseville Planning Department. The following policies shall apply to the preservation and treatment of historical and cultural resources in the North Central Plan Area.

### 6.6.1 Historical and Cultural Resource Policies

1. The rock walls in Parcels 84 and 93 will be retained in an undisturbed state as illustrated in Figure 6-2, Rock Walls in Preserve Areas.
2. All archaeological sites located in wetland preserves, park preserves, parks and other open space areas shall be left in an undisturbed state.
3. Prehistoric sites outside of preserve areas, designated as significant by the EIR, will be protected through capping or excavation as determined by the City.
4. In the event any previously unidentified historic surface or subsurface archaeological features or deposits are uncovered during construction, work in that immediate vicinity will halt immediately and a qualified archaeologist will be contacted for determination of resource significance. In addition, the State Office of Historic Preservation shall be notified.
5. Uncovered artifacts that can be relocated shall be donated to the City for inclusion in the Maidu Park Native American Center as approved by the City and a tribal representative.

6. Rock walls that are not designated for preservation may be collected by the City and reused at the City's expense. From the date of issuance of the grading permit or the use permit, whichever comes earlier, the City will have five (5) days to advise the property owner of the intent to remove rocks.

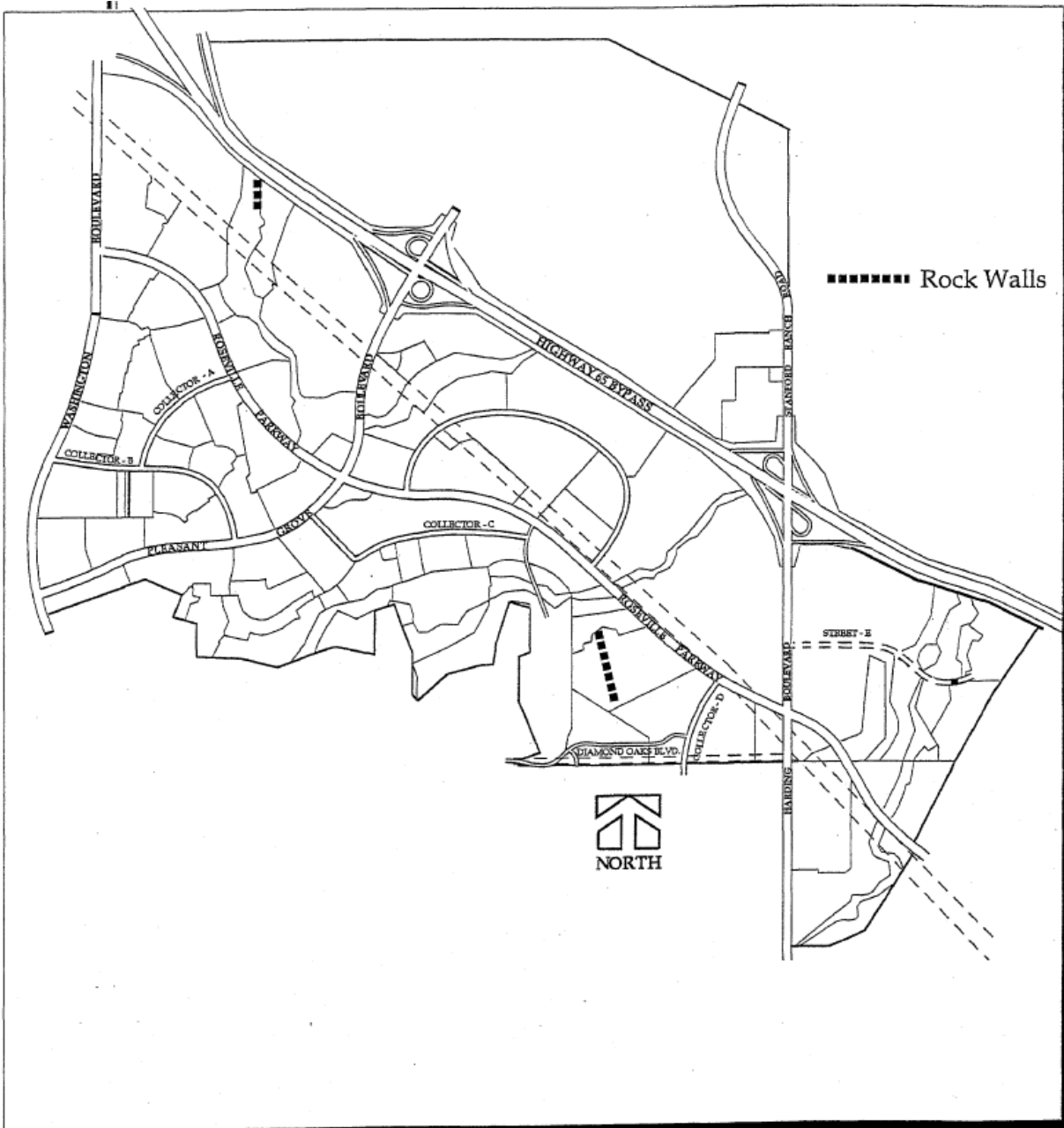


Figure 6-2: Rock Walls in Preserve Areas



## 7. Public Facilities and Service Element

The North Central Roseville Specific Plan includes a variety of public facilities and services intended to support and serve the needs of Plan Area residents. The services include: schools, parks, libraries, and fire protection. Major infrastructure facilities include: electric utilities, water, sewer, and storm drainage systems. The locations of schools, parks, the fire station, and library are shown in Figure 7-1, Public Facilities and Services Plan.

The Plan Area street system is described separately in the Circulation Element, Section 5. A brief discussion of the proposed methods for financing and phasing the construction of these improvements is contained in Section 8, the Implementation Plan.

### 7.1 Fire Protection

Fire protection is provided throughout the City by the Roseville Fire Department. Each of the three existing, fully-manned fire stations provide first response protection to a designated sub-area of the City. As further growth occurs, additional stations will be built to provide overlapping response areas in the City's fire protection master plan.

A fire station is planned within the North Central Specific Plan Area on the northeast side of Pleasant Grove Boulevard as illustrated in Figure 7-1. A median break and traffic signal will be provided on Pleasant Grove Boulevard at this location. Station #2, on Junction Boulevard, will provide initial response into the Plan Area until the planned station is built. Primary backup for the planned station will come from Station # 1 on Oak Street.

### 7.2 Police Protection

The North Central Roseville Specific Plan Area will be served in total by the Roseville Police Department. The police department provides all operations and patrols out of a central station located on Oak Street. No police substations are anticipated in the Plan Area at this time, although a small substation site may be considered within the regional mall site at a future date.

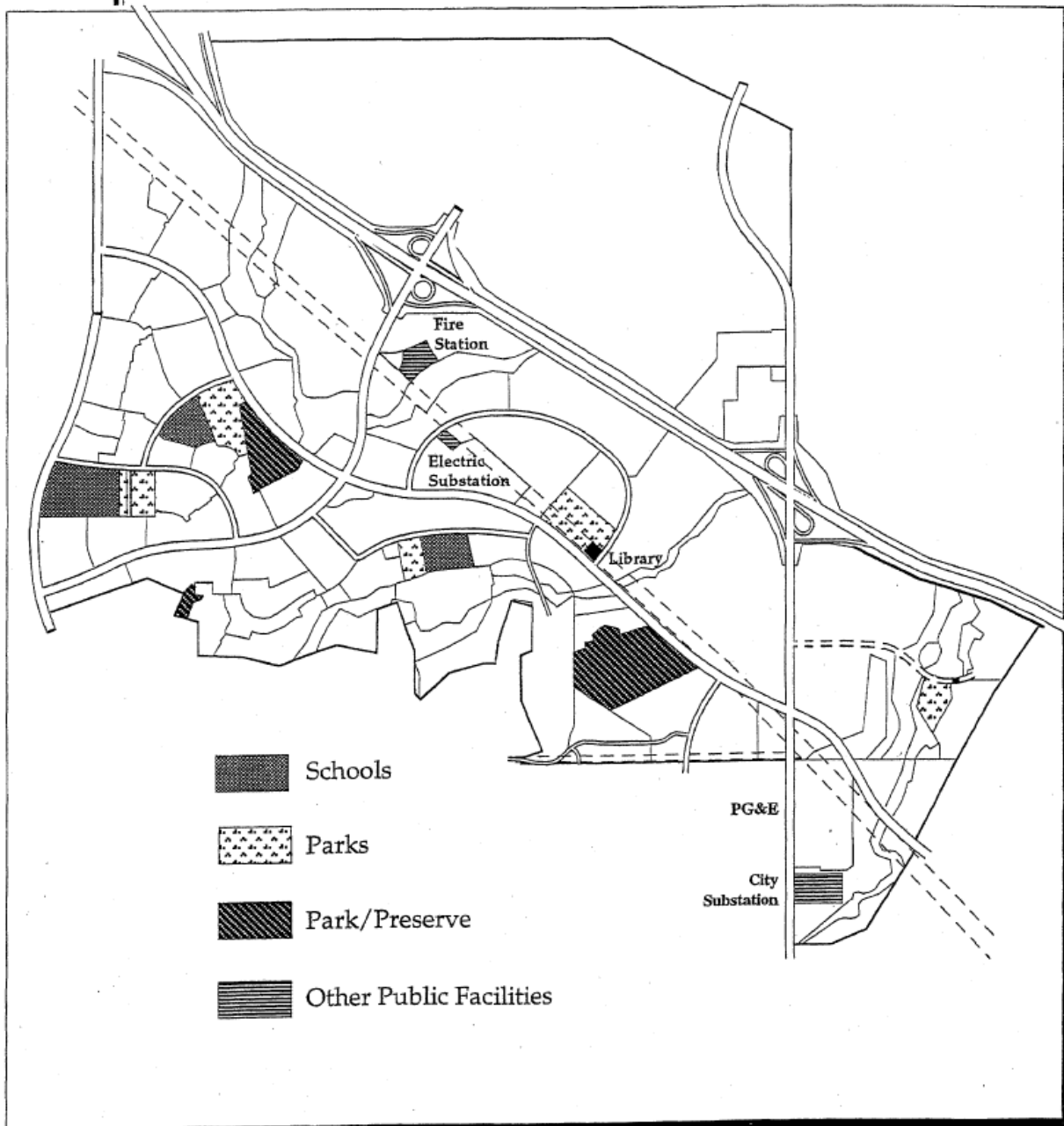


Figure 7-1: Public Facilities and Services Plan

### 7.3 Elementary Schools

A total of 34.42 acres have been designated as K-6 and intermediate school sites within the Plan Area to accommodate the projected elementary school age population. The North Central Roseville Specific Plan falls entirely within the boundaries of the Roseville City School District. The Roseville District will serve the kindergarten through 8th grade population generated from the 4,711 dwelling units allocated to the Plan Area. The District is currently planning for an elementary school along Shasta Street approximately one mile south of the Plan Area. The proposed school, with a total capacity of 600 students, serves the central area of the City, including students for existing residential areas adjacent to the North Central Roseville Specific Plan Area.

**Table 7-1  
K-8 Enrollment Projection at Full Buildout  
Roseville City School District**

Land Use	Units	K-6 Yield Rates	K-6 Students	7-8 Yield Rates	7-8 Students	Total K-8
R-3 to R-5	2,110	0.3993	843	0.1193	252	1,095
R-5+ to R-10	488	0.3993	195	0.1193	58	253
R-14 to R-20	2,113	0.1660	351	0.0400	76	427
<b>Total</b>	<b>4,711</b>		<b>1,389</b>		<b>386</b>	<b>1,775</b>

Buildout of the Plan Area will generate an estimated total of 1,775 students in kindergarten through 8th grade as indicated in Table 7-1. Approximately 1,389 students, or seventy-eight percent (78%) of the total elementary school students will be in grades K-6 with the remaining 386 students (22% of the total elementary school students) in grades 7-8. The assumed capacity of each school is approximately 600 students; therefore, two school sites will be required to serve kindergarten through 6th grade students in the Plan Area.

An 8.0 acre elementary school site (Parcel 72) south of Roseville Parkway will serve the residential neighborhood at the west end of the Plan Area. This site is adjacent to a 9.92 acre neighborhood park (Parcel 52) and 13.84 acre wetland preserve (Parcel 91) to allow the potential for shared recreational facilities and nature studies. A second elementary (K-6) school located in the south central portion of the Plan Area (Parcel 74) will accommodate an additional 600 students. This site is also adjacent to a 4.49 acre neighborhood park (Parcel 53) and a wetland preserve area (Parcel 87).

An intermediate school site of 18.46 acres (Parcel 73) is located at the western edge of the Plan Area adjacent to Washington Boulevard. This site will serve the 386 7th and 8<sup>th</sup> grade students generated from the North Central Roseville Specific Plan Area, as well as the balance of the City west of Interstate 80. The site is adjacent to an 7.97 acre neighborhood park (Parcels 50A, 50B, 50C, and 50D) to allow for shared facility use. The total site allocated for the intermediate school and park site is 26.43 acres. The intermediate school site will be buffered from Washington Boulevard by the required fifty foot (50') landscape corridor and a masonry soundwall.

There is a potential for joint school/park use of recreation facilities at all of the school sites in the Plan Area. In addition to joint use park sites, the District would like to propose other joint use activities, such as a library, gym or multi-purpose room. It is estimated that approximately 4 acres of each 8 acre elementary school site and 10.7 acres of the intermediate school site will be used for sports and recreation. Outside normal school hours these facilities will be available to residents of the Plan Area, on a joint use basis. Thus, the active recreation area in the Plan will be augmented by approximately 18.7 acres.

#### 7.4 High School

The Roseville Joint Union High School District serves the entire North Central Roseville Specific Area. Table 7-2 indicates that full buildout of the Plan Area will generate an estimated total of 704 residents of high school age. The entire Plan Area is within the Roseville High School attendance boundary.

**Table 7-2  
9-12 Student Enrollment Projection at Full Buildout  
Roseville High Scholl District**

Land Use	Units	Yield Rates	High School Students
R-3 to R-5	2,110	0.2362	404
R-5+ to R-10	488	0.2362	217
R-14 to R-20	2,113	0.0445	83
<b>TOTALS</b>	<b>4,711</b>		<b>704</b>

### 7.5 Recreation and Parks

The standard for neighborhood and community park dedication and development in the City of Roseville equates to a total of 9 acres of parkland per 1,000 people. Given the planned housing stock of 4,711 dwelling units, the Plan Area will generate an estimated population of 9,814 residents. As summarized in Table 7-3, Summary of General Plan Park Area Requirements, at 9 acres per 1,000 residents, a total of 88.32 acres of parkland will need to be dedicated in the Plan Area.

**Table 7-3  
Summary of General Plan Park Area Requirements**

Park Type	Service Area	Acres/1000 Population	Total Population	Acres Required
Neighborhood	¼-Mile	2.5	9,814	24.53
Community	1-2 Miles	1.5	9,814	14.72
City-Wide	Total City	5.0	9,814	49.07
		9.0		88.32

Park acreage required by land ownership is summarized in Table 7-4, North Central Roseville Specific Plan Park Acreage Requirements by Ownership.

**Table 7-4  
North Central Roseville Specific Plan Park Requirements by Ownership**

Property Owner	Total Units	Population	Park Acreage Requirements
RPIP	3,830	8,384	75.46
Shaffer Mgmt.	235	586	5.27
Mourier Const.	90	238	2.14
Roseville 150 Center	0	0	0
333 Mavrias	340	598	5.38
PG&E	0	0	0
City/Roseville	3	8	0.07
	4,711	9,814	88.32

2010 census data shows a slight increase in population per household. This table does not reflect the newer census data.

The North Central Roseville Specific Plan specifically designates a total of nine (9) park sites totaling 48.25 acres. This acreage does not include the park/preserve areas that total 48.53 additional acres, nor any recreation potential within the lower watershed area totaling 116.83 acres, or the pedestrian bikeway corridors totaling 5.54 acres. The park preserve areas are designated to preserve significant vernal pool and wetland resources and provide passive recreational opportunities such as walking, jogging, adventure play for children, bird watching and other forms of nature study. The lower watershed area will incorporate a bike trail system that links to the backbone system. These areas, therefore, will provide recreational opportunities for bicycling, walking and jogging. Because of the possible recreational opportunities in the park/preserve and lower watershed areas, these properties are given partial credit in meeting the park acreage requirement. Park preserves are granted a credit of one-half per acre and the lower watershed are granted one-tenth, acre of credit per acre. The total credit allocated to the Plan Area is 85.31 acres. The actual acreage set aside is 218.79 acres. This is significantly higher than the 88.32 acres of parkland required. The future park sites and park preserve areas are schematically illustrated in Figures 7-2 through 7-9.

The park facilities provided are summarized on Table 7-5, North Central Roseville Specific Plan Park and Recreation Allocation.

#### 7.5.1 Neighborhood Parks

The neighborhood parks in the Plan Area are generally intended to provide visual open space, a neighborhood focal point and recreational facilities which lend themselves to small scale or passive activities. Eight such park sites are designated in the Plan measuring 2.55 to 13.33 acres in size. Three park sites are adjacent to planned school facilities. The school sites will be accessible to Plan Area residents during non-school hours and will provide approximately 18.7 acres of additional active recreational area. The joint use sites may include soccer fields, ball diamonds, and multi-use field areas.

In addition to the park sites within the North Central Roseville Specific Plan Area there is a 5 acre site immediately south of Diamond Oaks Boulevard. The site is within an easy walking distance of the homes along the south side of the Plan Area and, while not within the North Central Plan, will be available as a neighborhood park for residents in this portion of the Plan.

**Table 7-5  
North Central Roseville Specific Plan  
Park and Recreation Allocation**

<u>Active Parks</u>	<u>Actual Acres</u>	<u>Allowances</u>	<u>Park Credit</u>
Parcel 50 (A, B, C, & D)	7.97		
Parcel 52	9.93		
Parcel 53	4.49		
Parcel 55B	3.23		
Parcel 55C	2.90		
Parcel 55D	2.55		
Parcel 56	13.33		
Parcel 57	5.66		
	50.06	1:1	48.25
<u>Park/Preserve</u>			
Parcel 90	1.81		
Parcel 91	13.84		
Parcel 93	33.09		
	48.74	0.5:1	24.37
<u>Pedestrian/Bikeway Corridor</u>	<u>Actual Acres</u>	<u>Allowances</u>	<u>Park Credit</u>
Parcels 17A & 17B (portion)	3.36		
Parcel 18C (portion)	2.18		
	5.54	0.2:1	1.11
<u>Lower Watershed</u>			
Parcel 80A	3.66		
Parcel 80B	3.92		
Parcel 81A	3.92		
Parcel 81B	1.59		
Parcel 82	6.11		
Parcel 83	16.27		
Parcel 84	37.62		
Parcel 85	15.45		
Parcel 86	1.98		
Parcel 87	18.08		
Parcel 88	12.36		
	<u>120.96</u>	0.1:1	12.10
<b>TOTAL</b>	<b>225.30</b>		<b>85.31</b>

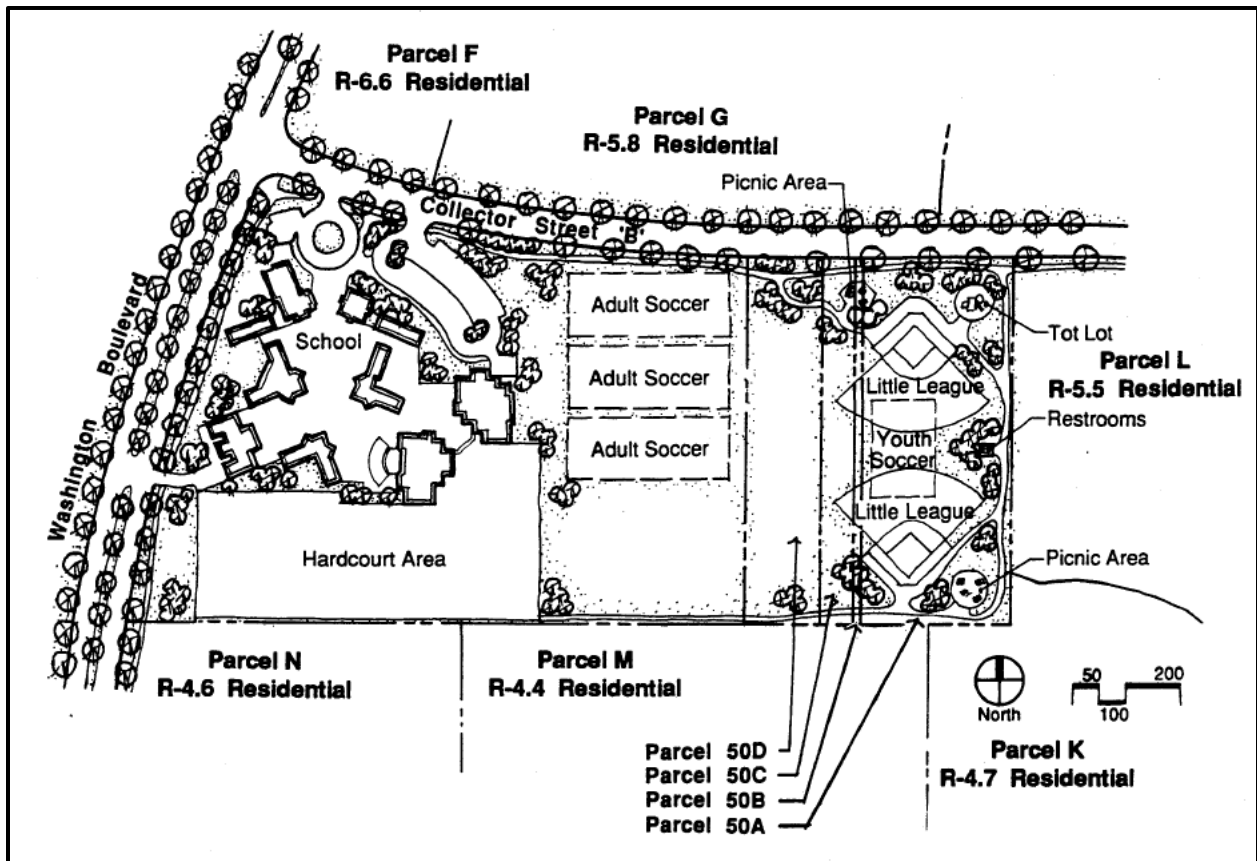


Figure 7-2: Neighborhood Park

*Parcel 50 (A, B, C, D): 7.97 acres*

This neighborhood park is located adjacent to the intermediate school site on the east side of Washington Boulevard, thereby providing the opportunity for joint use. The park is part of the neighborhood center and interfaces with the adjacent residential. The park is linked to the surrounding area it is intended to serve through the Plan Area pathway system. The facilities envisioned for this park will provide active recreational opportunities and may include:

- 2 Little League diamonds
- 1 youth soccer field
- 1 tot lot
- 2 picnic areas
- 1 multi-purpose facility (joint use)

[FIGURE 7-3 AND TEXT DELETED 1/96, (pdf 377)]

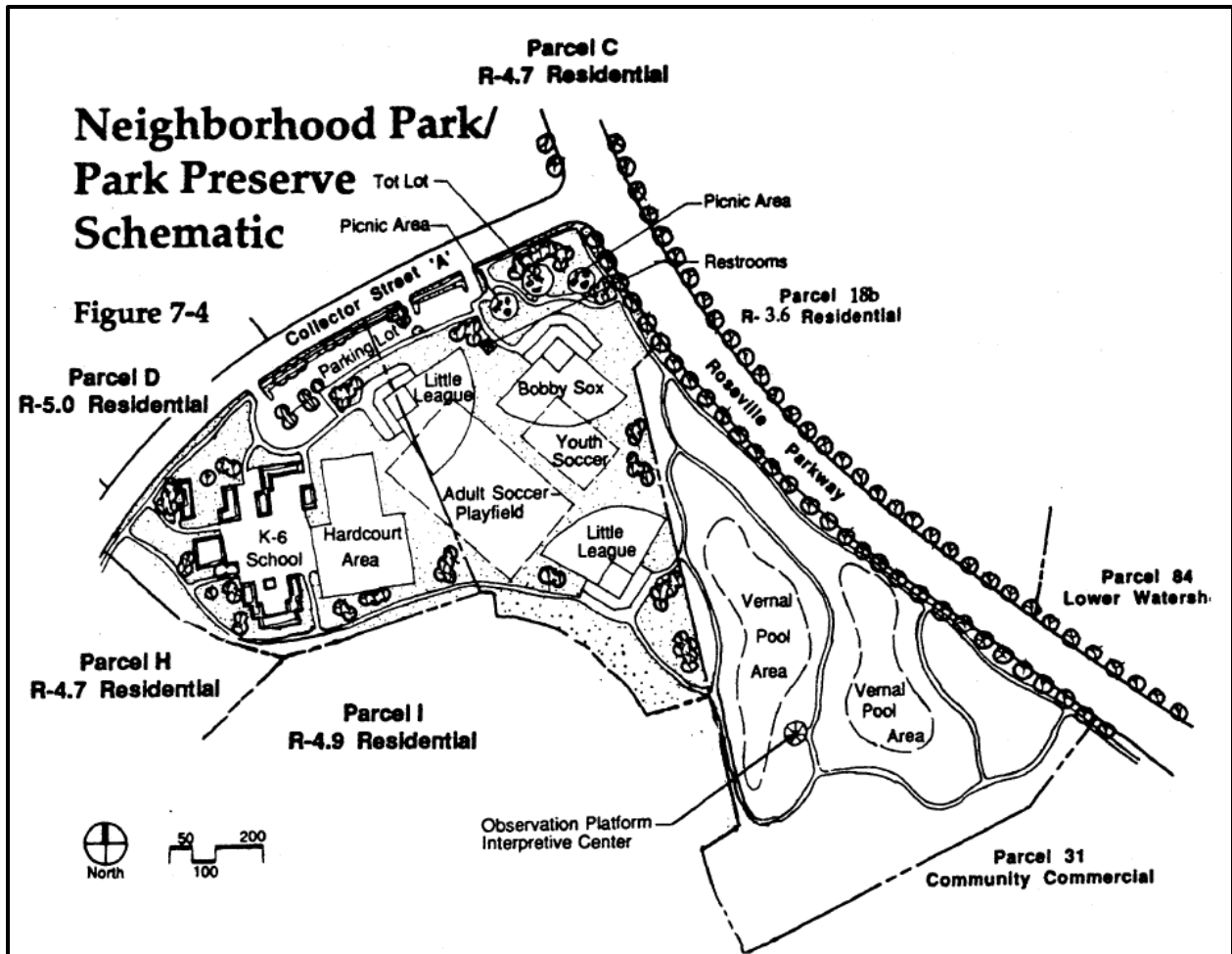


Figure 7-4: Neighborhood Park/Park Preserve Schematic

**Parcel 52: 9.93 Acres**

This neighborhood park and park/ preserve combination are located immediately southwest of the Roseville Parkway and adjacent to an elementary school site, providing opportunities for joint use. The park will interface with the community park across Roseville Parkway and the residences in Neighborhood A. Pedestrian access to the adjacent neighborhoods will be provided by sidewalks along the residential streets and the pathway system through the adjacent park preserve. The park provides facilities for active and passive recreation for both children and adults which may include:

- 2 Little League diamonds
- 1 Bobby Sox diamond
- 1 totlot
- 2 picnic areas
- 1 youth soccer field
- 1 adult soccer field
- 1 multi-purpose playfield
- parking

The park preserve area provides for the preservation of vernal pools and the opportunity for nature study. The pathways in the preserve will provide linkages between Neighborhood A and the adjacent commercial, park, and school sites. The park preserve will include only passive recreation facilities which may consist of cycling, jogging, interpretive trails, and an observation platform.

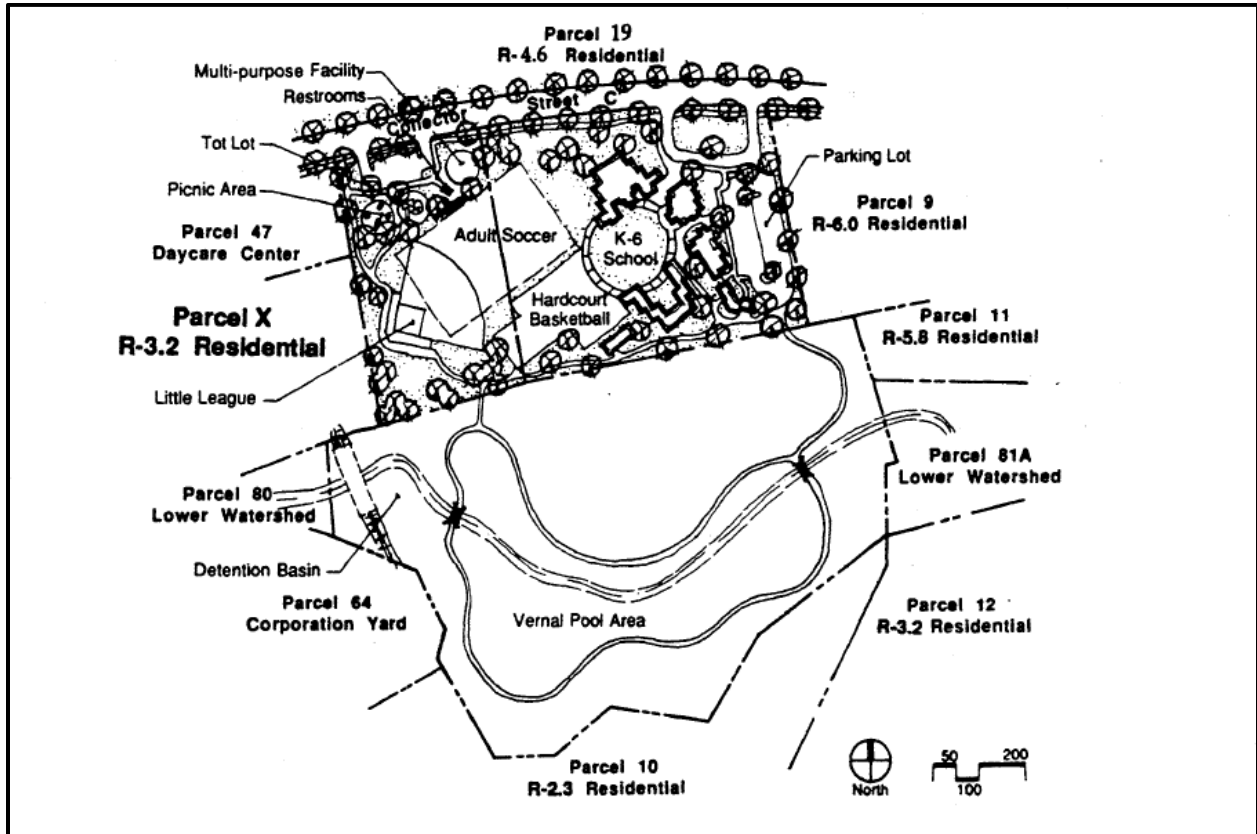


Figure 7-5: Neighborhood Park/Lower Watershed Schematic

This adjoining neighborhood park and lower watershed area are adjacent to an elementary school site south of Roseville Parkway. The park will interface with surrounding uses to create a neighborhood center. The upper portion of the park immediately adjacent to the school provides several facilities which complement joint use and may include:

- 1 Little League diamond
- picnic area
- 1 tot lot
- 1 multi-purpose facility (joint use)
- parking

This park also ties into the lower watershed bike/pedestrian trail system. A pathway will connect from the park to the pedestrian/bike trail system in the lower watershed. The adjacent lower watershed area is undeveloped and will accommodate seasonal flows and vernal pool mitigation, adventure play area, nature study, trail systems, and passive recreation.

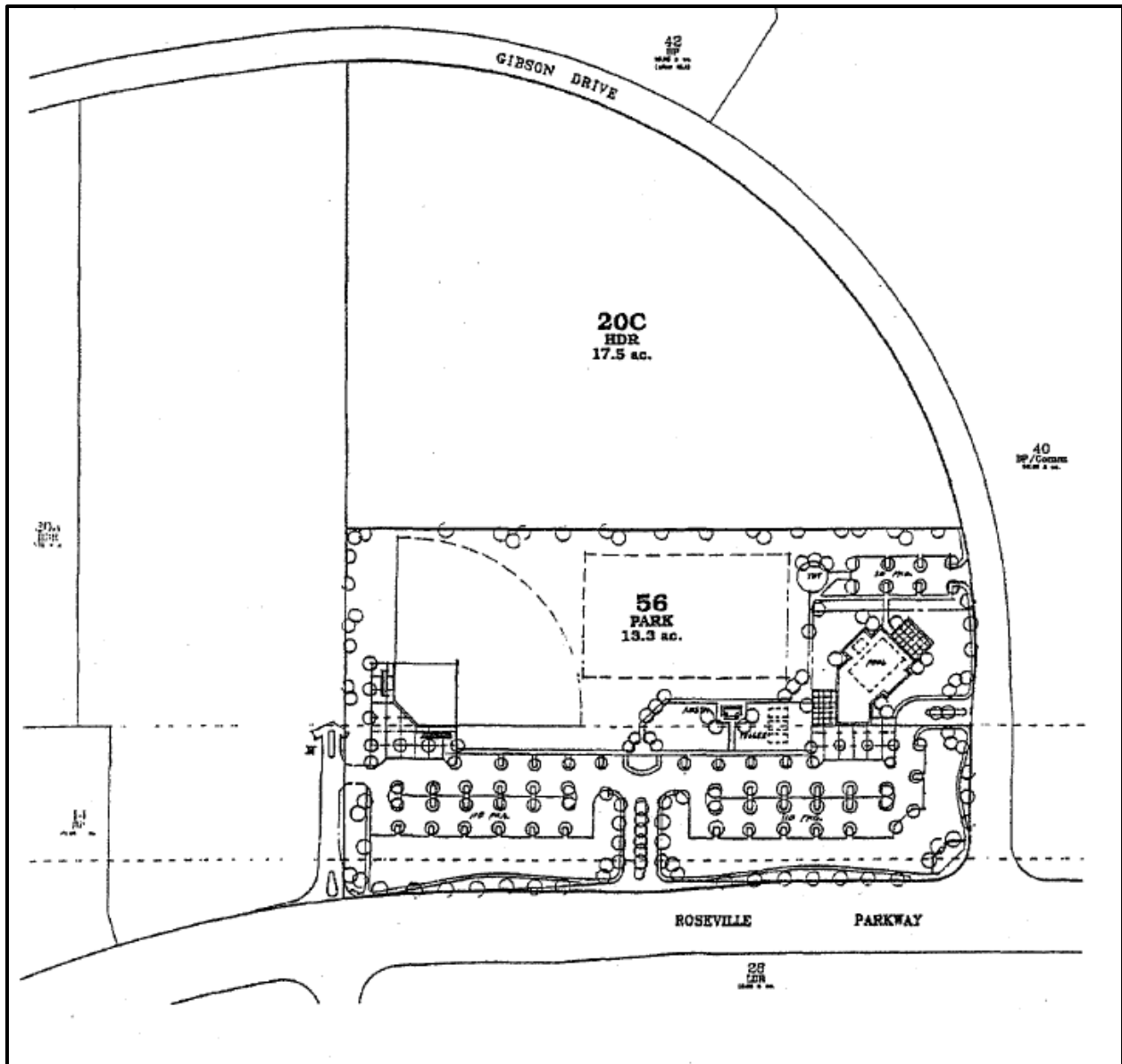


Figure 7-6: Community Park Schematic

This community park is located adjacent to the north side of Roseville Parkway. Due to its central location adjacent to high density residential and intensive nonresidential land use, the park is intended to serve as a focal point of community activity and active recreation. Use of the site may include the following facilities:

- Community Center/public library/gym
- band shell/amphitheater
- 1 Bobby Sox/Little League diamond
- 2 adult soccer fields
- pool building
- swimming pool
- diving pool

- 2 basketball courts
- 2 softball diamonds
- 4 tennis courts
- play area
- picnic areas
- parking (including potential
- shared park and ride lot)
- Parking will be provided within the electrical easement adjacent to Roseville Parkway.

[FIGURE 7-6a AND TEXT DELETED 6/97]

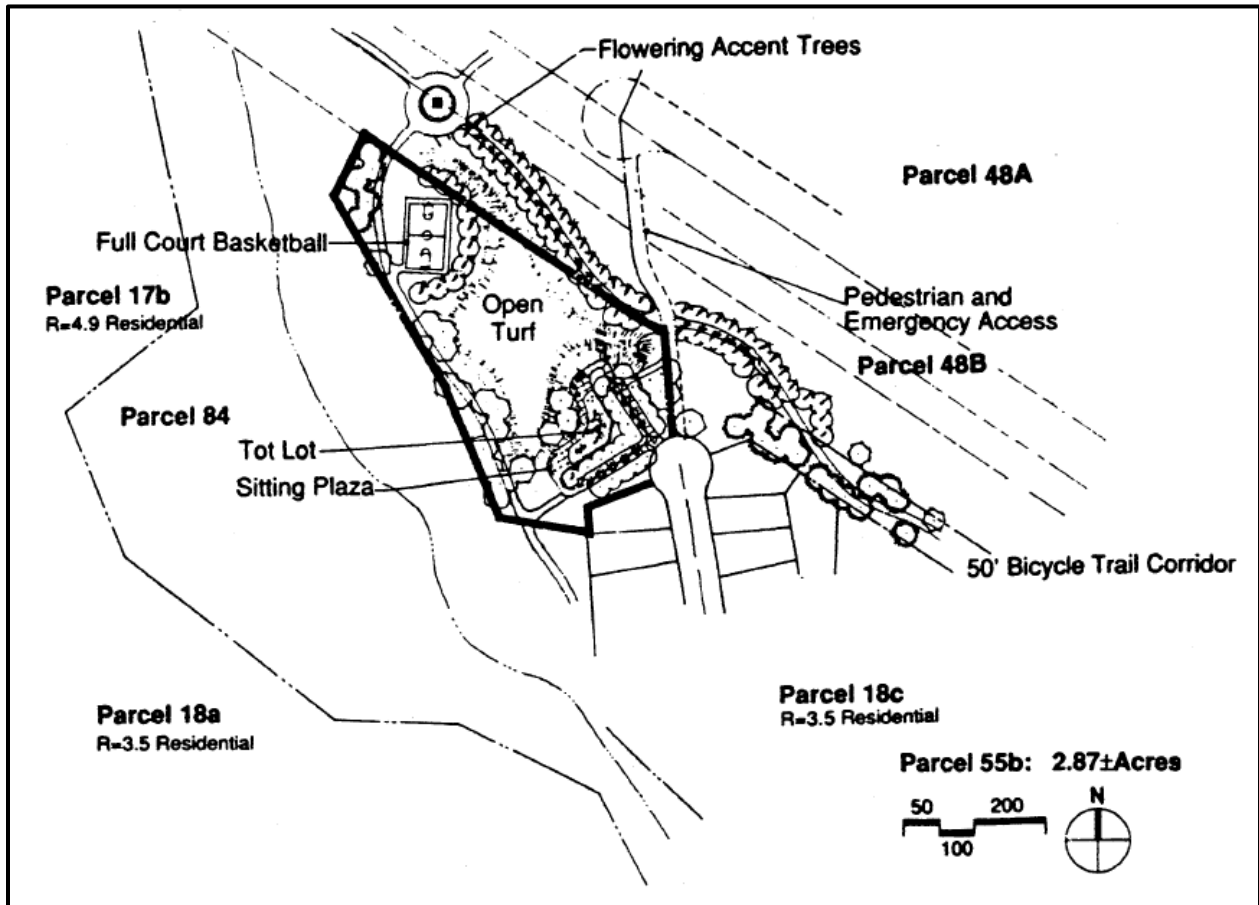


Figure 7-3b: Neighborhood Park

*Parcel 55B: 3.23 Acres*

This neighborhood park is located adjacent to lower watershed Parcel 84. This park is connected to a larger community park. The pedestrian/bike trail system in the lower watershed area to the west will include:

- 1 basketball court
- 1 tot lot
- open turf areas

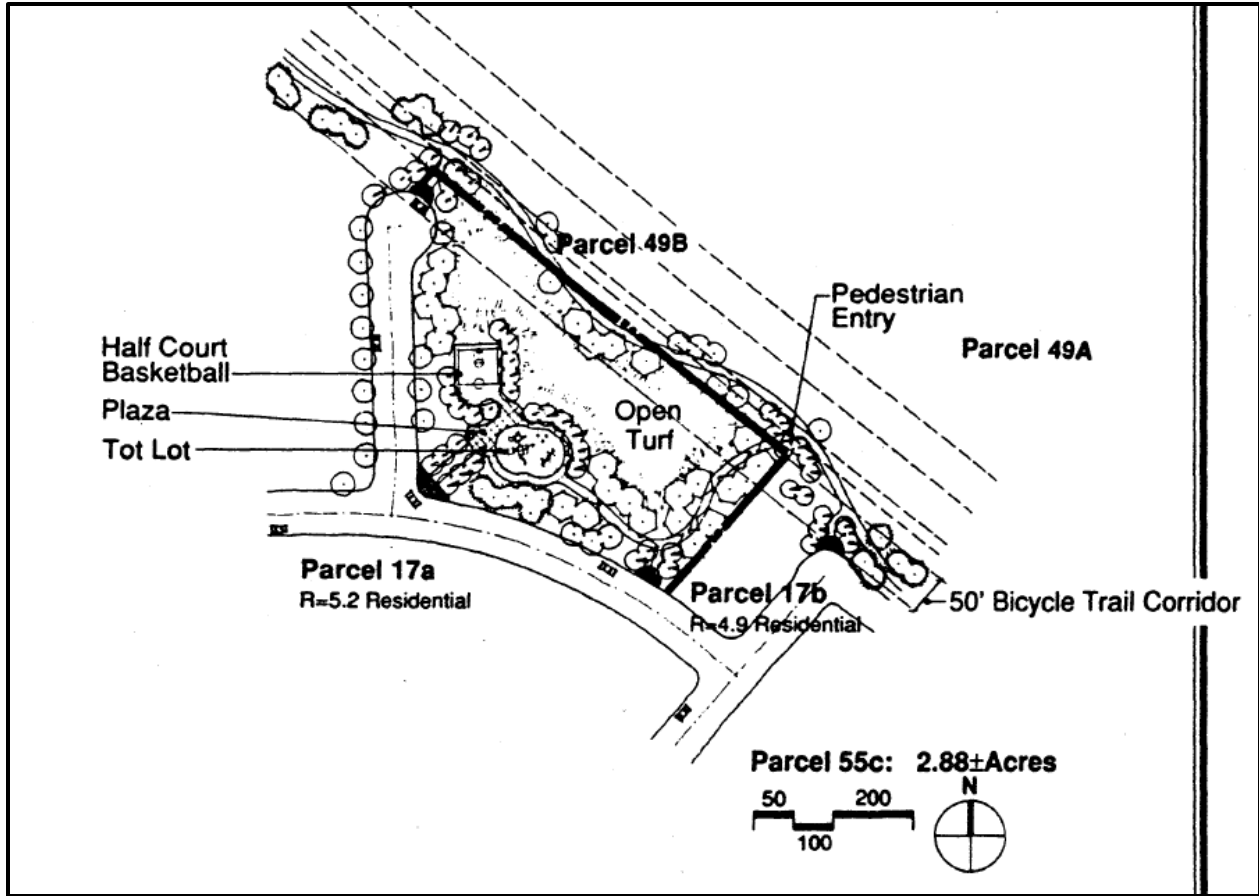


Figure 7-6c: Neighborhood Park Schematic

*Parcel 55C: 2.9 Acres*

This neighborhood park is located south of light industrial property (Parcels 49A and 49B) and immediately adjacent to Pleasant Grove Creek. The park's location will provide a centralized focal point for the adjacent neighborhood and a direct link to the plan-wide pedestrian/bike path which links to other parks and recreation facilities in the plan area. Recreation facilities in this park may include:

- 1 half court basketball court
- 1 picnic area and tot lot
- open turf areas

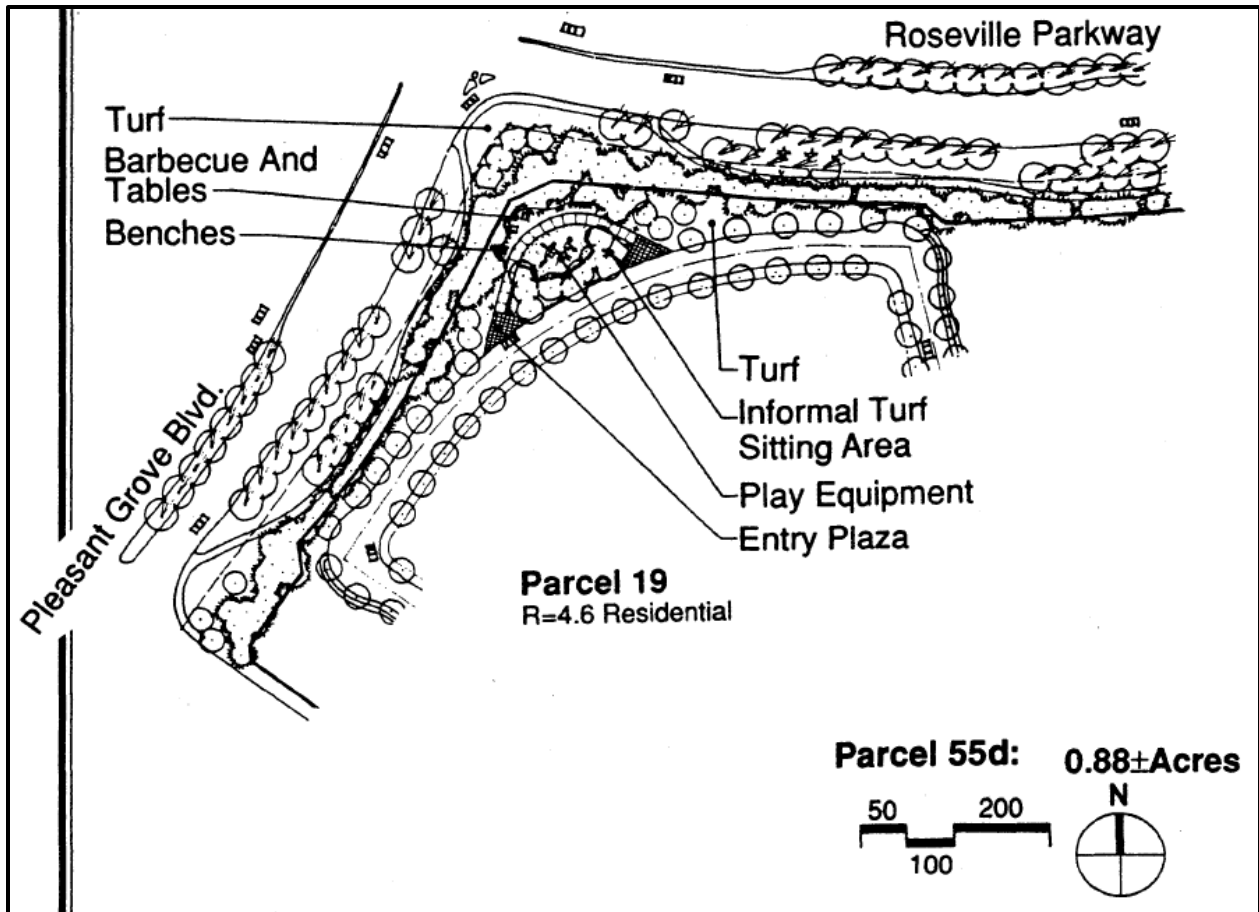


Figure 7-6d: Neighborhood Park Schematic

*Parcel 55D: 2.55 Acres*

This neighborhood park is located in the southeast corner of the intersection of Roseville Parkway and Pleasant Grove. The park will provide a focal point for neighborhood interaction and an expanded play area with tot lot for neighborhood children. Recreation facilities in this park may include:

- 1 picnic area
- 1 tot lot
- open turf areas

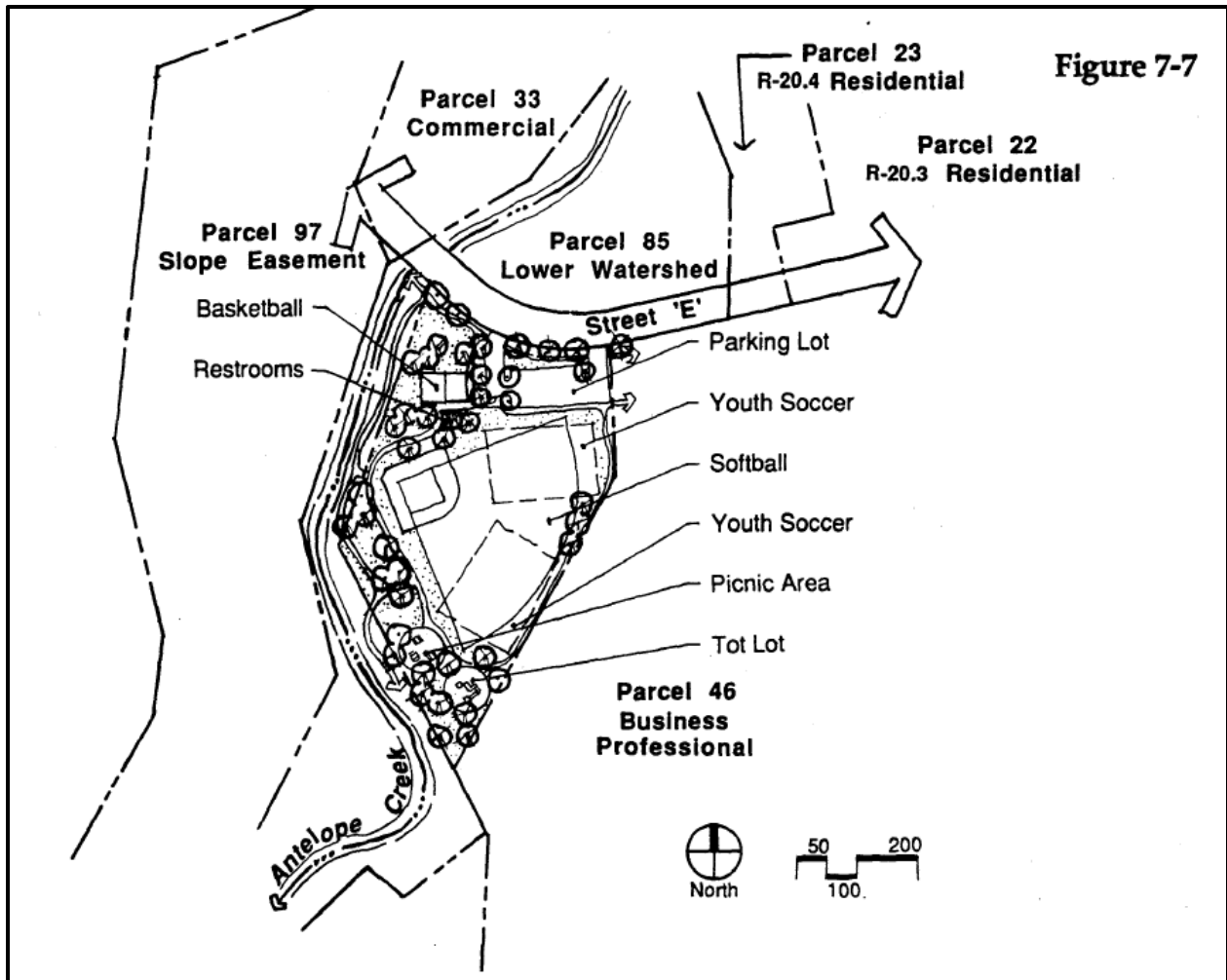


Figure 7-7: Neighborhood Park Schematic

*Parcel 57: 5.66 Acres*

This neighborhood park is located adjacent to Antelope Creek, east of Harding Boulevard. The site will take advantage of the riparian and oak woodland environments existing along the adjacent creek and slope easement. The interior trail system in the park will connect with the pedestrian/bike trail system in the adjacent lower watershed area, which will connect north and south to Rocklin and established areas in Roseville. The facilities available to residents of the nearby high-density housing (Parcels 22 and 23), the employees of the business-professional (Parcels 45 and 46), and the employees and users of the regional commercial developments (Parcel 36), may include:

- 2 basketball courts
- 1 softball diamond
- 2 youth soccer fields
- tot lot
- picnic area
- parking

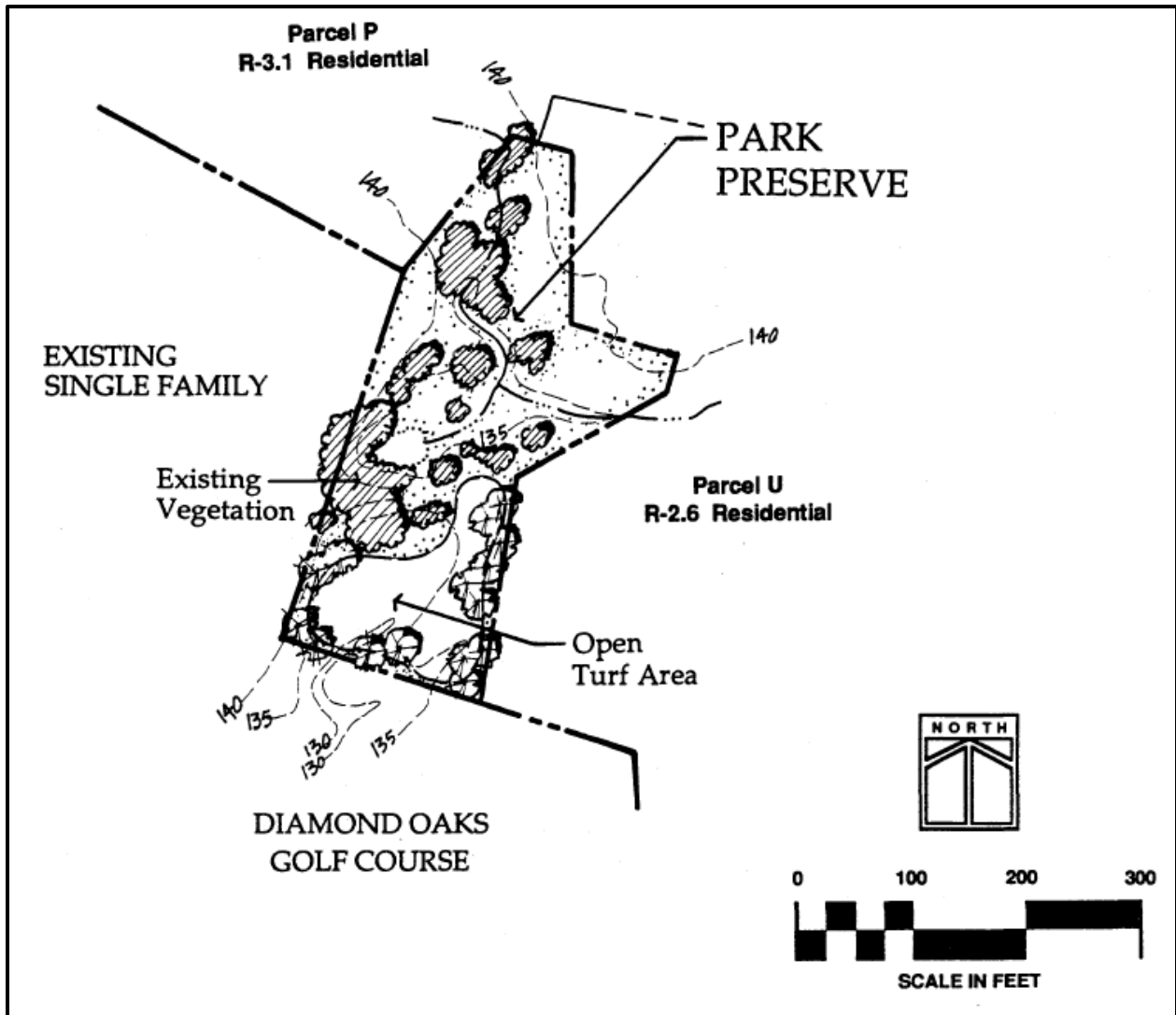


Figure 7-8: Park/Preserve Schematic

*Parcel 90: 1.81 Acres*

This park/preserve consists of a small riparian area located north of Diamond Oaks Golf Course within a single family residential neighborhood. The preserve may provide limited facilities such as a small turf area, picnic area, and benches. The site is intended to serve as a passive recreation/ open space area oriented to the immediate neighborhood.

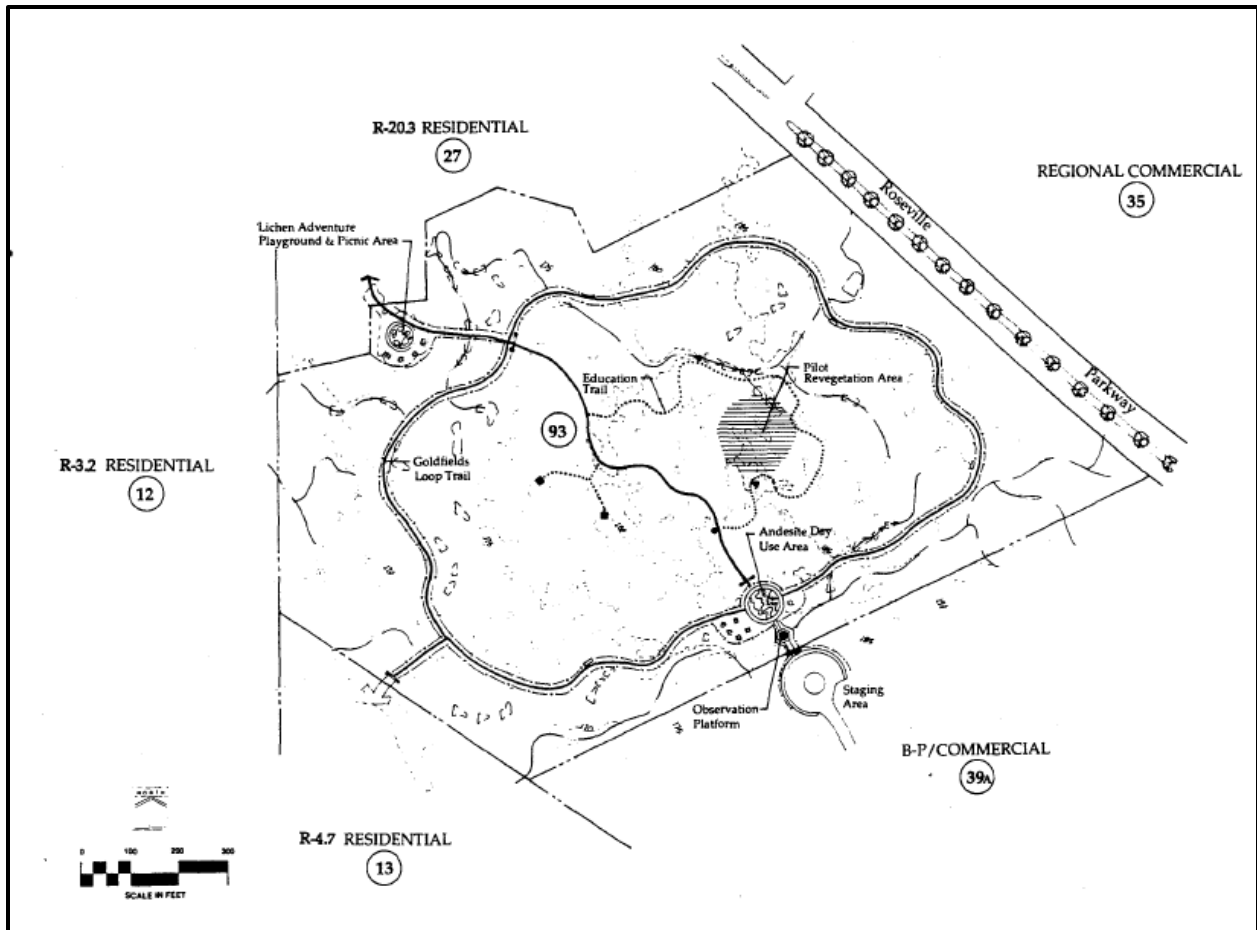


Figure 7-9: Park/Preserve Schematic

#### Parcel 93: 33.09 acres

This park/ preserve includes large areas for vernal pool/ wetland preservation and mitigation. It is intended that this facility will serve as an educational center for the native wetlands that occur in the Plan Area. Opportunities exist for nature study and education. The trail system within the preserve will provide a connection between the adjacent residential and BP I Commercial uses. Passive recreation facilities will include:

- observation platforms
- walking/ jogging trails
- interpretive trails
- playground/ picnic area

#### 7.5.2 Community Parks

A community park site, totaling 13.33 acres in size, is designated in the Plan (Parcels 56). The community park is intended to serve as a focal point of activity in the community and provide the opportunity for active recreational facilities, such as softball and field sports. The location of this park, adjacent to major roadways, potential light rail corridors, potential future light rail, multi-family, non-residential uses allows for intensive development of facilities that would conflict with single family residential land uses. Tournament play and night games could be accommodated in this location.

The community park will include facilities that support community activities such as a public library, a community center building, a band shell or amphitheater, playfields, barbecue and picnic facilities, and softball diamond.

### 7.5.3 Park Preserves

Three park preserve areas (Parcels 90, 91, and 93) comprise 48.74 acres of the Plan Area and are designated to preserve vernal pool and wetland resources. In addition, the preserve areas provide passive recreation opportunities for activities such as jogging, walking, and nature study.

### 7.5.4 Golf Course Maintenance Facility

A 1.48 acre site (Parcel 64) is designated adjacent to the Diamond Oaks Municipal Golf Course for maintenance facility. This facility will be accessible from within the golf course. The maintenance facility is to be adequately screened by landscaping and fencing from nearby residential and open space areas. In addition, residential lots adjacent to the corporation yard shall have enlarged rear yard setbacks of no less than fifty feet (50'). Sewer and water will be provided by extension of the utilities from the residential subdivision planned to the south of the corporation yard (Parcel 10) at the time this subdivision is constructed. This site is to be conveyed to the City in exchange for an equivalent land area along the north edge of the golf course (Parcel 16).

### 7.6 Library

A site for a community branch library may be located in the 13.33 acre community park on Roseville Parkway (Parcel 56). This facility, along with the Central Library in downtown Roseville, will serve the needs of Plan Area residents. The branch library will require a building site of approximately 10,000 square feet.

### 7.7 Solid Waste Disposal

Solid waste disposal for Plan Area development is provided by the Western Regional landfill Authority at a landfill site northwest of the City. This site has a remaining collection expectancy of about thirty-five years. Recycling of household materials is encouraged. Recycling collection centers are permitted in all non-residential uses with the operation of such facilities to be in compliance with the City Zoning Ordinance. In addition, localized collection facilities are permitted within multi-family projects for the use of project residents.

### 7.8 Water

The City of Roseville has water rights to 32,000 acre feet per year (AF/Y) from Folsom lake under a contract with the Federal Bureau of Reclamation. The current city-wide usage is approximately 13,500 (AF /Y) for the existing population of approximately 40,000 residents. Current capacity of the city water treatment plant is 24 million gallons per day (mgd), but it is designed to expand to 48 mgd. Present peak usage is around 22 mgd.

Water will be delivered to the North Central Specific Plan Area via a thirty-six inch (36") trunkline water main from the Northeast Roseville Specific Plan Area. Water will be distributed throughout the Plan Area via a series of 12 to 42 inch lines located in the major arterial and collector streets. Distribution water lines will be installed at the time the streets are constructed in accordance with the Public Infrastructure Phasing Plan contained in the Implementation Section (Section 8). Major Plan Area water facilities are shown on the Water Plan, Figure 7-10.

Water conservation is a major long term consideration for all jurisdictions in the Sacramento region, including Roseville. Conservation efforts can include moderation of individual use and limitations on public use. The Plan policies on landscaping recommend the use of drought tolerant plant materials (Section 3) and efficient irrigation systems. The Landscape Design Guidelines will establish appropriate plant lists for the Plan Area.

### 7.9 Wastewater Disposal

The northwestern portion of the Plan Area is within the North Roseville-Rocklin Sewer Assessment District and is served by the Roseville Sewage Treatment Plant located on Booth Road near Dry Creek. The plant serves a large portion of the south Placer area and has an 11.75 million gallon per day (mgd) treatment capacity. The plant is currently operating at approximately 7.0 mgd. The plant is designed to be expanded in increments of 6 mgd. The far eastern portion of the Plan Area is not currently within the North Roseville-Rocklin Sewer Assessment District, but shall be annexed into such district.

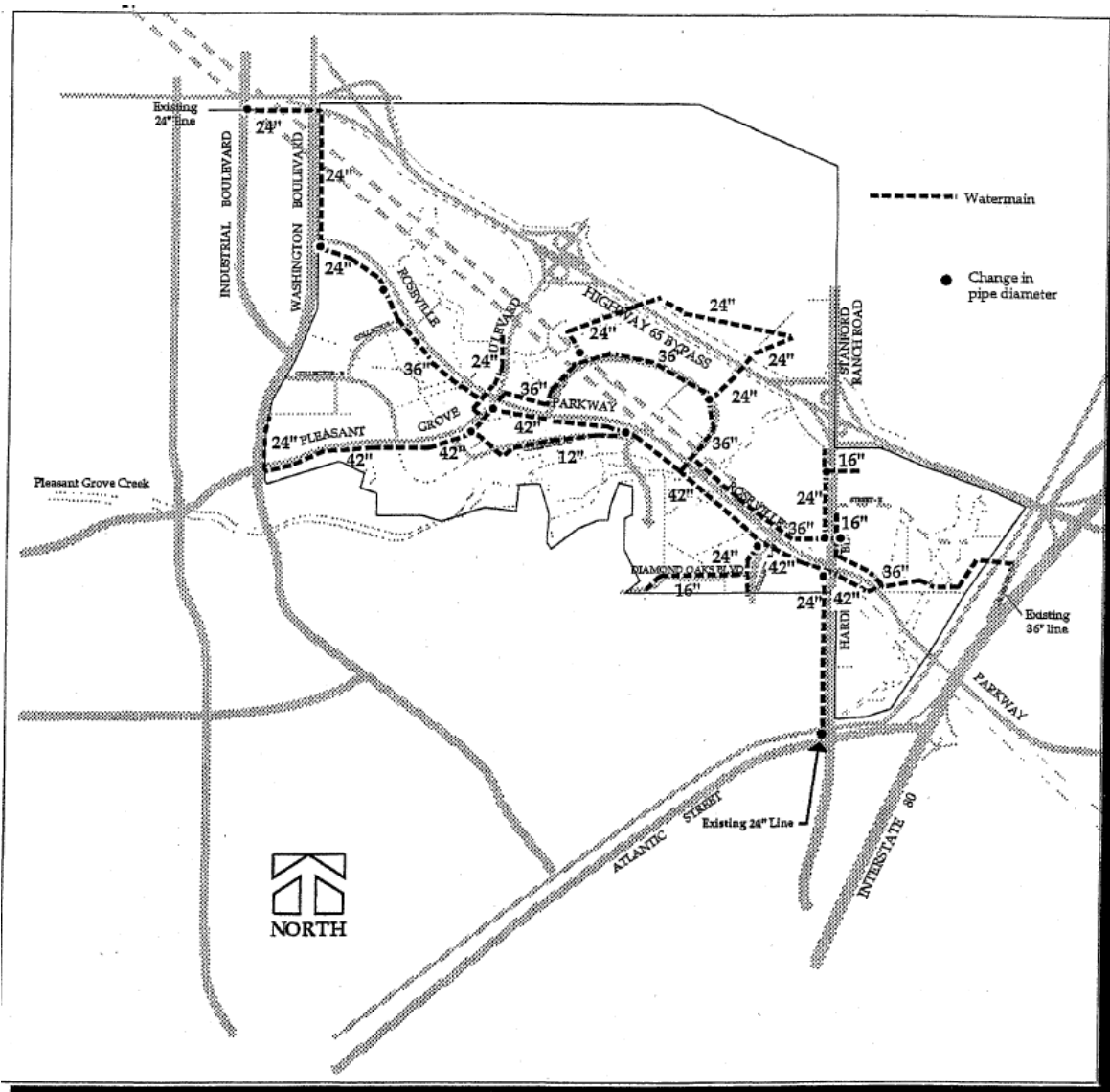


Figure 7-10: Water Plan

Proposed sewer line installations and that portion of the Plan Area within the Sewer District are reflected in Figure 7-11, Wastewater Disposal.

There will be an off-site sewer line following the drainage of Pleasant Grove Creek to the lift station in the northwest corner of the proposed municipal golf course in the Northwest Specific Plan Area, as illustrated in Figure 7-12, Off-Site Sewer Line Location. The construction of this line shall occur in accordance with the mitigation specified in the Plan Area EIR.

### 7.10 Storm Drainage

Over ninety percent (90%) of the North Central Roseville Specific Plan Area drains into Pleasant Grove Creek. The southern part of the site drains into the South Branch of Pleasant Grove Creek. The northern portion of the Plan Area drains into a channel that eventually merges with Pleasant Grove Creek. This major drainage channel originates in Rocklin and also carries drainage from the Sunset Whitney Golf Course. The remainder of the property along the eastern edge of the Plan Area drains into Antelope Creek.

Natural run-off in the Plan Area is generally very swift due to the shallow soil depths and underlying rock. These natural conditions have resulted in a run-off coefficient similar to that projected for paved areas. With the exception of Antelope Creek, the existing channels run dry within a short period of time, but handles relatively large volumes during and shortly after rainfall. Run-off on the Mehrten formations will not be increased substantially by development. However, development will tend to concentrate flows faster than natural conditions.

Major storm drainage will occur by means of open, natural channels in the lower watershed areas. These corridors will also serve as a pedestrian and bicycle circulation system that connects the community with the parks, schools, and retail shopping areas.

Implementation of a Master Drainage Plan which identifies drainage facilities designed to accommodate ultimate flow is required. The Master Drainage Plan shall identify appropriate measures, including detention basins that will be implemented to reduce the 100-year discharge to an amount less than or equal to pre-project flow conditions at the project limits. Detention basins, to be located within the Plan Area, have been preliminarily designed by Gill and Pulver, Civil Engineers, as illustrated in Figure 7-13, Proposed Detention Basin Sites. No development in the Plan Area will be approved until the drainage plans are approved by the City Council. In addition, the detention basin located on Parcel 87 (Site II) shall be constructed no later than October 1, 1991. The detention basin to be located at Site I will accommodate drainage from a total of 995 acres including approximately 380 acres within the NCRSP and approximately 615 acres within the Highland Reserve North Specific Plan. No more than a total of 50% of this drainage shed or 497.5 acres will be approved for development until the Site I detention basin (located on Parcel 42 of the Highland Reserve North Specific Plan) or a comparable City approved alternative is constructed.

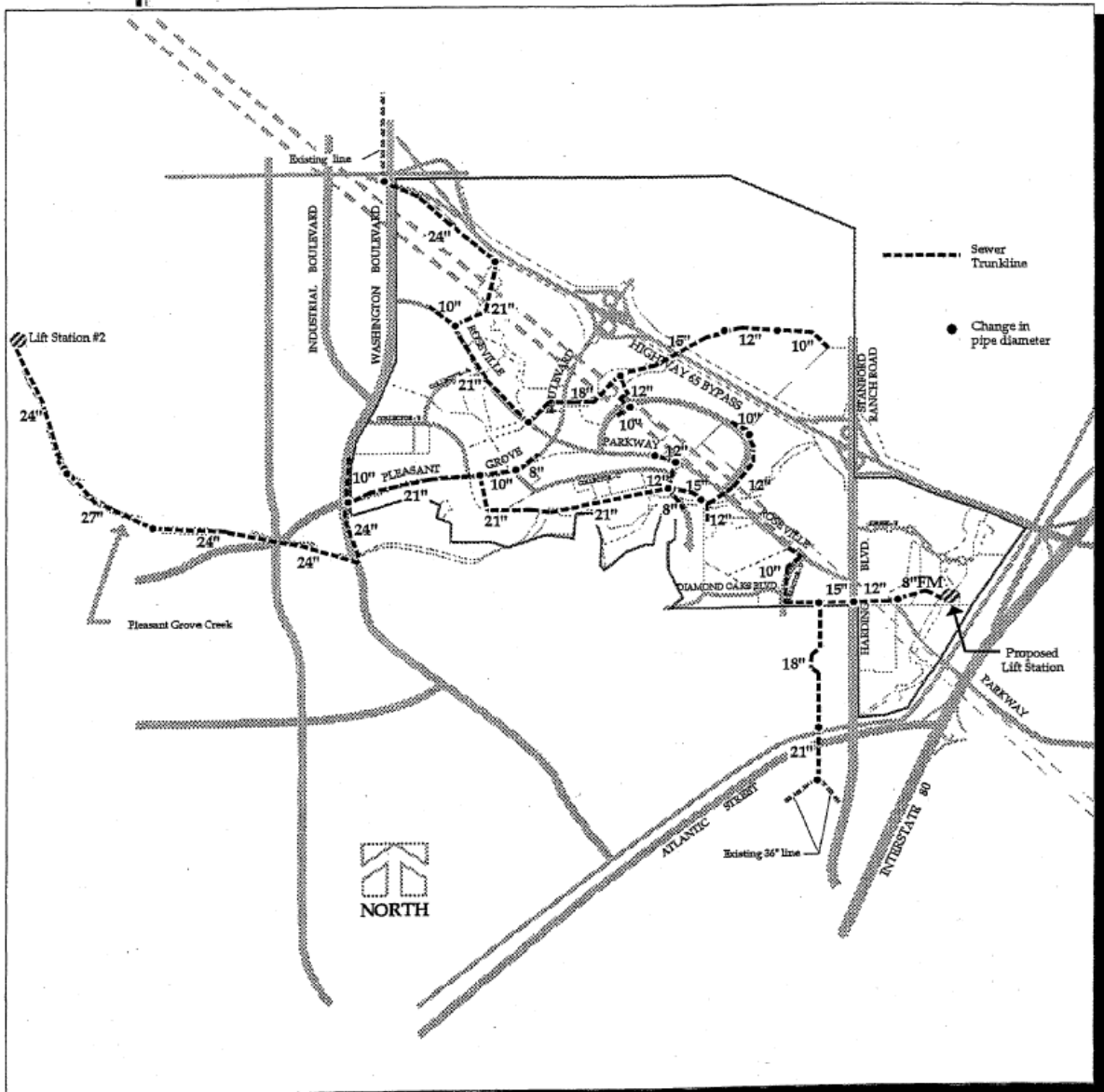


Figure 7-11: Wastewater Disposal

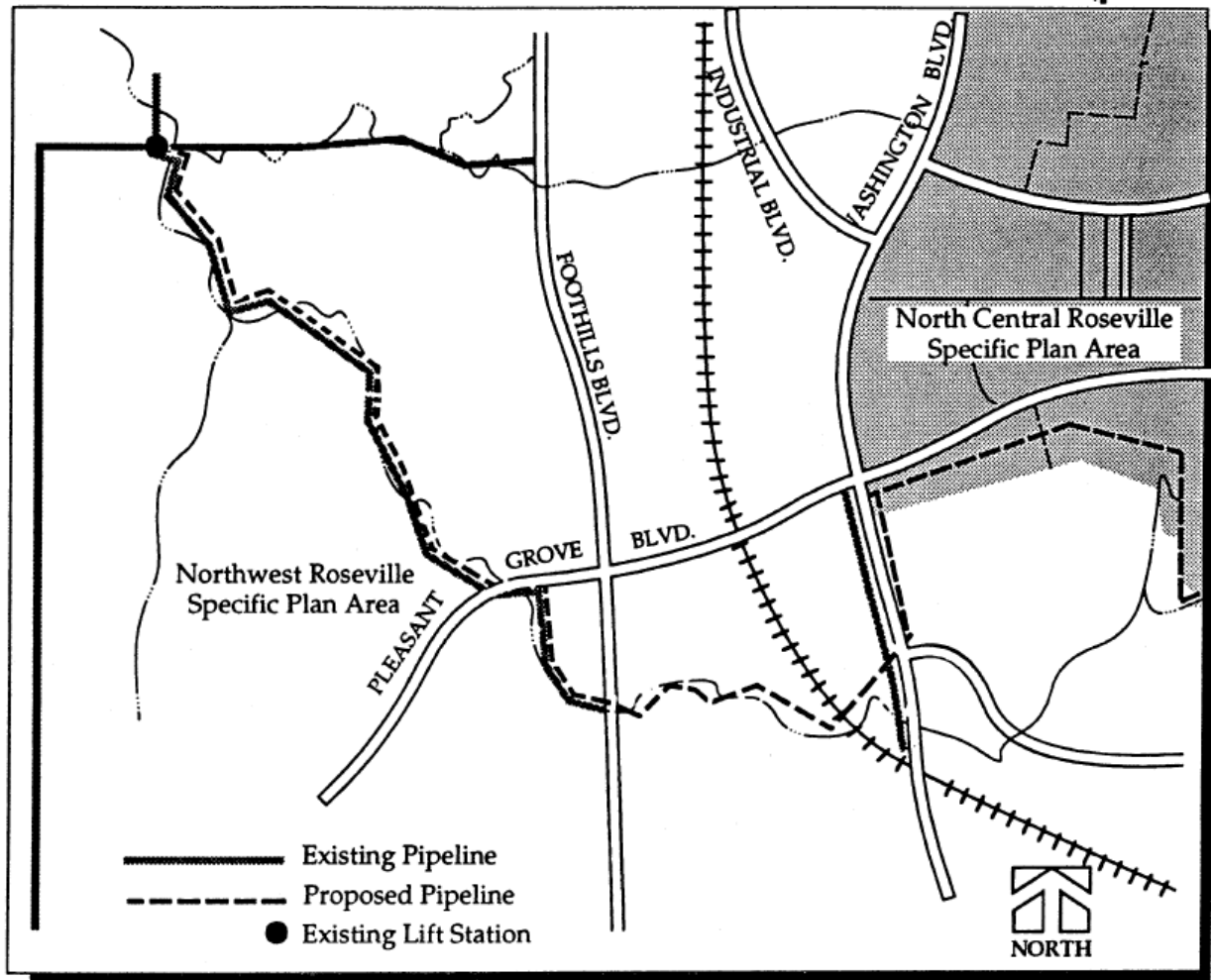


Figure 7-12: Off-Sewer Sewer Location

The City Public Works Department shall review all drainage facilities in the Plan Area as part of the approval and implementation of individual project plans. Overall run-off volumes from the North Central Roseville Specific Plan Area will be mitigated through the Plan Area participation in the regional drainage study as specified in the Plan Area development agreements.

## 7.11 Utilities

### 7.11.1 Electric Service

The Roseville Electric Utility Department supplies electricity in the Plan Area. Roseville purchases electricity from the Western Area Power Administration (WAPA), a Federal Agency. The City contracts for additional electricity from the Northern California Power Agency (NCPA).

Main electrical feeder lines exist within the Plan Area in the powerline easement adjacent to Roseville Parkway.

### 7.11.2 Energy Conservation

Energy will be required in the Plan Area for transportation, lighting, communications, and for water heating and environmental control. Conservation of energy can be achieved primarily through reduction

of automobile use and by modifying the man made environment to reduce heating and cooling requirements. Reduction of auto use is addressed in the Circulation Element (Section 5) and Land Use Element (Section 2). Reduction of energy required for cooling is achieved by providing shade in parking areas, around building and along streets. Reduction of energy required for space heating is achieved by proper siting of buildings to enhance solar heating and by building designs which utilize insulation, dual panel glazing, weather stripping and other techniques designed to minimize heat loss. All development within the Specific Plan Area shall give consideration to life cycle energy requirements in the design of individual buildings and in the siting and landscaping of building groups.

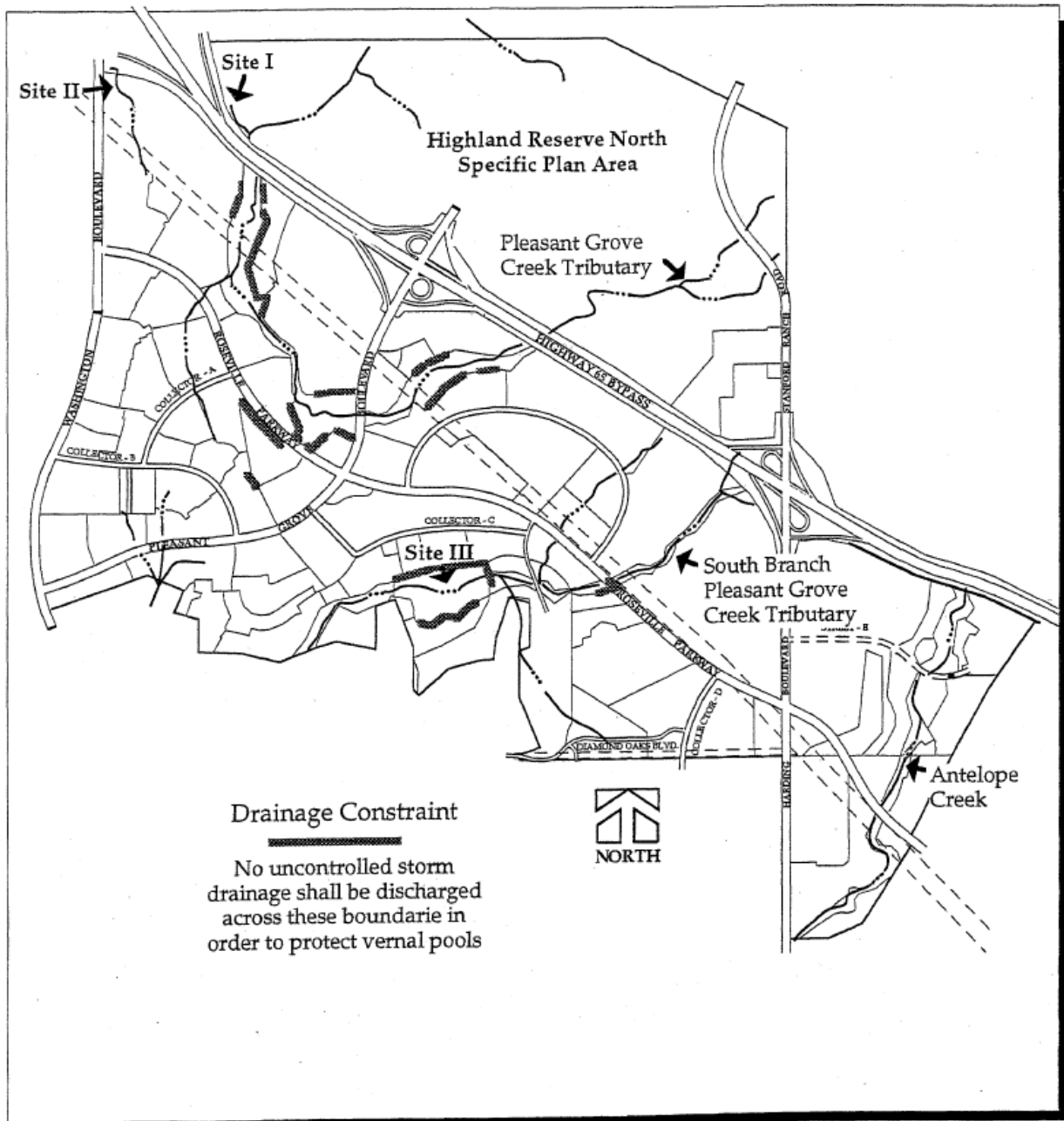


Figure 7-13: Proposed Detention Basin Sites

### 7.11.3 Planned Electric Substation

A one acre site is designated for an electric substation south of the powerline easement in the vicinity of the business-professional land use area north of the Roseville Parkway on Eastpark Drive. This site shall be buffered from adjacent land uses by installation of a wall constructed of masonry or other acceptable material, and by a minimum ten feet (10') wide landscaping strip as specified in the North Central Roseville Specific Plan Landscape Design Guidelines.

### 7.11.4 Street Lighting

Street lighting will be provided along all major arterials, with the exception of a portion of Stanford Ranch Road, at intervals in accordance with City policy. Energy-efficient systems will be employed and cut-off type luminaries will be used to reduce light spillage and glare impacts on private property. The portion of Stanford Ranch Road adjacent to Parcels 101 and 102 will not be lighted until the time that such areas may be approved for future development by the City.

### 7.11.5 Natural Gas

Natural gas will be provided by Pacific Gas and Electric Company (PG&E) on request and in accordance with the rules and tariffs of the California Public Utilities Commission. Although PG&E's long-range plans provide for availability of gas service to accommodate increased demand, delivery of gas service to any particular development will need to be reviewed by PG&E as each development is proposed. The growth envisioned in the Specific Plan area will place significant demands on existing transmission and distribution systems. Any new development will have a cumulative impact on PG&E's system and rights-of-way outside the development boundaries.

### 7.11.6

The North Central Plan Area is serviced by Roseville Telephone Company. Distribution lines to individual tracts will occur as development takes place. The builder will dig trenches and provide one-inch (1 ") PVC conduit from each unit to the service box at the property line, as required. All electrical and communication lines will be installed underground. Appurtenant facilities such as transformers may be installed at grade level.

## 7.12 Specific Plan Public Facilities and Services Policies

The following policies shall apply to the provision and implementation of public facilities and services in the North Central Roseville Specific Plan Area.

### 7.12.1 General Policies

1. Dedication requirements for all public facilities including parks, lower watershed preserve areas, park/preserves, fire station and electric substations shall be set forth in the Development Agreement entered into by the City and the appropriate property owner.
2. All public and institutional uses shall be designed and landscaped in a manner that complements adjacent non-public uses and shall incorporate landscaping, setbacks and siting standards similar to those required in adjacent land uses.
3. All public facilities which are open to the general public shall provide pedestrian access to adjacent uses and to the Plan Area pathway system, where feasible.
4. All public facilities are to be constructed consistent with policies in the Resources Management Element, Section 6 and the Plan Area EIR.

#### 7.12.2 Fire Protection

1. A break in the landscape median and a traffic signal shall be provided along Roseville Parkway at the entry to the planned fire station site.
2. Uses which require a business plan for emergency response per City ordinance shall also require a review of hazardous materials management techniques and disposal methods in the use permit.
3. All development projects in the North Central Roseville Specific Plan Area shall be reviewed by the City of Roseville Fire Department for the inclusion of fire prevention measures. Contact with the Fire Department early in the project design stage is encouraged.

#### 7.12.3 Police Protection

1. Police Department review of all development proposals will be required in the project review process. Contact with the Police Department early in the project design stage is encouraged.
2. Private security features such as quality door and window hardware are encouraged.
3. At the time that a regional mall is developed in the Plan Area, consideration will be given by the City to a police substation on the site.

#### 7.12.4 School Facilities

1. Landowners will be required to fully mitigate school impacts in accordance with the Development Agreement adopted for the North Central Roseville Specific Plan.
2. All school sites designated in the Plan Area shall be reserved for purchase by the School Districts in accordance with the provision of the Plan Area development agreements.
3. Unless provided on adjoining land uses, substantial setbacks and landscape buffering shall be provided within school sites.
4. The school districts and City are encouraged to enter into joint-use agreements to maximize public use of facilities and avoid duplication of services.
5. A fifty foot (50') wide landscape corridor and six foot (6') high masonry sound wall shall be provided along Washington Boulevard to buffer the intermediate school site.

#### 7.12.5 Recreation and Parks

1. Parks shall be designed to minimize maintenance requirements.
2. All park facilities shall provide direct connections to adjacent uses and the Plan Area pathway systems. Substantial setbacks and landscape buffering shall be provided within park sites abutting other land uses unless such adjoining uses provides such buffering.
3. Parks shall be designed to facilitate surveillance by adjoining residents, security services and police.
4. Parks shall be of an appropriate size, and shall contain apparatus and features consistent with the recreation needs of nearby residents and employees.
5. **DELETED 1/96.**
6. The golf course maintenance facility is to be adequately screened by berms, landscaping or fences from nearby residences and open space areas. In addition, adjacent residential lots backing onto the corporation yard shall have enlarged rear yard setbacks of no less than fifty feet (50'). Sewer and water shall be provided to the facility by the extension of the utilities from the residential subdivision planned to the south of the corporation yard (Parcel 10) at the time such subdivision is constructed.

#### 7.12.6 Library

1. A community branch library site may be provided within the Community Park (Parcel 56).

#### 7.12.7 Solid Waste Disposal

1. Recycling collection is permitted in all non-residential uses in accordance with the City Zoning Ordinance. In addition, localized collection facilities are permitted within multi-family projects for the use of project residents. Property owners within the Plan Area will participate in any recycling program adopted on a city-wide basis by the City of Roseville.

#### 7.12.8 Water

1. Efficient plumbing fixtures, irrigation systems, drought tolerant plantings, and other methods should be utilized within the Plan Area to reduce water consumption. The North Central Roseville Specific Plan Area Landscaping Design Guidelines will generally specify plant materials with low water requirements.

#### 7.12.9 Sewer

1. Strict implementation of all conditions and requirements of the Section 1601-03 Streambed Alteration Agreement and the Plan Area EIR shall be followed for the sewer line extension along the South Branch Pleasant Grove Creek.

#### 7.12.10 Storm Drainage

1. Urban run-off should be directed to the pre-existing watershed.
2. The Roseville Public Works Department shall review all drainage facilities prior to improvement and approval of individual project plans.
3. The detention basin located on Parcel 87 shall be constructed no later than October 1, 1991.
4. No more than a total of 50% of this drainage shed or 497.5 acres will be approved for development until the Site I detention basin (located on Parcel 42 of the Highland Reserve North Specific Plan) or a comparable City approved alternative is constructed.
5. Overall run-off volumes from the North Central Roseville Specific Plan Area will be mitigated through Plan Area participation in a regional drainage project as specified in the Plan Area Development Agreement.

#### 7.12.11 Utilities

1. Utility easements to serve specific parcels will be designated at the time of project review to provide services as required.
2. All electrical, telephone, and cable services shall be placed underground.
3. Public utilities, such as transformers, terminal boxes, meters, fire risers, backflow preventers and other similar facilities, shall be screened and oriented away from public view to the extent feasible.
4. The electrical substation shall be screened by a masonry wall or other type of wall acceptable to the City, and a minimum ten foot (10') wide landscape corridor as specified in the Landscape Design Guidelines.
5. As a method of reducing solar gain, which may increase interior air conditioning requirements, trees shall be planted and maintained in all non-residential parking areas to ensure that, within fifteen (15) years of planting, at least fifty percent (50%) of the parking area is shaded at mid-day during the summer season.

6. All habitable structures shall be designed and oriented to maximize the potential for energy conservation wherever feasible. Such measures shall include, but are not limited to, utilization of both passive and active solar systems.
7. Throughout the Plan Area deciduous trees, which aid summer cooling and allow solar gain for winter heating, shall be appropriately sited.
8. All cost effective energy conservation and peak usage reduction measures shall be incorporated into development design including those measures required by Title 24 and the City of Roseville Energy office.

## 8. IMPLEMENTATION

The Implementation section addresses the enforcement of standards, the funding of capital improvements and the phasing of development.

### 8.1 Land Use Regulation

#### 8.1.1 Zoning

All land within the Plan Area, with the exception of Parcel 69, shall be zoned as Planned Development zoning districts described in the Roseville Zoning Ordinance in this Specific Plan and in the Plan Area Development Agreement. Parcel 69 shall be zoned as General Industrial in accordance with the processing of the City of Roseville Zoning Ordinance.

#### 8.1.2 Development Agreement

The property owners, subject to the provisions of this Specific Plan, will execute development agreements in accordance with City Ordinance No. 802. No parcel in the Plan Area will be re-zoned until the agreement with respect to such parcel has been executed. The development agreements will set forth the infrastructure improvements, public dedication requirements, landscaping amenities and other contributions to be made by a property owner, in return for guarantees by the City that certain land uses and densities in effect at the time of execution of agreement will not be modified.

#### 8.1.3 Easements

In most instances the bikeways and pedestrian pathways will be located within a street right-of-way or other public ownership. In those few instances where the open space areas will be privately owned such as the bike trail adjacent to Highway 65 in Parcels 40 and 42, dedication of public access easements will be required to assure right of access by the general public for bikeways and pedestrian pathways.

Where the landscape corridor extends beyond the public right-of-way, adjacent to multi-family and non-residential uses an easement shall be granted to the City for landscaping maintenance purposes and bikeway access.

Easements for public utilities access and maintenance will be dedicated to the City in accordance with the provisions of the development agreements and as required through the design review for individual projects.

#### 8.1.4 Development Plan Approval

All individual development plan proposals in the North Central Roseville Specific Plan shall be subject to the standard development review process as specified by City Ordinance.

### 8.2 Maintenance of Common Facilities

Private common areas in multi-family developments, non-residential uses, and other open space areas will be maintained by the property owner. Landscape setback areas along arterial streets will be maintained by the property owner in multi-family residential and all non-residential properties. Landscaped setback areas adjacent to single family residential areas shall be maintained using funding provided by a Landscape and Lighting Maintenance District created for this purpose. The Landscape and Lighting Maintenance District shall maintain the following, as listed in The Development Agreements:

- scenic corridor along Roseville Parkway, Pleasant Grove Boulevard, Harding Boulevard and Washington Boulevard to all park sites;
- all public pedestrian and bicycle pathways which connect single-family residential uses to arterials and park sites;
- pedestrian and bicycle pathways within the wetland preserve and setback areas;
- detention basins.

### 8.3 Public Facility Financing

Public facilities will be provided through a variety of mechanisms including land dedications, reimbursement agreements and assessment districts as described below.

#### 8.3.1 Land Dedication and Conveyances

Land will be conveyed to the City for the following purposes:

- Parks and Park Preserves
- Street Right-of-Way
- Lower Watershed Preserves
- Fire Station
- Electric Substation
- Golf Course Corporation Yard

School sites will be reserved in accordance with the provisions of the Plan Area Development Agreement.

#### 8.3.2 Assessment Districts

A portion of the North Central Plan Area is within the North Roseville-Rocklin Sewer Assessment District: This District funded the basic sewer system now in place, as well as improvements to the City Wastewater Treatment Facility. Those portions of the Plan Area currently not in this district will be annexed into it.

The basic street improvements, wastewater collection system, water distribution and storm drainage, electric infrastructure and street lighting facilities that are to be constructed as required to develop the land use in the Plan Area, shall be funded by a Community Facilities District, Mello-Roos District, City fee or other funding mechanism instituted by the city.

The Community Facilities District will be formed to fund the basic sewer, water, drainage, and street improvements required. The specific street improvements that the mechanism will fund include:

- Harding Boulevard Interchange
- Harding Boulevard extension
- Pleasant Grove Boulevard
- Roseville Parkway
- Diamond Oaks Boulevard
- Eastpark Drive
- Collector streets

In addition, the CFD mechanism will fund parks, schools, and lighting and landscaping improvements in the Plan Area.

### 8.3.3. Developer Financing

All of the primary infrastructure will be funded under the City fee program, the proposed Community Facilities District or a similar financing mechanism. The construction of other street, street light, water or sanitary sewer trunklines will be financed by the developer subject to, if appropriate, reimbursement agreements administered by the City or the developer. These agreements will require subsequent developers within and outside of the Plan Area to reimburse the master developer for a portion of the initial installation cost based on a "fair share" formula. The specific provision of these methods as applied to the Specific Plan Area will be adopted in the Plan Area Development Agreements.

### 8.3.4 Parks Funding

Park improvements within the North Central Plan Area will be funded by a fee on residential development established through City ordinance and as specified in the Plan Area Development Agreements.

## 8.4 Sequencing of Improvements

The City shall not be required to process any application for a tentative map, use permit or building permit for Parcels A, B, C, D, E, F, G, M, N, O, P, U, or V until the Community Facilities District has been formed, the special tax levied and the bonds financing the improvements described in Section 3.B.3 of the development agreement have been sold. The City shall not be required to process applications for tentative maps or use permits for Parcels H, I, J, K, L, Q, R, S, T, W, X, Y, 9, 10, 11, 12, 13, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 40, 41, 45, 46, or 47 until the improvements described in Section 3.B.3 of the Development Agreement are under construction. The City shall not be required to process applications for tentative maps or use permits for any other parcel within the plan area (20, 21, 42, 43 or 48) until the improvement described in the development agreement have been completed and have been accepted by the City of Roseville. Figure 8-1 graphically shows the sequencing of improvements.

## 8.5 Mitigation Monitoring

A mitigation monitoring program consistent with Public Resources Code §21081.6 has been adopted with the Final Environmental Impact Report for the Specific Plan and is incorporated as an appendix in this Plan.

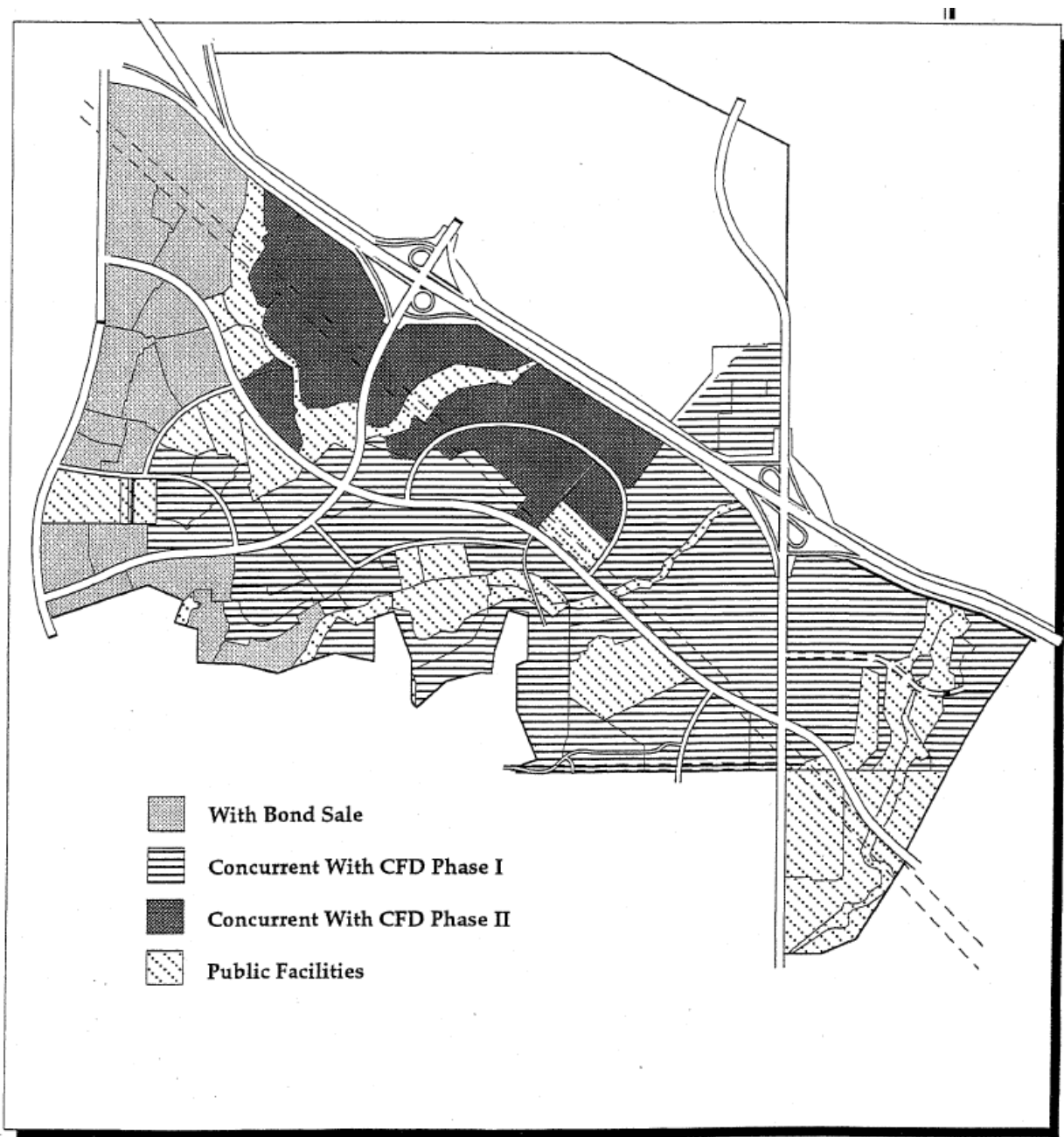


Figure 8-1: Processing Sequence